

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
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Si necesita ayuda para comprender esta información, por favor llame 503-588-6173

DECISION OF THE PLANNING ADMINISTRATOR

CLASS 3 SITE PLAN REVIEW / CLASS 2 ADJUSTMENT CASE NO.: SPR-ADJ23-17

APPLICATION NO.: 22-125120-PLN

NOTICE OF DECISION DATE: June 29, 2023

SUMMARY: Proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements.

REQUEST: A Class 3 Site Plan Review application for a proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements; together with a Class 2 Adjustment to the development standards of the Fairview Refinement Plan II refinement plan to:

- 1) Allow the southeast corner of the building to exceed the maximum allowed 20-foot setback abutting Strong Road SE and allow less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line;
- 2) Allow the proposed building to exceed the maximum 20-foot side street setback abutting the private internal street;
- 3) Allow the off-street parking area to the north of the proposed building to be setback less than the minimum required 20-foot setback abutting Lindburg Road SE and the private internal street; and
- 4) Allow the minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

The subject property is approximately 1.2 acres in size, zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and located at 3985 Lindburg Road SE (Marion County Assessor Map and Tax Lot Number: 083W11AB02900).

APPLICANT: Sarah Rose, AC+Co, on behalf of Reid Saunders Evangelistic Association

LOCATION: 3985 Lindburg Rd SE, Salem OR 97302

CRITERIA: Salem Revised Code (SRC) Chapters 220.005(f)(3) – Class 3 Site Plan Review; 250.005(d)(2) – Class 2 Adjustment

FINDINGS: The findings are in the attached Decision dated June 29, 2023.

DECISION: The **Planning Administrator APPROVED** Class 3 Site Plan Review and Class 2 Adjustment Case No. SPR-ADJ23-17 subject to the following conditions of approval:

Condition 1: The off-street parking area to the north of the proposed building shall be configured to meet the maneuvering and turnaround requirements of SRC 806.035(f)(2).

- Condition 2:** The applicant shall coordinate with Cherriots to locate and construct a transit stop conforming to applicable Salem Area Mass Transit District Standards and the Public Works Design Standards (PWDS) on Strong Road SE.
- Condition 3:** Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE.
- Condition 4:** Dedicate a public access easement for Village Center Loop SE within the development.
- Condition 5:** A maintenance agreement for the perpetual operation and maintenance of Village Center Loop SE shall be provided.
- Condition 6:** Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and the Public Works Design Standards (PWDS).
- Condition 7:** Additional windows shall be incorporated into the ground floor facades of the building facing Lindburg Road SE and Strong Road SE. The additional windows shall be provided by extending the specific windows identified in Attachment I to this decision to the floor of the building.
- Condition 8:** The adjusted development standards shall only apply to the specific development proposal shown in the approved site plan. Any future development, beyond what is shown in the approved site plan, shall conform to all applicable development standards of the Unified Development Code and the Fairview Refinement Plan II refinement plan, unless adjusted through a future land use action.

The rights granted by the attached decision must be exercised, or an extension granted, by July 15, 2027, or this approval shall be null and void.

Application Deemed Complete:	<u>May 12, 2023</u>
Notice of Decision Mailing Date:	<u>June 29, 2023</u>
Decision Effective Date:	<u>July 15, 2023</u>
State Mandate Date:	<u>September 9, 2023</u>

Case Manager: Bryce Bishop, Planner III, bbishop@cityofsalem.net, 503-540-2399

This decision is final unless written appeal and associated fee (if applicable) from an aggrieved party is filed with the City of Salem Planning Division, Room 320, 555 Liberty Street SE, Salem OR 97301, or by email at planning@cityofsalem.net, no later than 5:00 p.m., Friday, July 14, 2023. The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section, SRC Chapter(s) 220 and 250. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The Hearings Officer will review the appeal at a public hearing. After the hearing, the Hearings Officer may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review by contacting the case manager, or at the Planning Desk in the Permit Application Center, Room 305, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>

BEFORE THE PLANNING ADMINISTRATOR OF THE CITY OF SALEM

DECISION

IN THE MATTER OF APPROVAL OF)	FINDINGS & ORDER
CLASS 3 SITE PLAN REVIEW &)	
CLASS 2 ADJUSTMENT)	
CASE NO. SPR-ADJ23-17;)	
3985 LINDBURG ROAD SE)	JUNE 29, 2023

In the matter of the consolidated Class 3 Site Plan Review and Class 2 Adjustment application submitted by the applicant, the Reid Saunders Evangelistic Association, the Planning Administrator, having received and reviewed the evidence and the application materials, makes the following findings and adopts the following order as set forth herein.

REQUEST

A Class 3 Site Plan Review application for a proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements; together with a Class 2 Adjustment to the development standards of the Fairview Refinement Plan II refinement plan to:

- 1) Allow the southeast corner of the building to exceed the maximum allowed 20-foot setback abutting Strong Road SE and allow less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line;
- 2) Allow the proposed building to exceed the maximum 20-foot side street setback abutting the private internal street;
- 3) Allow the off-street parking area to the north of the proposed building to be setback less than the minimum required 20-foot setback abutting Lindburg Road SE and the private internal street; and
- 4) Allow the minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

The subject property is approximately 1.2 acres in size, zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and located at 3985 Lindburg Road SE (Marion County Assessor Map and Tax Lot Number: 083W11AB02900).

PROCEDURAL FINDINGS

1. On December 29, 2022, an application for a Class 3 Site Plan Review and Class 2 Adjustment was submitted by Sarah Rose, of AC+CO Architecture Community, on behalf of the applicant and property owner, the Reid Saunders Evangelistic Association, for a proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements.

Because multiple land use applications are required in connection with the proposed development, the applicant chose to consolidate and process them together as one pursuant to SRC 300.120(c). When multiple applications are consolidated, the review process for the application follows the highest numbered procedure type required for the land use applications involved, and the Review Authority is the highest applicable Review Authority under the highest numbered procedure type. Based on these requirements, the proposed consolidated Class 3 Site Plan Review and Class 2 Adjustment is required to be reviewed by the Planning Administrator and processed as a Type II procedure.

2. After additional requested information was provided by the applicant, the application was deemed complete for processing on May 12, 2023, and notice of filing of the application was subsequently sent on May 22, 2023, pursuant to Salem Revised Code (SRC) requirements.
3. The 120-day state mandated local decision deadline for the application is September 9, 2023.

SUBSTANTIVE FINDINGS

1. Background

The application under review by the Planning Administrator is a consolidated Class 3 Site Plan Review and Class 2 Adjustment for development of an approximate 1.2-acre property located at 3985 Lindburg Road SE (**Attachment A**).

The proposal includes development of a new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements. Vehicular access to the proposed development will be provided by a proposed private street, Village Center Loop SE, which passes through the site and connects to Lindburg Road SE to the east and Strong Road SE to the south.

Bicycle and pedestrian access to, within, and through the development will be provided via sidewalks along existing streets on the perimeter of the site as well as a sidewalk and multi-use path proposed along Village Center Loop SE.

2. Applicant's Plans and Statement

Land use applications are required to include a statement addressing the applicable standards and approval criteria of the Salem Revised Code and must be supported by proof they conform to such standards and approval criteria. The plans submitted by the applicant depicting the proposed development, and in support of the proposal, are attached to the decision as follows:

- Site Plan: **Attachment B**
- Landscaping Plan: **Attachment C**
- Building Elevations: **Attachment D**

The written statement provided by the applicant addressing the applicable approval criteria associated with the proposal is included as **Attachment E**.

3. Summary of Record.

The following items are submitted to the record and are available: 1) all materials and testimony submitted by the applicant, including any applicable professional studies such as traffic impact analysis, geologic assessments, stormwater reports, and; 2) materials, testimony, and comments from public agencies, City Departments, neighborhood associations, and the public.

All application materials are available on the City's online Permit Application Center at <https://permits.cityofsalem.net>. You can use the search function without registering and enter the permit number listed here: 22 125120.

4. Neighborhood Association and Public Comments.

The subject property is located within the boundaries of the Morningside Neighborhood Association.

Neighborhood Association Contact: SRC 300.310 requires an applicant to contact the neighborhood association(s) whose boundaries include, and are adjacent to, property subject to specific land use application requests. Pursuant to SRC 300.310(b)(1), land use applications included in this proposed consolidated land use application request require neighborhood association contact. On December 29, 2022, the applicant contacted the Morningside Neighborhood Association to provide details about the proposal; thereby satisfying the requirements of SRC 300.310.

Neighborhood Association Comments: Notice of the application was provided to the Morningside Neighborhood Association pursuant to SRC 300.520(b)(1)(B)(v), which requires notice to be sent to any City-recognized neighborhood association whose boundaries include, or are adjacent to, the subject property. No comments were received from the neighborhood association.

Homeowners Association: Pursuant to SRC 300.520(b)(1)(B)(iv), notice is required to be provided to any active and duly incorporated Homeowners' Association (HOA) involving property subject to a Type II land use application. As indicated on the application form submitted by the applicant, the subject property is not located within a Homeowners' Association; therefore, HOA notice is not applicable.

Public Comments: In addition to providing notice to the neighborhood associations, notice was also provided, pursuant to SRC 300.520(b)(1)(B)(ii), (iii), (vi), & (vii), to property owners and tenants within 250 feet of the subject property. Prior to the comment deadline, four public comments were received that are included as **Attachment F**. The comments received expressed concern and opposition to the proposal, in summary, regarding the following issues:

- A. Traffic impacts. Comments received express concern regarding the traffic impacts of the proposed development. It is explained that the proposal would bring too much traffic into the area, there is no room for more cars, and if the property is developed it will eliminate the currently quality of the quiet and isolated streets.

Staff Response: The subject property and the surrounding area is located within the Fairview Mixed-Use (FMU) zone. The purpose of the FMU zone, and the Fairview Plan and various adopted refinement plans that implement the zone, is to provide for the mixed-use development of the former Fairview Training Center site. In addition, the subject property is also located within the VC (Village Center) area of the FMU zone and the Fairview Refinement Plan II refinement plan. The FMU zone, under SRC 530.010, describes the Village Center area as comprising:

“...the most intense and pedestrian-oriented residential, commercial, employment, and public services uses. Residential uses will have densities of no less than 16 dwelling units per net acre. Nonresidential uses include a mix of large and small scale commercial establishments, which cumulatively will be limited to not more than approximately 80,000 square feet of pedestrian-oriented retail. Office uses are encouraged.”

As identified above, non-residential uses are allowed and encouraged in the Village Center area of the FMU zone and therefore envisioned for the subject property. The traffic associated with the development of non-residential uses within the zone has also been accounted for in the Transportation Impact Analysis originally conducted with the adoption of the Fairview Plan and subsequently updated with the adoption of later refinement plans. In the most recent trip generation update conducted for the Fairview Refinement Plan II refinement plan in 2021, at total of approximately 50,000 square feet of general office use and approximately 20,000 square feet of specialty retail is identified as being anticipated in the refinement plan area. The proposed 9,000 square-foot office building is less than the total 50,000 square feet of office use anticipated in the refinement plan area under the transportation impact analysis. As such, the amount of traffic generated by the proposed use will not exceed that which has been previously evaluated and can be accommodated by the existing transportation system.

- B. Loss of open space. Concern is expressed about the loss of open space that will result from development of the property. It is explained that nearby residents enjoy the current empty space that is proposed to be built on because it is a nice empty field with lots of walking trails, wildlife, and nature sounds.

Staff Response: The subject property is located within the Village Center (VC) area of the FMU zone and the Fairview Refinement Plan II refinement plan. The Village Center area is intended to be the most intense and pedestrian-oriented residential, commercial, employment, and public services uses area. While the subject property is currently vacant and undeveloped, and has served as a form

of open area due to it not yet being developed, the refinement plan does not specifically identify the property as being an area reserved for open space or recreation. Instead, the refinement plan intends for the property to be further developed. Open space and recreational opportunities are, however, still available to serve the area in the form of Fairview Park, an approximate 26-acre community park located across Lindburg Road SE from the subject property.

- C. Reduction to parking requirements. Concern is expressed about the proposed variance to parking requirements for the proposed development. It is explained that reducing the amount of required parking will exacerbate an already difficult parking situation for local residents. The neighborhood currently experiences a significant deficiency in the provision of sufficient street parking along Lindburg Road due to 435 apartment units without sufficient parking and this situation exacerbates when vehicles are parked on both side of Lindburg Road resulting in limited visibility at the curve located at the intersection of Lindburg Road and Shall Street.

Staff Response: The proposed development includes four Class 2 Adjustment requests to specific development standards included in the refinement plan, but does not, however, include a request to vary or reduce the minimum number of parking spaces required for the development. Under the Fairview Refinement Plan II refinement plan, the minimum parking requirement for the proposed office use is one space per 500 square feet of building area, and 50 percent of the required parking may be located on the street within 200 feet of the lot it serves. Based on this standard, the proposed 9,000 square-foot building requires a minimum of 18 parking spaces and nine of those spaces may be located on the street. As show on the site plan submitted by the applicant (**Attachment B**), a total of 26 parking spaces are provided to serve the proposed office building. Seventeen of the spaces provided are located in the proposed parking lot to the north of the building and the remaining nine spaces are located on the private street, Village Center Loop SE, that is being extended through the property. The number of parking spaces provided to serve the development exceeds the minimum parking requirements of the refinement plan and therefore a variance or adjustment to the parking standards is not required in conjunction with the proposal.

- D. Proposed use is not appropriate in consideration of historical context of site. Concern is expressed about the nature of the proposed use and the impacts it may have on the neighborhood considering both the historical context of the site and the nature of the organization proposing the development. It is explained that the organization is primarily focused on aggressive evangelism and events, which does not seem to be an appropriate use for development of this historically significant site where individuals with mental health issues were held and which carries a complex history within the community. It is indicated that the proposed development's proximity to residential areas serves as an infringement on the quality of life for residents living nearby and that the organization's preference for large gatherings and festivals raises concerns about noise, privacy, and traffic patterns. It is explained that such activities could be invasive to the surrounding

residents, highlighting the inadequacy of current zoning laws in providing protection and maintaining environmental quality for local residents. It is instead recommended that the property be developed with a use that preserves the history of the site by establishing a memorial, park, community center with educational resources about mental health and the social history of the institution, or an establishment that provides mental health resources and support services for the community.

Staff Response: Section 2 (Permitted Land Uses) of the Fairview Refinement Plan II refinement plan establishes those land uses that are allowed within the refinement plan area. Pursuant to this section, the Permitted Use Table (SRC 530-1) is adopted as the permitted uses for the Adaptive Use (AU), Mixed-Intensity (MI), and Village Center (VC) areas of the refinement plan. Under the Fairview Mixed-Use zone permitted uses Table 530-1, Office use is specifically identified as a permitted use in the VC area and is therefore a permitted use on the subject property.

Office use is characterized under the City's Use Classification chapter, per SRC 400.050(a), as:

"...establishments that provide business and professional services to individuals, public or nonprofit entities, and/or businesses in an enclosed building, generally in an office environment. There are few visits by the general public, and little walk-in traffic. Those who come to the site are mostly employees, and, to a lesser extent, clients. Laboratory research is not a major component of the activities on-site."

As identified above, the Office use category does not include establishments that involve large gatherings and festivals that could be impactful and invasive to surrounding residents. Instead, the Office use category includes activities that provide business and professional services in an office environment. Because the use proposed by the applicant is an office, the use of the site is limited to that of an office and activities such as events and large gatherings are not allowed as an office use.

In regard to the concern raised pertaining to the appropriateness of the proposed use in consideration of the historic use of the former Fairview Training Center site, a determination of whether a use is allowed is based on whether the use is identified as being allowed in the refinement plan. In the case of the Fairview Refinement Plan II refinement plan, it defers to the table of allowed uses included in the FMU zone under SRC 530.040, Table 530-1, or order to identify those uses where are allowed and prohibited in the refinement plan area. In this case, Office is identified as a permitted use in the Village Center area of the refinement plan and is therefore a permitted use on the subject property.

- E. Insufficient justifications for requested adjustments. Concern is expressed that the justification provided for the requested adjustments is insufficient when considering the inherent purpose of the existing development standards.

Concerning the requested adjustment to the maximum building setback from Strong Road and the minimum required 70 percent lot frontage requirement, it is explained that this restriction is intended to maintain consistency in landscape and neighborhood aesthetics and that allowing this deviation could disrupt these aesthetics and create a negative visual impact.

Staff Response: As identified under Section 8 of this decision, the underlying purpose of this standard is to ensure that buildings are located in close proximity to the street in order to an active and inviting pedestrian environment. The requested adjustment to allow a small section of the southeast corner of the building to be setback approximately 1.25 feet beyond the maximum allowed 20-foot setback from Strong Road SE does not impact the building's appearance from the street because the majority of the building façade facing Strong Road is located with the minimum 10-foot to maximum 20-foot setback from Strong Road SE.

The portion of the adjustment pertaining the minimum required 70 percent lot frontage requirement was requested by the applicant due to the lot being a corner lot and the large amount of street frontage associated with the property. In order to meet this standard a minimum of 70 percent of the street frontages of the lot abutting both Lindburg Road SE and Strong Road SE would be required to be occupied by buildings placed at the setback line. In order to achieve this, the building would have to be designed as long narrow building stretched out along the frontages of these two streets, which would make the building less functional. The applicant has instead proposed to locate the building at the southeast corner of the lot adjacent to intersection of Strong Road and Lindburg Road so it can be placed in proximity to this prominent street intersection within the Fairview development and has also incorporated additional design details into the building to promote an urban and pedestrian-friendly appearance adjacent to these streets through an increased ground floor building height, deep roof overhangs, a main building entrance located close to the intersection of Strong Road and Lindburg Road, and an increased amount of ground floor windows in the building facades facing these streets. In order to further meet the underlying purpose of this standard, a condition of approval (Condition 7) has been included in this decision requiring the provision of additional windows on the building's facades facing Lindburg Road and Strong Road.

Concerning the requested adjustment to the maximum 20-foot side setback abutting the private internal street, it is explained that this regulation helps to maintain adequate buffer zones between structures to ensure appropriate distances for fire safety concerns, natural light, and air circulation and that deviating from this standard could compromise these crucial concerns.

Staff Response: The requested adjustment to the maximum 20-foot side street setback applies between the proposed building and Village Center Loop SE to the north. The adjustment requested by the applicant seeks approval to allow the building to be setback more than 20-feet from Village Center Loop. The increased building setback between Village Center Loop and the proposed

building actually results in a greater separation/buffer between the proposed building and surrounding properties, as well as improved distances for fire safety concerns, natural light, and air circulation.

Concerning the requested adjustment to the minimum required 20-foot setback for the off-street parking area to the north of the building from Lindburg Road SE and Village Center Loop SE, it is explained that this standard aims to minimize traffic congestion and maintain pedestrian safety and that if the proposed development is allowed to deviate from this setback, the safety of pedestrians and the flow of traffic might be compromised. In addition, it is indicated that this will create an unpleasant view from the street which is inconsistent with the existing neighborhood and will negatively impact the walking experience for pedestrians.

Staff Response: The underlying purpose of the minimum required 20-foot parking setback from streets is to buffer pedestrians on sidewalks from adjacent parked vehicles. As shown on the site plan, the proposed parking area is setback approximately 14 feet to 15 feet from Lindburg Road SE and approximately 7 feet to 8 feet from Village Center Loop SE. Although the proposed parking area is setback less than the minimum required 20-foot setback abutting these streets, the proposed setback of 14 feet to 15 feet abutting Lindburg Road actually exceeds the minimum required 10-foot parking setback from a street that is generally required elsewhere throughout the City under the City's off-street parking chapter (SRC Chapter 806), and the 7-foot to 8-foot setback abutting Village Center Loop falls just short of this required 10-foot setback. As indicated in the written statement provided by the applicant and shown on the proposed landscaping plan, the setback areas between the proposed parking lot and Lindburg Road and Village Center Loop include sufficient landscaping to buffer the parking lot from the adjacent sidewalks consistent with the underlying purpose of this standard.

Concerning the requested adjustment to the minimum required 0.75 FAR (floor-area-ratio), it is explained that this regulation ensures a balance between built-up areas and open spaces, as well as adequate provision for stormwater management and green spaces. It is indicated that allowing a deviation to this standard could result in environmental imbalances and increase risk of flooding.

Staff Response: Floor-area-ratio (FAR) is a development standard intended to regulate development density/intensity on a site. The Fairview Refinement Plan II refinement plan requires sites within the Village Center (VC) area of the refinement plan to be developed with a minimum FAR of 0.75. This means that a minimum of 75 percent of the total of the lot would be required to be covered by a building one-story in height. Based on the 1.2-acre size of the subject property, a roughly 40,000 square-foot one-story building would be required to be developed on the property. Alternatively, a two-story building with 20,000 square feet on each floor could be developed to meet the standard or a four-story building with 10,000 square feet on each floor could be developed. The proposed building is approximately 9,000 square feet in size, which falls below the required 0.75 FAR

based on the site of the lot. The smaller proposed building size does not, however, contribute to an environmental imbalance or an increased risk of flooding. Instead, the smaller proposed building size results in more area on the site available for stormwater management and landscaping.

5. City Department Comments

Building and Safety Division and Fire Department – Reviewed the proposal and provided comments indicating that: 1) It is assumed that the lots will be combined thereby eliminating concerns over building fire separation areas and utilities crossing property lines; and 2) The architect shall review the proposed building for fire sprinkler requirements due to the narrow road width.

Staff Response: The subject property is a single unit of land that was created as Lot 74 with the Legacy Heights subdivision plat. Because the subject property consists of only one unit of land, there are no internal property lines that need to be eliminated in order to accommodate the proposed development.

Fire sprinkler requirements for the building will be determined at the time of building permit review for the proposed building.

Public Works Department – Reviewed the proposal and provided comments pertaining to City infrastructure required to serve the proposed development. Comments from the Public Works Department are included as **Attachment G**.

6. Public Agency Comments

Salem Area Mass Transit District (Cherriots) – Reviewed the proposal and provided comments that are included as **Attachment H**. Cherriots indicates, in summary, that a transit stop has been identified as being needed in connection with the proposed development on Strong Road SE. As such, Cherriots requests construction of the transit stop in conformance with the applicable Salem Area Mass Transit District standards. Cherriots explains that on-street parking shall be restricted in the area of the transit stop in order to ensure unobstructed access by transit.

Staff Response: Pursuant to SRC 803.035(r), transit stops conforming to the applicable standards of the Salem Area Mass Transit District are required to be constructed when a transit stop is identified as being needed by the Transit District in connection with a proposed development. Because Cherriots has identified the need for a transit stop on Strong Road SE frontage of the property, construction of the transit stop is required per SRC 803.035(r). The site plan submitted by the applicant identifies a proposed new transit stop located on Strong Road. A condition of approval has been established with this decision requiring the transit stop to be constructed in conformance with the requirements of SRC 803.035(r) and Cherriots' applicable standards.

In addition to the required stop on Strong Road SE, Cherriots has also identified the need for a transit stop on Lindburg Road SE along the frontage of the property.

However, based on the nature of the sidewalk improvement along the property's Lindburg Road frontage, no additional improvements are required in conjunction with the proposed development because the existing sidewalk is suitable to accommodate this future stop

DECISION CRITERIA FINDINGS

7. Analysis of Class 3 Site Plan Review Approval Criteria

Salem Revised Code (SRC) 220.005(f)(3) sets forth the following criteria that must be met before approval can be granted to an application for Class 3 Site Plan Review. The following subsections are organized with approval criteria, followed by findings of fact upon which the decision is based. Lack of compliance with the following criteria is grounds for denial or for the issuance of conditions of approval to satisfy the criteria.

SRC 220.005(f)(3)(A): The application meets all applicable standards of the UDC.

Finding: The subject property is designated "Mixed-Use" on the Salem Area Comprehensive Plan Map and zoned FMU (Fairview Mixed-Use). Pursuant to SRC 530.015, development within the FMU zone is required to be undertaken pursuant to the Fairview plan and subsequent refinement plans. The Fairview Plan is the master plan that identifies the overall goals and policies for development of the Fairview site and refinement plans are detailed regulatory plans that implement the Fairview Plan.

Pursuant to SRC 530.030, standards and processes stipulated in an approved refinement plan supersede the standards and processes of the Unified Development Code (UDC) and shall be used as review criteria for any specific development proposal within the area covered by the approved refinement plan.

Because the subject property is located within an area of the Fairview site which has an approved refinement plan, the standards of the *Fairview Refinement Plan II* refinement plan supersede many of those of the Salem Revised Code (SRC) and are the standards applicable to the proposed development.

The proposed development conforms to the applicable standards of the Fairview Refinement Plan II refinement plan and the applicable standards of the Unified Development Code (UDC) as described below:

Fairview Refinement Plan II

Allowed uses and development standards within Fairview Refinement Plan II are differentiated based on specific overlay areas established by the FMU zone.

Pursuant to Section 3 of the refinement plan (*General Allocation and Identification of Major Proposed Land Uses*), there are three zones/overlay areas defined in the FMU zone and Fairview Plan which are present in the refinement plan. These

zones/overlay areas include the MI (Mixed-Intensity), AU (Adaptive Use), and VC (Village Center) areas.

The property proposed for development with this application is located within the VC (Village Center) area of the refinement plan and is therefore subject to the standards of the Fairview Refinement Plan II refinement plan applicable to development within the Village Center (VC) area.

Refinement Plan Section 2 (Permitted Land Uses)

Allowed uses within the refinement plan are identified under Section 2 (Permitted Land Uses) of the refinement plan. Pursuant to this section, the allowed uses within the refinement plan are, with a few limited exceptions, the uses identified in the Permitted Uses Table of the FMU zone, specifically Table 530-1.

The proposed development includes a new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements. As identified under Table 530-1 of the FMU zone, Office is a permitted use in the VC area and is therefore a permitted use on the subject property within the refinement plan.

Refinement Plan Section 7 (Table 1) (Development Standards)

▪ Density:

Allowed residential densities within the VC area of the refinement plan are identified in the following table:

VC Area Residential Density		
Overlay Area	Min. Density	Max. Density
VC Area	13 du/acre	35 du/acre

The proposal includes the development of a 9,000 square-foot office building. Because the proposal is for a commercial development rather than a residential development, the residential density requirements of the refinement plan are not applicable to the proposed development.

▪ Lot Standards:

Lot size and dimensions requirements applicable within the VC area of the refinement plan are summarized in the following table:

VC Area Lot Standards	
Lot Area	Min. 1,000 sq. ft.
Lot Width	Min. 20 ft.
	Max. 30 ft. per dwelling unit (<i>applicable to multiple family</i>)

Lot Depth	Min. 40 ft.
	Max. 200 ft. <i>(applicable to mixed-use/commercial and multiple family)</i>

The subject property is a legally established unit of land created as Lot 74 of the Legacy Heights subdivision plat that meets the applicable lot standards of the refinement plan.

▪ **Lot Coverage**

Lot coverage requirements applicable within the AU and VC areas of the refinement plan are summarized in the following table:

VC Area Lot Coverage Standards	
Building Coverage	Max. 70%
Building Footprint	Max. 10,000 sq. ft.
	Max. 1,000 sq. ft. <i>(applicable to accessory structures)</i>
Floor-Area-Ratio	Min. 0.75

Building Coverage. The proposed development conforms to the maximum building coverage standard of the refinement plan. The proposed development includes a two-story building that has a ground-floor square footage of approximately 6,957 ft. The resulting coverage of the proposed building on the 1.2-acre lot equals approximately 13.3 percent, which falls below the maximum building coverage requirement.

Building Footprint. The proposed development conforms to the maximum building footprint requirements for building and accessory structures established under the refinement plan. The proposed office building has a building footprint of approximately 6,957 square feet, which does not exceed the maximum 10,000 square-foot building footprint allowed under the refinement plan. Similarly, there are no accessory structures included within the development that have a footprint greater than 1,000 square feet.

Floor-Area-Ratio. Within the VC area of the refinement a minimum floor-area-ratio (FAR) of 0.75 is established. The proposed office building, which is approximately 9,000 square feet in size, therefore results in a FAR of 0.17 which falls below the minimum 0.75 FAR required within the VC area of the refinement plan. Because the proposed development does not meet the minimum required FAR, the applicant has requested a Class 2 Adjustment to this standard to reduce the minimum required FAR for the development. Analysis of the Class 2 Adjustment request and findings demonstrating conformance with the Class 2 Adjustment approval criteria are included under Section 8 of this report.

▪ **Setbacks:**

The setbacks for buildings, accessory structures, and parking areas within the VC area of the refinement plan are summarized in the table below:

VC Area Setbacks	
Building & Accessory Structures	
FMU Zone Boundary Setback	Min. 20 ft. <i>(applicable to all buildings and accessory structures)</i>
Strong & Lindburg Roads	Min. 10 ft. / Max. 20 ft.
	Min. 70% of lot frontage shall be occupied by buildings placed at the minimum setback line.
Front Abutting Street	Min. 10 ft. / Max. 20 ft.
Side Street	Min. 10 ft. / Max. 20 ft.
Side Interior	Min. 5 ft. <i>(applicable to multiple family)</i>
	Min. 8 ft. <i>(applicable any use abutting single family)</i>
	Min. 0 ft. <i>(applicable to all other)</i>
Rear Principal Building	Min. 5 ft.
Rear Other Buildings	Min. 2 ft. <i>(applicable to residential accessory buildings)</i>
	Min. 5 ft. <i>(applicable to all other accessory buildings)</i>
Parking Areas	
Front/Street Setback	Min. 20 ft.
Side Setback Adjacent to Residential	Min. 10 ft.
Side Setback Adjacent to Non-Residential	Min. 5 ft.

As shown on the site plan, the proposed development conforms to the minimum required setbacks within the refinement plan with the exception of the following areas:

- 1) Maximum building setback abutting Strong Road. As shown on the site plan, the southeast corner of the proposed office building exceeds the maximum allowed 20-foot setback abutting Strong Road SE.
- 2) Minimum lot frontage abutting Strong Road and Lindburg Road. As shown on the site plan, the proposed development does not include a minimum of 70 percent of the lot frontage of Lindburg Road SE and Strong Road SE with buildings placed at the minimum setback line.
- 3) Maximum side street building setback abutting private internal street. As shown on the site plan, the proposed office building exceeds the maximum allowed 20-foot setback from the proposed internal private street.
- 4) Minimum parking setback abutting street. As shown on the site plan, the proposed off-street parking area located to the north of the office building does not meet the minimum required 20-foot setback abutting Lindburg Road SE and the proposed private internal street.

Because portions of the proposed building and off-street parking area do not meet certain minimum required setbacks of the refinement plan, the applicant has requested a Class 2 Adjustment to these standards. Analysis of the Class 2 Adjustment request and findings demonstrating conformance with the Class 2 Adjustment approval criteria are included under Section 8 of this report.

▪ **Building Dimensions:**

Building dimension requirements applicable within the VC area of the refinement plan are summarized in the following table:

VC Area Building Dimension Standards	
Height	Max. 45 ft. <i>(applicable to buildings)</i>
	Max. 18 ft. <i>(applicable to accessory structures)</i>
Exterior Wall Length	Max. 100 ft. without 4 ft. change of plane <i>(applicable to mixed-use/commercial and multiple family)</i>

Height. As illustrated by the building elevation drawings (**Attachment D**), the height of the proposed two-story office building does not exceed the maximum allowed height of 45 feet. Similarly, there are no accessory structures included within the development that exceed the maximum allowed accessory structure height of 18 feet.

Exterior Wall Length. As shown on the site plan (**Attachment B**), the proposed office building does not include any exterior walls that exceed 100 feet in length. This refinement plan standard is therefore not applicable to the proposed development.

▪ **Parking & Loading:**

Parking and loading requirements applicable within the VC area of the refinement plan are summarized in the following table:

VC Area Parking & Loading Standards	
Parking Stalls	Min. 1 space per 500 square feet <i>(applicable to non-residential uses)</i>
Eligible On-Street Parking	50% of required parking may be located on street within 200 feet of the lot it serves <i>(applicable to non-residential uses)</i>
Surface Parking Coverage	Max. 40% of parcel
Bicycle Parking	Min. 1 space per 500 square feet <i>(applicable to non-residential uses)</i>

Parking Stalls & Eligible On-Street Parking. As identified in the above table, the refinement plan requires a minimum of one parking space per 500 square feet of

building area and a maximum of 50 percent of the required parking may be located within 200 feet of the lot it serves. The refinement plan also specifies that parking spaces provided within the development are required to meet the minimum stall dimensions established under the SRC.

In addition to the minimum off-street parking requirements included in the refinement plan, the City's off-street parking, loading, and driveways chapter (SRC Chapter 806) establishes a maximum limit on the number of parking space that may be provided based on the minimum number of spaces required. Pursuant to SRC Chapter 806, Table 806-2A, if the minimum number of spaces required equals 20 or less, the maximum allowed parking is 2.5 times the minimum number of spaces required. If the minimum number of spaces required is instead more than 20 spaces, the maximum allowed parking is 1.75 times the minimum number of spaces required.

Based on the above identified minimum and maximum off-street parking requirements, the proposed 9,000 square-foot office building requires the following vehicle parking:

Vehicle Parking Summary			
Use	Minimum Spaces Req.	Maximum Spaces	Spaces Provided
Office	18	45	17 (Off-Street)
			9 (On-Street)
Total:			26

As shown on the site plan, the proposed development includes a combination of both off-street and on-street parking. As summarized in the table above, the number of parking spaces provided for the proposed development conform to the minimum and maximum parking requirements of the refinement plan and the number of on-street parking spaces does not exceed 50 percent of the minimum required spaces.

Off-Street Parking Area Dimensions. SRC 806.035(e), Table 806-6, establishes minimum dimension requirements for off-street parking stalls and the drive aisles serving them. Based on the layout of the parking spaces within the development, the proposed parking stalls and access aisles must meet the following standards:

Minimum Parking Stall & Drive Aisle Dimensions		
Stall Type	Parking Stall Dimension	Drive Aisle Width ⁽¹⁾
90° Standard Stall	9 ft. x 19 ft.	24 ft.
90° Compact Stall ⁽²⁾	8 ft. x 15 ft.	22 ft.
	8 ft.- 6 in. x 15 ft.	
<u>Notes</u>		
(1) <u>Drive Aisle Width Serving Standard and Compact Stalls:</u> Pursuant to SRC 806, Table 806-6, when a parking lot drive aisle serves both		

standard and compact size parking stalls of 80 degrees or more, the drive aisle shall be a minimum of 24 feet.

- (2) Compact Stall Dimension Next to Wall or Post: Pursuant to SRC 806, Table 806-6, compact sized parking stalls next to a wall or post must be a minimum of 8-foot 6-inches in width.

As shown on the site plan for the proposed development, both standard size and compact size parking stalls are provided. The standard size off-street parking stalls conform to the minimum required 9-foot width and 19-foot depth and the compact size off-street parking stalls conform to the minimum required 8-foot width and 15-foot depth. Similarly, the proposed on-street parking spaces meet the minimum required size and dimensions standards included under Table 806-6 and all of the proposed parking spaces are served by drive aisles which exceed minimum required widths.

Surface Parking Coverage. The proposed development conforms to the maximum surface parking coverage standard of the refinement plan. The total area of the site proposed to be dedicated to surface parking is approximately 7,339 square feet. The resulting 14 percent surface parking coverage falls below the maximum 40 percent requirement and therefore conforms to this standard.

Off-Street Parking Area Turnaround. SRC 806.035(f)(2) requires that where a drive aisle in an off-street parking area terminates in a dead-end, a turnaround, as shown in Figure 806-9 and meeting the minimum dimensions set forth under Table 806-7, must be provide.

As shown on the site plan, the off-street parking area to the north of the proposed building terminates in a dead-end and a turnaround area is provided, but the turnaround is configured in a manner that does not conform to the configuration required under Figure 806-9 to ensure that a vehicle parking in the space located at the southwest corner of the parking lot has area to turnaround when exiting the space. In order to ensure that the vehicle maneuvering and turnaround requirements of SRC 806.035(f) are met, the following condition of approval shall apply:

Condition 1: The off-street parking area to the north of the proposed building shall be configured to meet the maneuvering and turnaround requirements of SRC 806.035(f)(2).

Bicycle Parking. Bicycle parking for the proposed development is required to be provided in the minimum amount of one space per 500 square feet of building area. Based on the proposed 9,000 square-foot building size, a minimum of 18 bicycle parking spaces are required for the proposed development.

As shown on the site plan, a total of 18 bike parking spaces are provided for the development. The proposed bike parking spaces are located within 50 feet of, and are clearly visible from, the front entrance of the building facing Lindburg Road SE and are 2 feet in width, 6 feet in length, and served by an access aisle

greater than 4 feet in width in conformance with the bike parking development standards included under SRC 806.060.

▪ **Driveway/Curb Cuts:**

Driveway and curb cut requirements applicable within the VC area of the refinement plan are summarized in the following table. The driveway and curb cut standards identified in the refinement plan apply only to private streets. Driveways and curb cuts on public streets are subject to the applicable provisions of the SRC.

Driveway & Curb Cut Standards	
Maximum Per Parcel	Max. 2
Access	Only from lesser class right-of-way (ROW)/Easement
Width (<i>Driveway serving 15+ stalls</i>)	Min. 10 ft. / Max. 18 ft.
Width (<i>Driveway serving 4-14 stalls</i>)	Min. 10 ft. / Max. 14 ft.
Width (<i>Driveway serving 1-4 stalls</i>)	Min. 12 ft.
Curb Cuts	Max. 2 per parcel

Vehicle access to the proposed development is provided by a private street, Village Center Loop SE, which extends through the property from Lindburg Road SE on the east to Strong Road SE to the south. As shown on the site plan, the proposed development is served by one driveway approach onto Village Center Loop which serves the 17 parking spaces in the parking lot to the north of the proposed building. Because the proposed development includes only one driveway approach that is 18 feet in width and which provides access to the Village Center Loop SE, the street with the lowest street classification, the proposed development meets the driveway and curb cut standards of the refinement plan.

SRC Chapter 530 (Fairview Mixed-Use Zone)

The majority of the standards applicable to the proposed development are contained in the *Fairview Refinement Plan II* refinement plan. The FMU zone, however, includes the following additional standards which apply to development generally with the FMU zone.

SRC 530.045 (General Development Standards)

- Nonresidential development in MI area. Except for activities falling under basic education, no building used exclusively for a nonresidential use within the MI area shall have a building footprint greater than 6,000 square feet. Activities falling under basic education located within the MI area may have a building footprint greater than 6000 square feet.

The subject property is located within the VC (Village Center), not the MI (Mixed-Intensity), overlay area of the refinement plan. This FMU zone standard is therefore not applicable to the proposed development.

- Open space. A minimum of 20 acres of land within the FMU zone shall be reserved as natural open space.

Natural open space areas are identified as part of the refinement plan approval process. The subject property is not identified in the refinement plan as being part of a natural open space area. This FMU zone standard is therefore not applicable to the proposed development.

- Maximum number of dwelling units. The maximum number of dwelling units permitted in the FMU zone shall be 2000.

The proposed development is for an office building and does not include any residential dwelling units. The number of dwelling units within the FMU zone has not yet reached the maximum limit of 2,000.

- FMU zone boundary setback. All buildings and accessory structures within the FMU zone shall be set back a minimum of 20 feet from the FMU zone boundary.

The subject property is not located on the perimeter of the former Fairview Training Center site adjacent to the FMU zone boundary. This FMU zone standard is therefore not applicable to the proposed development.

SRC 530.060 (Historic Preservation)

SRC 530.060 requires that any structure existing on December 24, 2003, identified for demolition shall, prior to issuance of a demolition permit, be documented according to the survey and inventory practices set forth by the Oregon State Historical Preservation Office.

There are no longer any existing buildings on the subject property. Buildings that previously existed were documented per the requirements of SRC 530.060 prior to their demolition. This FMU zone standard is therefore no longer applicable to the proposed development.

SRC 530.065 (Natural Resource Guidelines)

The FMU Zone recognizes the importance of the presence of natural resources on the site and how those resources help to define the special character of the property. As such, SRC 530.065 establishes natural resource guidelines to address their preservation. The Fairview Plan, any subsequent refinement plan, and any development within the FMU Zone shall identify how existing natural resources will be protected and how natural hazards will be mitigated through compliance with the following SRC chapters:

- SRC Chapter 808 (Preservation of Trees & Vegetation):

Pursuant to the FMU zone, specifically SRC 530.065, and the provisions of the Fairview Refinement Plan II refinement plan included under Section 10 (Tree Inventory and Preservation Plan), compliance with the City's tree ordinance is the means by which tree preservation and removal is regulated within the refinement plan.

The City's tree preservation ordinance (SRC Chapter 808) protects:

- 1) Heritage Trees;
- 2) Significant Trees (including Oregon White Oaks with diameter-at-breast-height (*dbh*) of 20 inches or greater and any other tree with a *dbh* of 30 inches or greater, with the exception of tree of heaven, empress tree, black cottonwood, and black locust);
- 3) Trees and native vegetation in riparian corridors; and
- 4) Trees on lots or parcels 20,000 square feet or greater.

The tree preservation ordinance defines "tree" as, "any living woody plant that grows to 15 feet or more in height, typically with one main stem called a trunk, which is 10 inches or more *dbh*, and possesses an upright arrangement of branches and leaves."

Under the City's tree preservation ordinance, pursuant to SRC 808.035(a), tree conservation plans are required in conjunction with development proposals involving the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, if the development proposal will result in the removal of trees.

Tree conservation plans are required to preserve all heritage trees, significant trees, trees and native vegetation within riparian corridors, and a minimum of 25 percent of the remaining trees on the property. If less than 25 percent of the existing trees on the property are proposed for preservation, the applicant must show that only those trees reasonably necessary to accommodate the development are designated for removal. If significant trees and trees within a riparian corridor are proposed for removal, the applicant must show that there are no reasonable design alternatives to enable preservation of those trees.

Because the proposal is for an office development, a tree conservation plan is not required, but the development must maintain compliance with the provisions of SRC Chapter 808. Because there are no existing trees on the subject property the tree preservation requirements of SRC Chapter 808 are not applicable to the proposed development.

▪ SRC Chapter 809 (Wetlands):

Grading and construction activities within wetlands are regulated by the Oregon Department of State Lands (DSL) and US Army Corps of Engineers. State and Federal wetland laws are also administered by the DSL and Army Corps, and

potential impacts to jurisdictional wetlands are addressed through application and enforcement of appropriate mitigation measures.

According to the Salem-Keizer Local Wetland Inventory (LWI), the subject property does not contain any wetland areas or hydric soils. The requirements of SRC Chapter 809 are therefore not applicable to the proposed development.

▪ **SRC Chapter 810 (Landslide Hazards):**

The City's landslide hazard ordinance (SRC Chapter 810) establishes standards and requirements for the development of land within areas of identified landslide hazard susceptibility.

According to the City's adopted landslide hazard susceptibility maps, the subject property does not contain any areas of mapped landslide hazard susceptibility. Therefore, a geologic assessment or geotechnical report is not required in conjunction with the proposed development.

SRC 800.055 (Solid Waste Service Areas)

SRC 800.055 establishes design standards that apply to all new solid waste, recycling, and compostable service areas, where use of a solid waste, recycling, and compostable receptacle of 1 cubic yard or larger is proposed.

A solid waste service area is defined under SRC 800.010(e) as, "An area designed and established for the purpose of satisfying the local collection franchisee service requirements for servicing receptacles, drop boxes, and compactors singularly or collectively."

As shown on the site plan for the proposed development, an enclosure for trash bins is provided to the northwest of the proposed off-street parking area adjacent to Village Center Loop SE. In the written materials provided from the applicant it is indicated that the trash enclosure will accommodate bins that are less than one cubic yard in size and will be serviced similar to residential cans. As such, the applicant indicates that the solid waste service area standards of SRC 800.055 are therefore not applicable to the proposed development.

Because the solid waste service area standards of SRC 800.055 apply only to receptacles one cubic yard or larger in size, the standards are not applicable to the proposed bins included within the development.

SRC 220.005(f)(3)(B): The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately.

Finding: The subject property is a corner lot with frontage on both Lindburg Road SE and Strong Road SE. Vehicular access to the development is proposed to be provided by an internal private street, Village Center Loop SE, which extends through the subject property from Lindburg Road SE on the east to Strong Road SE to the south.

Both Lindburg Road SE and Strong Road SE are designated as a collector streets under the City's Transportation System Plan (TSP). The standard for these streets is established in the refinement plan and requires a 28-foot-wide to 36-foot-wide improvement within a 60-foot-wide right-of-way. Comments from the Public Works Department (**Attachment G**) indicate that both Lindburg Road and Strong Road have approximate 35-foot-wide improvements with 60-foot-wide rights-of-ways. Lindburg Road and Strong Road are fully developed streets and meet the right-of-way width and pavement width standards required under the refinement plan and the TSP.

Comments from The Salem Area Mass Transit District (Cherriots) were provided that are included as **Attachment H**. The comments received indicate that a transit stop has been identified as being needed in connection with the proposed development on Strong Road SE. Cherriots therefore requests that a transit stop conforming to the applicable standards of the Salem Area Mass Transit District be constructed as part of the development. SRC 803.035(r) requires that transit stops conforming to the applicable standards of the Salem Area Mass Transit District shall be constructed when a transit stop is identified as being needed by the Transit District in connection with a proposed development. In order to ensure the proposed development conforms to the requirements of SRC 803.035(r), the following condition of approval shall apply:

Condition 2: The applicant shall coordinate with Cherriots to locate and construct a transit stop conforming to applicable Salem Area Mass Transit District Standards and the Public Works Design Standards (PWDS) on Strong Road SE.

Village Center Loop SE is a private street required under the refinement plan. The standard for this street is established in the refinement plan and is further modified by the approved subdivision which created the subject property and the lots in the surrounding area, the Legacy Heights Subdivision (Case No. SUB-FRPA20-03). The approved standards require Village Center Loop to be improved to a width of 20 feet to 58 feet within a private street right-of-way width of 44 feet to 87 feet depending on whether parking is included on the street and the angle of the parking stalls provided.

As shown on the site plan, the proposed width of Village Center Loop varies as it extends through the property and there are portions of street that include on-street parking and other portions that do not. Due to the location of required stormwater treatment facilities and the configuration of the site and the location of the street in relation to the proposed office building and parking lot, the design of the Village Center Loop deviates from the approved street cross section in certain areas. As such, the applicant has requested an alternative street standard in conjunction with the proposal to allow a variable street cross section width; a variable landscape strip width of less than 7 feet and more than 7 feet in certain locations; a variable sidewalk location along the street; and a diagonal parking stall depth that exceeds 17 feet.

SRC 806.035 authorizes approval of an alternative street standard where:

- 1) Existing development or physical constraints make compliance with the standards set forth in SRC Chapter 803 (Streets and Right-of-Way Improvements) impracticable;
- 2) The development site is served by fully developed streets that met the standards in effect at the time the streets were originally constructed; or
- 3) Topography or other conditions make construction that conforms to the standards impossible or undesirable.

In the case of the proposed development, the topography and configuration of the site, the required location of stormwater treatment facilities, and location of existing driveways onto Strong Road SE and Lindburg Road limit where Village Center Loop may be constructed through the site and therefore result in physical constraints that make compliance with the applicable street standards impracticable. As shown on the site plan, the proposed design of Village Center Loop includes required sidewalks (*including a required 10-foot-wide multi-use path on one side*), landscape strips, an improvement width sufficient to ensure the safe movement of vehicles, and diagonal parking stalls of a sufficient depth to meet the requirements of SRC Chapter 806. Because the design of the street includes all of the required elements to enable it to function as envisioned under the refinement plan, and due to the physical constraints associated with the site that make full compliance with the refinement plan standards impracticable, the requested alternative street standard is approved.

In order to ensure that Village Center Loop meets the applicable requirements of the refinement plan and SRC Chapter 803, the following conditions of approval shall apply:

Condition 3: Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE.

Condition 4: Dedicate a public access easement for Village Center Loop SE within the development.

Condition 1 of the subdivision approval which created the subject property (Case No. SUB-FRPA20-03) requires that where a lot or lots within a phase of the subdivision includes private facilities (*such as a private street*) that will not be under common ownership, a maintenance agreement is required providing for the perpetual maintenance and operation of such facilities. Because Village Center Loop SE is a private street that will not be under the common ownership and maintenance of a Homeowners' Association (HOA), the following condition of approval shall apply to ensure the perpetual operation and maintenance of Village Center Loop as required under Condition 1 of Phased Subdivision / Fairview Refinement Plan Minor Amendment Case No. SUB-FRPA20-03:

Condition 5: A maintenance agreement for the perpetual operation and maintenance of Village Center Loop SE shall be provided.

The proposal, as conditioned, ensures that the streets serving the development conform to the City's TSP, SRC Chapter 803, and the applicable provisions of the refinement plan; thereby ensuring that the transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the development. This approval criterion is met.

SRC 220.005(f)(3)(C): Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians.

Finding: Vehicle access to the proposed development is provided by a private street which extends through the site between Lindburg Road SE and Strong Road SE, and a driveway that connects to the private street. The private street provides safe access through the site and eliminates the need for separate driveways serving the development onto Lindburg Road SE and Strong Road SE, which are classified as higher traffic volume collector streets.

Pedestrian access to and throughout the development is provided by the sidewalks on the public streets which abut the site and a 10-foot-wide multi-use path and sidewalk which are associated with Village Center Loop SE. These features provide for safe and convenient pedestrian access and ensure that the proposed parking and driveways within the development minimize vehicle and pedestrian conflicts. This approval criterion is met.

SRC 220.005(f)(3)(D): The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Finding: The Public Works Department has reviewed the applicant's preliminary utility plan for the site and indicates that water, sewer, and stormwater infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development.

In regard to stormwater facilities, the Public Works Department indicates they have reviewed the stormwater report submitted by the applicant in conjunction with the proposed development and determined that it does not demonstrate that the design provides treatment and flow control for the developed site to meet the requirements of the Public Works Design Standards (PWDS) and SRC 71. In order to ensure the provision of adequate stormwater infrastructure to serve the proposed development, the following condition of approval shall apply:

Condition 6: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and the Public Works Design Standards (PWDS).

The proposed development, as conditioned, will be adequately served by City utilities. This approval criterion is met.

8. Analysis of Class 2 Adjustment Approval Criteria

Salem Revised Code (SRC) 250.005(d)(2) sets forth the following criteria that must be met before approval can be granted to an application for a Class 2 Adjustment. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings of fact upon which the decision is based. Lack of compliance with the following criteria is grounds for denial or for the issuance of conditions of approval to satisfy the criteria.

SRC 250.005(d)(2)(A): The purpose underlying the specific development standard proposed for adjustment is:

- (i) Clearly inapplicable to the proposed development; or***
- (ii) Equally or better met by the proposed development.***

Finding: The applicant has requested four Class 2 Adjustments to development standards of the Fairview Refinement Plan II refinement plan. The adjustments include:

- a) Allowing the southeast corner of the building to exceed the maximum allowed 20-foot setback abutting Strong Road SE and allow less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line;
- b) Allowing the proposed building to exceed the maximum 20-foot side street setback abutting the private internal street;
- c) Allowing the off-street parking area to the north of the proposed building to be setback less than the minimum required 20-foot setback abutting Lindburg Road SE and the private internal street; and
- d) Allowing the minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

Maximum Building Setback Abutting Strong Road & Minimum Building Frontage Requirements:

The applicant has requested a Class 2 Adjustment to allow the southeast corner of the proposed building to exceed the maximum allowed 20-foot setback abutting Strong Road SE and to allow less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line.

The written statement provided by the applicant (**Attachment E**) indicates, in summary, that due to the existing lot and its physical constraints the required 10-foot to 20-foot building setback has been provided along the east façade of the building but it is not possible on the northern, southern, and western facades. The applicant explains that to provide a building that would comply with this standard is not possible on the north because the site is bisected with a private drive; due to the angle of the private drive and location the site, a building that would comply with the standard on the west would be an irregular shaped building with non-traditional construction; and the utility easement near the southeast corner of the building

prevents the building from complying with the maximum setback abutting Strong Road in that area.

In regard to the minimum required lot frontage abutting Lindburg Road SE and Strong Road SE, the applicant indicates that due to the extensive street frontage of this corner lot, a building what would comply with the frontage requirement of 70 percent along Strong Road and Lindburg Road is unfeasible. The applicant explains that the building would need to be disproportionately long and narrow to meet the standard and it would create additional site disturbance than necessary. The applicant indicates that the site is also bisected along both frontages with a private street, making frontage achievable. The applicant explains that they have provided 37 percent frontage along Strong Road SE and 25 percent frontage along Lindburg Road SE. In addition, the applicant indicates that they are proposing a building that has a first floor building height of at least 14 feet, large ground floor windows facing Strong Road and Lindburg Road totaling 41 percent of the façade, a primary building entrance adjacent to the intersection of Strong Road and Lindburg Road, large overhanging soffits around the first floor of the building, and a canopy over the building entry adjacent to the surface parking lot and ADA stalls. The applicant indicates that these building elements offset the smaller sized building and reduced lot frontage by ensuring that those portions of the building which do occupy the required setbacks along the public street are designed to visually reinforce and support an active and inviting pedestrian environment at the intersection of Strong Road and Lindburg Road, which is the key most prominent and significant street intersection within the Fairview Training Center site located in the core of the Village Center area.

As indicated in the applicant's written statement, Strong Road SE and Lindburg Road SE are prominent streets within the former Fairview Training Center Site that provide east-west and north-south street connectivity. In order to promote an urban and pedestrian-friendly environment along these streets the Fairview Refinement Plan II refinement plan establishes a maximum building setback of 20 feet from these streets while also requiring that a minimum of 70 percent of the street frontage of a lot abutting these streets must be occupied by buildings placed at the minimum setback line.

As shown on the site plan (**Attachment B**), a portion of the southeast corner of the proposed building adjacent to the intersection of Strong Road and Lindburg Road slightly exceeds the maximum allowed 20-foot setback abutting Strong Road. Rather than being setback no further than 20 feet from the street, this portion of the building is setback approximately 21.5 feet. A minimum of 70 percent of the lot frontages of the site abutting Strong and Lindburg Road are also not occupied by buildings placed at the minimum setback line.

As previously identified, the underlying purpose of the building setback and corresponding lot frontage standard is to promote a welcoming and pedestrian-friendly environment where buildings are located in close proximity to the street and the presence of vehicles are minimized.

In the case of the proposed development, the subject property is relatively unique because it is not only a corner lot with frontage on two streets, but there is also an additional private street which bisects the property resulting in additional street frontage. As shown on the site plan, the proposed building has been sited in a manner to help reinforce a pedestrian-friendly environment abutting Strong Road and Lindburg Road by locating the building, rather than parking, at the southeast corner of the lot near where the two streets intersect, which also helps to make the intersection more visually prominent.

In addition to siting the building adjacent to the intersection of Strong and Lindburg Road, the proposed building incorporates additional design details, as identified in the applicant's written statement, including an increased ground floor building height, roof overhangs for weather protection, a primary building entrance near the intersection of Strong and Lindburg Roads, and an increased amount of windows facing these streets to promote a pedestrian-friendly environment in keeping with the underlying purpose of this building setback and lot frontage standard.

Due to the physical constraints associated with the site resulting from it being bisected by a private street, and through the building's siting adjacent to the intersection of Strong and Lindburg Roads together with the additional design elements incorporated into the building to give it a more urban and pedestrian-friendly appearance, the proposed development takes measures to equally meet the underlying purpose of the standard proposed for adjustment.

As shown on the building elevations, windows are provided on the ground floor facades of the building facing Lindburg Road and Strong Road. As indicated in the applicant's written statement, these windows equal approximately 41 percent of the façade. The 41 percent ground floor windows provided, however, falls short of the minimum 65 percent typically required in those zones within the City where buildings are required to be designed to include ground floor windows to promote ground floor building transparency and a welcoming and inviting pedestrian environment (e.g. CB (Central Business District Zone), MU-I (Mixed-Use-I Zone), and MU-II (Mixed-Use-II Zone)). In order to ensure that the proposed building is designed in a manner to otherwise equally meet the underlying purpose of the building setback and frontage standard, the following condition of approval shall apply to require additional windows on building facades facing Lindburg Road and Strong Road:

Condition 7: Additional windows shall be incorporated into the ground floor facades of the building facing Lindburg Road SE and Strong Road SE. The additional windows shall be provided by extending the specific windows identified in Attachment I to this decision to the floor of the building.

The proposed development, as conditioned, ensures that although the proposed building does not occupy the amount of street frontage adjacent to Lindburg Road and Strong Road that would otherwise be required, the design and appearance of the building will be such that it promotes and active and inviting pedestrian environment along these streets in a manner that equally meets the underlying purpose of the standard. This approval criterion is met.

Maximum Side Street Setback:

The applicant has requested a Class 2 Adjustment to allow the proposed building to exceed the maximum 20-foot side street setback abutting Village Center Loop SE.

The written statement provided by the applicant (**Attachment E**) indicates, in summary, that due to the existing lot and its physical constraints the required 10-foot to 20-foot building setback has been provided along the east façade of the building but it is not possible on the northern facade. The applicant explains that to provide a building that would comply with this standard is not possible on the north because the site is bisected with a private drive.

Staff concurs with the findings included in the applicant's written statement. The subject property is unique in that it has street frontage on four sides (*two frontages that abut public streets and two frontages that abut a private street*). As shown on the site plan, the proposed building is sited adjacent to the intersection of Strong Road and Linburg Road in order to respond to the prominence of this intersection and promote an active and pedestrian-friendly environment. In order to meet the maximum 20-foot building setback abutting Village Center Loop SE to the north, the building would have to be stretched out and designed to be long and narrow which would ultimately result in a less functional building. The underlying purpose of this standard is to ensure that buildings are located in relatively close proximity to streets in order to promote an active and inviting pedestrian environment along the street. Due to the number of street frontages adjacent to the property, and in order to develop a functional building on the site, the building has been sited to prioritize placement adjacent to Strong Road and Linburg Road. The building's placement adjacent to this street intersection ensures that a pedestrian-friendly environment is created along Strong Road and Lindburg Road in a manner that meets the underlying purpose of the standard. This approval criterion is met.

Minimum Parking Setback Abutting Street:

The applicant has requested a Class 2 Adjustment to allow the off-street parking area to the north of the proposed building to be setback less than the minimum required 20-foot setback abutting Lindburg Road SE and the private internal street.

The written statement provided by the applicant (**Attachment E**) indicates, in summary, that due to the angled private drive, the 20-foot minimum required setback from parking lots abutting streets cannot be met. The applicant explains that there is not adequate room on the site to accommodate this setback along the private drive nor Lindburg Road and there is a pinch point due to the angled private drive that the parking cannot accommodate. The applicant indicates that they have setback the surface parking lot 20'-0" from the private road itself; however, the constraints of the site limit us to not be able to achieve 20'-0" from the private street right-of-way, which is inclusive of the entire street section of landscape strips and sidewalks. The applicant explains that they believe the proposed development meets the intent of the setback requirements because the areas between the road and parking area are

heavily landscaped and will provide visual screening of the parking lot, which will offset the reduced setback depth.

Staff concurs with the findings included in the applicant's written statement. The underlying purpose of this standard is to ensure that parking areas are adequately separated from the street in order to promote a pedestrian-friendly environment. Although the proposed parking area will be setback less than the minimum required 20 feet from Lindburg Road SE to the east and Village Center Loop SE to the north and west, the parking area will be surrounded by landscaping that will provide increased visual screening of the parking lot to offset the reduced setback distance; thereby equally meeting the underlying purpose of the standard. This approval criterion is met.

Minimum Required Floor-Area-Ratio:

The applicant has requested a Class 2 Adjustment to allow the minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

The written statement provided by the applicant (**Attachment E**), indicates, in summary, that due to the large area of the site, a building that would meet the standard floor area ratio requirement of 0.75 would be enormous. This standard would require a 40,000 square foot building on the site. Also, because of the private drive, landscaping and sidewalk requirements and GSI detention, there is no room left on the site for a building this size. The applicant indicates that they have provided a 9,000 square foot building in a 52,093 square foot lot, resulting in a FAR of 0.17, and are deficient to the standard by 0.58. The applicant explains that they believe a building meeting this standard would be infeasibly large and would result in failing all other required standards. The applicant indicates that they are proposing a building that has a first floor building height of at least 14 feet; large ground floor windows are being provided facing Strong Rd and Lindburg Road, totaling 41% of the façade; a primary building entrance adjacent to the intersection of Strong Road and Lindburg Road is being provided; large overhanging soffits around the first floor building area are included in the development; and a canopy over the entry adjacent the surface parking lot and ADA stalls have been provided. The applicant indicates that these building elements offset the smaller sized building by ensuring that those portions of the building which do occupy the required setbacks along the public street are designed to visually reinforce and support an active and inviting pedestrian environment at the intersection of Strong Road and Lindburg Road, which is the key most prominent and significant street intersection within the Fairview Training Site located in the core of the Village Center area. The applicant explains that the proposed development therefore equally or better meets the intent of this standard rather than providing a building that completely covers the site.

Staff concurs with the findings included in the applicant's written statement. In the case of the proposed development, the subject property is unique because it's not only a corner lot with frontage on two streets, but there is also a private street which bisects the property. As indicated in the applicant's written statement, an approximate 40,000 square-foot building would be required on the site in order to

conform to the minimum required 0.75 FAR of the refinement plan. The minimum required size of the building exceeds that which is feasibly needed by the applicant. In order to help meet the underlying purpose of the minimum FAR standard, the applicant has added a second story to the northern portion of the building to provide for a greater development intensity for the site.

Due to the physical constraints associated with the site resulting from it being bisected by a private street, and through the building's siting adjacent to the intersection of Strong and Lindburg Roads together with the additional design elements incorporated into the building to give it a more urban and pedestrian-friendly appearance, the proposed development includes measures to equally meet the underlying purpose of the standard proposed for adjustment. This approval criterion is met.

SRC 250.005(d)(2)(B): If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.

Finding: The subject property is zoned FMU. Pursuant to SRC 110.025(a), Table 110-1, the FMU zone is a mixed-use zone rather than a residential zone. Because the subject property is not located within a residential zone, this approval criterion is not applicable to the proposed development.

SRC 250.005(d)(2)(C): If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.

Finding: A total of four Class 2 Adjustments have been requested in conjunction with the proposed development. The written statement provided by the applicant (**Attachment E**) indicates that the cumulative effect of all of the requested adjustments will result in a project this is still consistent with the intent of the FMU zone and that the requested adjustments will not detract from the intent and overall purpose of the zone.

Staff concurs with the findings included in the applicant's written statement. Pursuant to SRC 530.001, the overall purpose of the FMU zone is to encourage innovative planning resulting in mixed-use development, improved protection of open spaces and natural features, and greater housing and transportation options. The provisions of the Fairview Refinement Plan II refinement plan implement this overall purpose.

The four Class 2 Adjustments requested in conjunction with the proposed development will not cumulatively result in a project that is inconsistent with the overall purposes of the FMU zone, the Fairview Plan, or the Fairview Refinement Plan II refinement plan. The proposed development instead will introduce commercial use that has been intended for this area of for the former Fairview Training Center site and envisioned in the refinement plan. The proposed development provides for pedestrian connectivity within and through the site and includes safe and convenient vehicular access.

The four requested adjustments are limited to the minimum necessary to accommodate the proposed development without compromising the purpose of the zone and refinement plan. This approval criterion is met.

Any future development, beyond what is shown in the proposed plans, shall conform to all applicable development standards of the UDC, unless adjusted through a future land use action. As such, the following condition of approval shall apply:

Condition 8: The adjusted development standards shall only apply to the specific development proposal shown in the approved site plan. Any future development, beyond what is shown in the approved site plan, shall conform to all applicable development standards of the Unified Development Code and the Fairview Refinement Plan II refinement plan, unless adjusted through a future land use action.

9. Conclusion

Based upon review of SRC Chapters 220 and 250, the applicable standards of the Salem Revised Code and the Fairview Refinement Plan II refinement plan, the findings contained herein, and due consideration of comments received, the application complies with the requirements for an affirmative decision.

IT IS HEREBY ORDERED

Class 3 Site Plan Review and Class 2 Adjustment Case No. SPR-ADJ23-17 is hereby **APPROVED** subject to SRC Chapters 220 and 250, the applicable standards of the Salem Revised Code and the Fairview Refinement Plan II refinement plan, conformance with the approved site plan included as Attachment B, and the following conditions of approval:

- Condition 1:** The off-street parking area to the north of the proposed building shall be configured to meet the maneuvering and turnaround requirements of SRC 806.035(f)(2).
- Condition 2:** The applicant shall coordinate with Cherriots to locate and construct a transit stop conforming to applicable Salem Area Mass Transit District Standards and the Public Works Design Standards (PWDS) on Strong Road SE.
- Condition 3:** Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE.
- Condition 4:** Dedicate a public access easement for Village Center Loop SE within the development.
- Condition 5:** A maintenance agreement for the perpetual operation and maintenance of Village Center Loop SE shall be provided.

Condition 6: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and the Public Works Design Standards (PWDS).

Condition 7: Additional windows shall be incorporated into the ground floor facades of the building facing Lindburg Road SE and Strong Road SE. The additional windows shall be provided by extending the specific windows identified in Attachment I to this decision to the floor of the building.

Condition 8: The adjusted development standards shall only apply to the specific development proposal shown in the approved site plan. Any future development, beyond what is shown in the approved site plan, shall conform to all applicable development standards of the Unified Development Code and the Fairview Refinement Plan II refinement plan, unless adjusted through a future land use action.

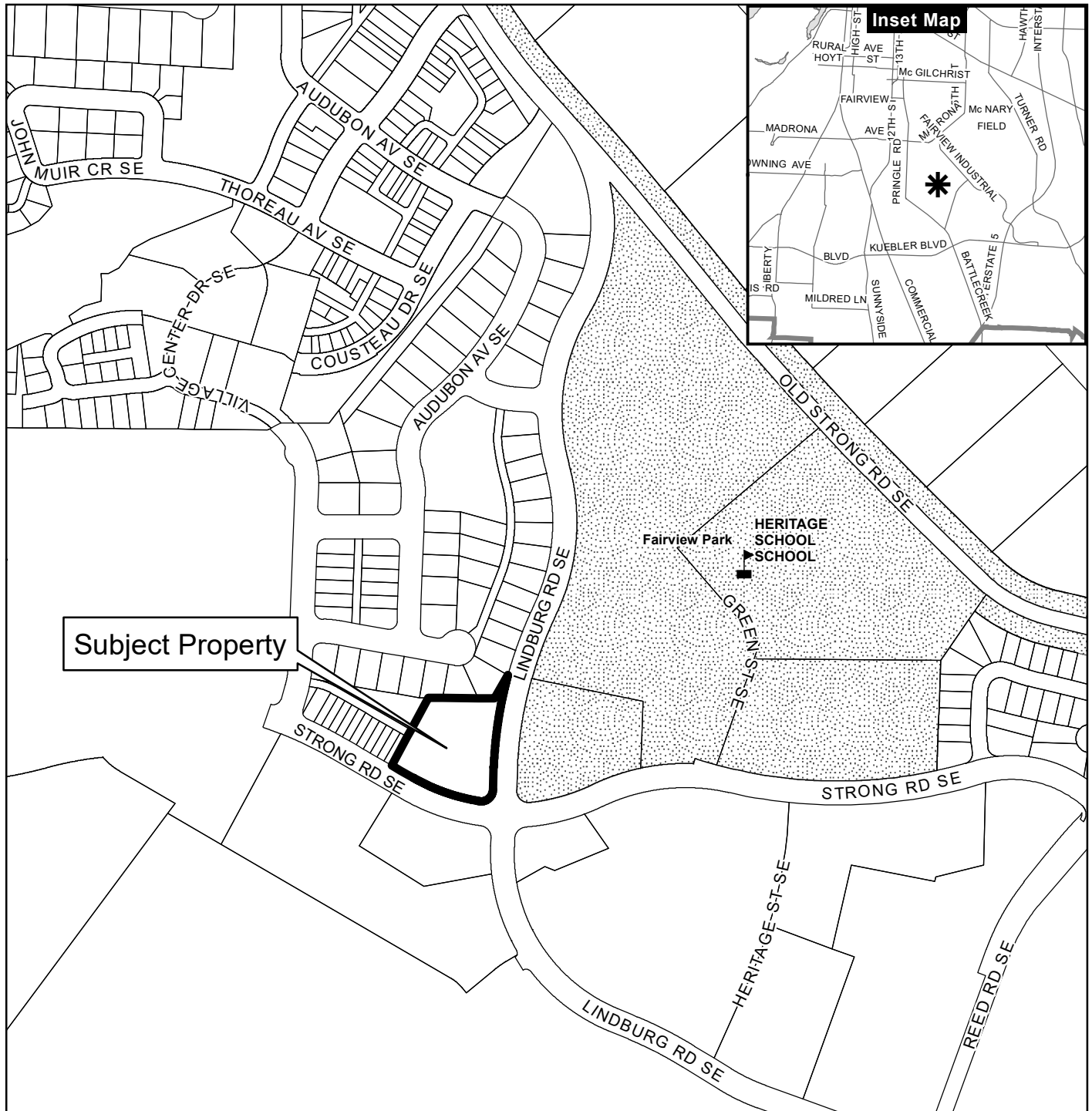


Bryce Bishop, Planner III, on behalf of
Lisa Anderson-Ogilvie, AICP
Planning Administrator

- Attachments:
- A. Vicinity Map
 - B. Site Plan
 - C. Landscape Plan
 - D. Building Elevations
 - E. Applicant's Written Statement
 - F. Public Comments
 - G. City of Salem Public Works Department Comments
 - H. Salem Area Mass Transit District (Cherriots) Comments
 - I. Required Modifications to Proposed Windows

<http://www.cityofsalem.net/planning>

Vicinity Map 3985 Lindburg Road SE



Legend

- Taxlots
- Urban Growth Boundary
- City Limits
- Outside Salem City Limits
- Historic District
- Schools
- Parks

0 100 200 400 Feet



CITY OF Salem
AT YOUR SERVICE
Community Development Dept.

This product is provided as is, without warranty. In no event is the City of Salem liable for damages from the use of this product. This product is subject to license and copyright limitations and further distribution or resale is prohibited.

Attachment B

- GENERAL NOTES:
- 1. GENERAL NOTES APPLY TO
 - 2. DIMENSIONS ARE TO EXIST EXISTING CONCRETE UNLESS
 - 3. DRAWINGS ARE DIAGRAMMATIC ONLY AND SHOULD NOT BE SCALED. NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES OR QUESTIONABLE DIMENSIONS PRIOR TO PROCEEDING WITH AREA OF QUESTIONABLE WORK.
 - 4. IN CASE OF ANY CONFLICTS IN THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, THE CONTRACTOR IS REQUIRED TO INCLUDE THE BETTER QUALITY AND LARGER QUANTITY OF THE WORK.
 - 5. CONTRACTOR AND BIDDERS SHALL USE COMPLETE SETS OF CONTRACT DOCUMENTS; NEITHER THE OWNER NOR ARCHITECT ASSUMES RESPONSIBILITY FOR ERRORS OR MISINTERPRETATIONS RESULTING FROM THE USE OF INCOMPLETE SETS OF CONTRACT DOCUMENTS.
 - 6. COORDINATE AND PROVIDE WOOD BACKING FOR ALL OFCI ITEMS

- REFERENCE NOTES:
- 1 NEW PAINTED ARROW MARKINGS
 - 2 NEW CONCRETE WHEELSTOP
 - 3 NEW 4" PAINTED PARKING STRIPE
 - 4 NEW 4" PAINTED PARKING SAFETY STRIPES AT 2'-0" o.c.
 - 5 NEW HANDICAP ACCESSIBLE PARKING SIGN
 - 6 NEW PAINTED HANDICAP SYMBOL
 - 7 NEW CONCRETE PAD FOR TRASH BINS, PROVIDE A 6'-0" HIGH CEDAR FENCE WITH GATE
 - 8 NEW DEDICATED BICYCLE PARKING AREA
 - 9 EXISTING RETAINING WALL TO REMAIN, PROTECT AT ALL TIMES
 - 10 NEW CONCRETE STAIR, REFER TO CIVIL DRAWINGS
 - 11 NEW GALVANIZED METAL HANDRAIL
 - 12 NEW GALVANIZED METAL GUARDRAIL
 - 13 NEW BIKE RACK
 - 14 EXISTING CONCRETE SIDEWALK TO REMAIN
 - 15 EXISTING ASPHALT TO REMAIN
 - 16 EXISTING GUTTER TO REMAIN
 - 17 NEW OFCI BENCH
 - 18 NEW RETAINING WALL, REFER TO CIVIL DRAWINGS
 - 19 NEW OFCI WATER FEATURE AND BOLDER, REFER TO LANDSCAPE DRAWINGS
 - 20 NEW IRRIGATION DCA, REFER TO LANDSCAPE DRAWINGS
 - 21 EXISTING ASPHALT TO BE SAWCUT AND REMOVED FOR UTILITY CONNECTION AND NEW CURB CONSTRUCTION. INFILL WITH NEW ASPHALT TO MATCH EXISTING, REFER TO CIVIL DRAWINGS.
 - 22 DEPRESSED CURB CUT, TAPERED, REFER TO CIVIL DRAWINGS
 - 23 NEW 12" TALL PAINTED LETTERING, COMPACT AT SIM CONDITION, NO PARKING
 - 24 NEW OFCI MONUMENT SIGN
 - 25 NEW LOCATION FOR BUS TRANSIT STOP, AT SIM CONDITION PROVIDE A 6'-0" WIDE BRIDGE OVER SWALE OF PERVIOUS MATERIAL

- SITE LEGEND
- NEW CONCRETE PAVEMENT, REFER TO CIVIL DRAWINGS
 - NEW A. C. PAVEMENT, REFER TO CIVIL DRAWINGS
 - NEW LANDSCAPED AREA, REFER TO LANDSCAPE DRAWINGS
 - NEW GSI DETENTION AREA, REFER TO CIVIL DRAWINGS
 - EXISTING CURB TO REMAIN, REFER TO CIVIL DRAWINGS
 - NEW CONCRETE CURB, REFER TO CIVIL DRAWINGS
 - PROPERTY BOUNDARY
 - NEW CONCRETE PAVEMENT JOINT
 - NEW CLEAN OUT, (E) DESIGNATES EXISTING TO REMAIN, REFER TO CIVIL DRAWINGS
 - NEW MAN HOLE, (E) DESIGNATES EXISTING TO REMAIN, REFER TO CIVIL DRAWINGS
 - NEW DOWN SPOUT, REFER TO CIVIL DRAWINGS
 - NEW CATCH BASIN, (E) DESIGNATES EXISTING TO REMAIN, (D) DENOTES TO BE REMOVED, REFER TO CIVIL DRAWINGS
 - NEW WATER METER, (E) DESIGNATES EXISTING TO REMAIN, REFER TO CIVIL DRAWINGS
 - NEW FIRE HYDRANT (E) DESIGNATES EXISTING TO REMAIN, REFER TO CIVIL DRAWINGS
 - EXISTING POWER POLE TO REMAIN
 - EXISTING ELECTRICAL BOX TO REMAIN
 - EXISTING POWER JUNCTION BOX TO REMAIN
 - EXISTING LIGHT POLE TO REMAIN
 - NEW SIGN, (E) DENOTES EXISTING TO REMAIN
- CURB RADII:
- 1 2'-0"
 - 2 3'-0"
 - 3 4'-0"
 - 4 5'-0"
 - 5 15'-0"
 - 6 25'-0"

In the event conflicts are discovered between the original signed and sealed documents prepared by the Architects and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

JOB NO. 2022.0003
DATE MAY 19, 2023
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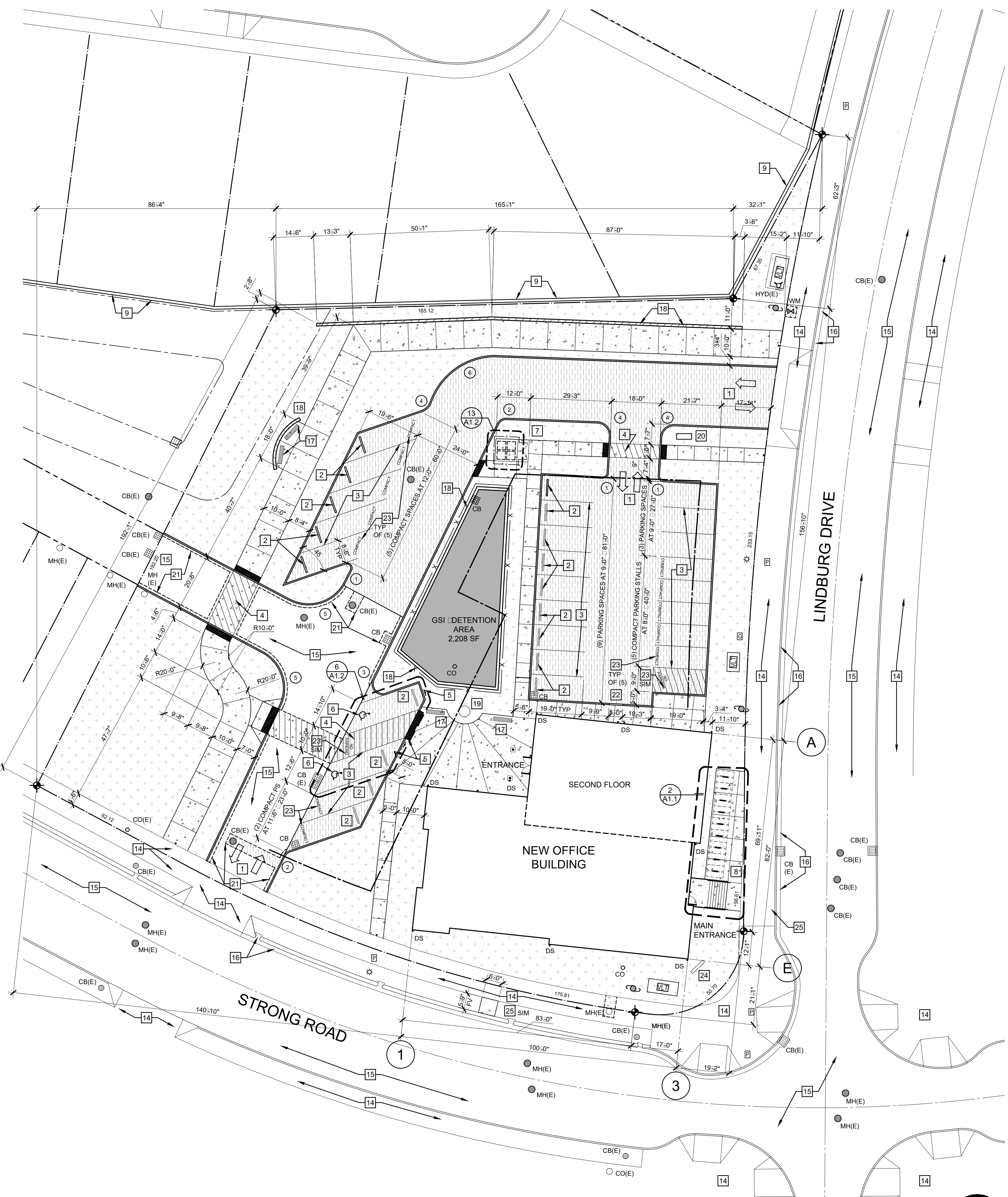
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ARCHITECTURE
COMMUNITY
1100 Liberty St SE, Suite 200
Salem, OR 97302-5385
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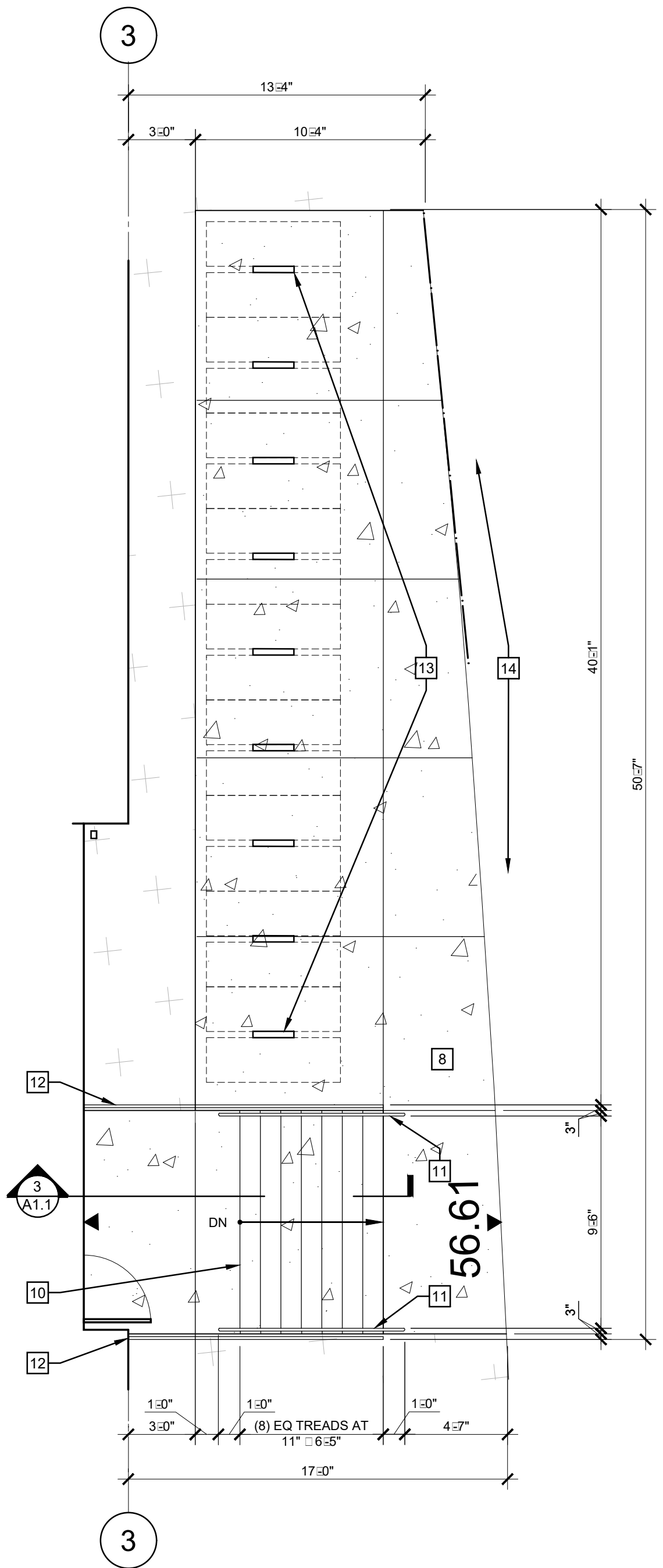
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SALEM, OREGON 97302

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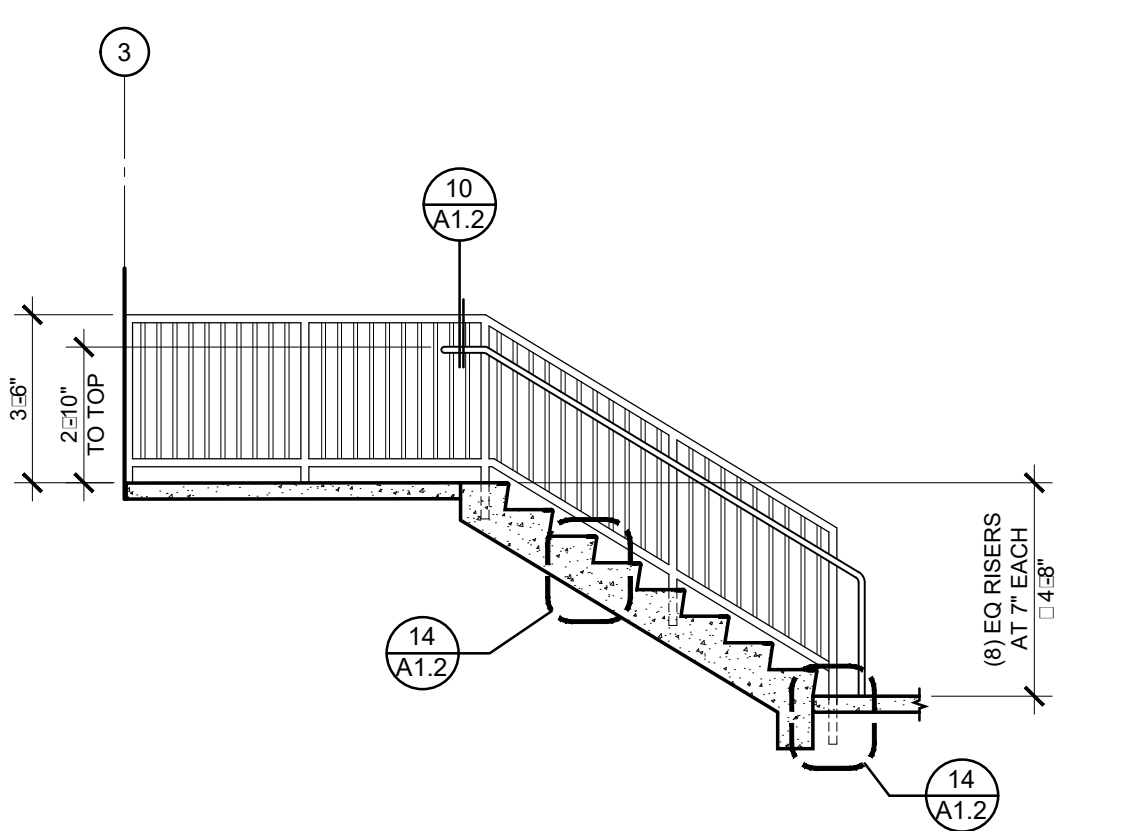
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1 SITE PLAN
SCALE: 1"=20'-0"



2 ENLARGED BIKE RACK PLAN
SCALE: 1"=10'-0"



3 EXTERIOR STAIR SECTION
SCALE: 1/4"=1'-0"

2203-A1.1-03

Attachment C



1012 Pine Street
Silverton, Oregon
503.784.6494
Project #: 1449C

Legend:

- 2" TO 6" ROUND ROCK
- 1' TO 3' DIAMETER ROCK BOULDERS

General Notes:

1. VERIFY ALL UTILITIES PRIOR TO CONSTRUCTION. SEE CIVIL DRAWINGS. CALL BEFORE YOU DIG. NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
2. SEE ARCHITECTURAL DRAWINGS FOR SITE PLAN.
3. SEE CIVIL DRAWINGS FOR GRADING, UTILITIES, AND STORMWATER FACILITY.
4. PLANTING REQUIREMENTS SEE TABLES THIS SHEET.
5. PLANT SCHEDULES SEE SHEET L1.2.
6. PLANTING NOTES AND DETAILS SEE SHEET L1.3.
7. IRRIGATION PLAN SEE SHEETS L2.1 AND L2.2.

Water Quality Planter Planting Requirements

Planter Number	Facility SF	Trees	Large Shrubs	Small Shrubs	Grasses / Herbs / Groundcover
1	1635	2	12	68	2,980 (1572 SF)

Requirements per 100 SF
1 Evergreen or Deciduous Tree -OR-
4 Large Shrubs -OR-
6 Small Shrubs
Herbaceous and Groundcover at least 75% of Facility

General Landscape Planting Requirements

REQUIRED PLANTS:	1 PLANT PER 20 SQUARE FEET = 1 PLANT UNIT (PU)
1 MATURE TREE = 15 PU	
1 SHADE TREE = 10 PU	
1 EVERGREEN TREE = 5 PU	
1 ORNAMENTAL TREE = 2 PU	
1 LARGE SHRUB = 2 PU	
1 SMALL/MEDIUM SHRUB = 1 PU	
LAWN/GROUNDCOVER = 1 PU PER 50 SF	

LANDSCAPE SQUARE FOOTAGE = 17,978 SF
NUMBER OF REQUIRED PUs = 899 PU
40% PU REQUIRED AS TREES = 360 PU

Plants	# of Plants	Plant Units
Shade Trees	31	310
Evergreen Trees	8	40
Ornamental Trees	6	12
Large Shrubs	130	260
Small / Medium Shrubs	510	510
Lawn / Groundcover	4952 sf	99
Total Plant Units*		1231

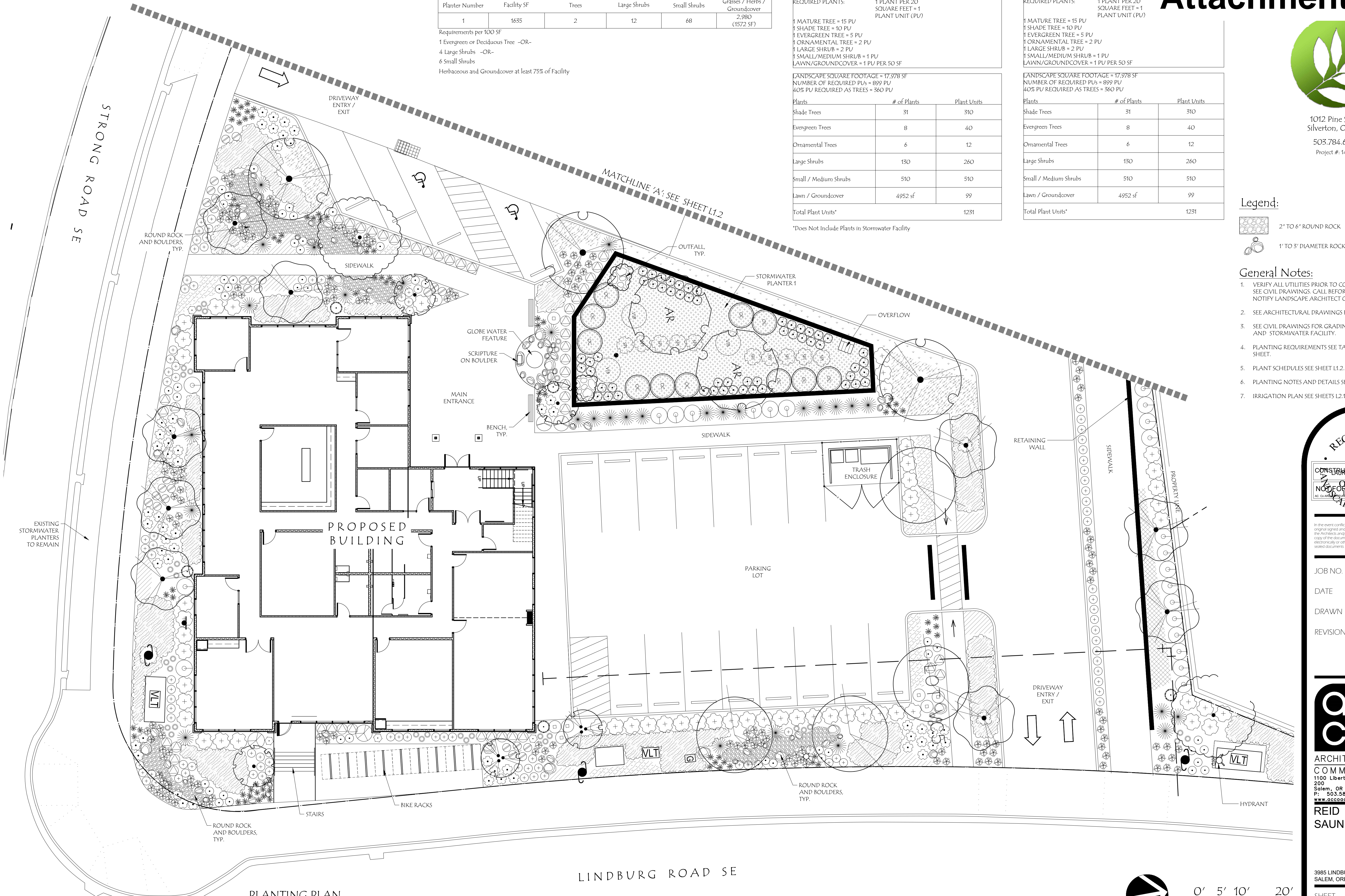
*Does Not Include Plants in Stormwater Facility

Parking Lot Planting Requirements

REQUIRED PLANTS:	1 PLANT PER 20 SQUARE FEET = 1 PLANT UNIT (PU)
1 MATURE TREE = 15 PU	
1 SHADE TREE = 10 PU	
1 EVERGREEN TREE = 5 PU	
1 ORNAMENTAL TREE = 2 PU	
1 LARGE SHRUB = 2 PU	
1 SMALL/MEDIUM SHRUB = 1 PU	
LAWN/GROUNDCOVER = 1 PU PER 50 SF	

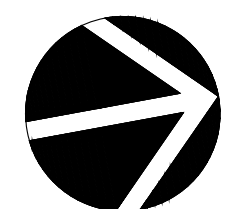
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Evergreen Trees	8	40
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Large Shrubs	130	260
Small / Medium Shrubs	510	510
Lawn / Groundcover	4952 sf	99
Total Plant Units*		1231



PLANTING PLAN
SCALE: 1" = 10'-0"

LINDBURG ROAD SE



0' 5' 10' 20'
SCALE



In the event conflicts are discovered between the original signed and sealed documents prepared by the Architect and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

JOB NO. 2022.0003
DATE JAN 30, 2023
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SALEM, OREGON 97302
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Stormwater Facility Planting Notes:

1. THE LANDSCAPE CONTRACTOR IS TO THOROUGHLY REVIEW THE SITE. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLAN AND EXISTING CONDITIONS THE LANDSCAPE ARCHITECT IS TO BE IMMEDIATELY NOTIFIED.
2. IF THE LANDSCAPE CONTRACTOR STARTS WORK BEFORE SITE CONDITIONS ARE READY, THEY WILL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS RELATING TO THE CONDITION.
3. SEE CITY OF SALEM STORMWATER REQUIREMENTS FOR MORE INFORMATION.
4. CONTAINER STOCK MAY BE PLANTED YEAR ROUND IF CONDITIONS PERMIT. PLANT AFTER 48 HOURS OF DRY WEATHER TO AVOID SOIL COMPACTION. USE JUTE OR COIR MATTING TO PREVENT EROSION IF NEEDED.
5. PLANT MATERIALS SHALL BE FREE OF DISEASE, INJURY, AND INSECT INFESTATION. UNHEALTHY OR DAMAGED PLANTS SHALL BE REPLACED BY LANDSCAPE CONTRACTOR.
6. SEE CIVIL PLANS FOR GRADING, EROSION CONTROL AND SITE PREPARATION.
7. MULCH SHRUBS ON SIDE SLOPES WITH 2" DEPTH MIN. AND 18" DIAMETER WIDE AREA WITH CHEMICAL FREE AGED COMPOST. DO NOT USE MULCH IN ZONE 1 AND OTHER FREQUENTLY INUNDATED AREAS.
8. FACILITY TO BE IRRIGATED WITH IRRIGATION SYSTEM FOR A MINIMUM OF 2 YEARS. SEE IRRIGATION PLAN. PLANTS TO RECEIVE A MINIMUM OF 1" OF WATER PER WEEK FROM JUNE 15TH TO OCTOBER 15TH THE FIRST YEAR AND BE MONITORED TO MAINTAIN HEALTHY CONDITIONS. WATER AMOUNTS MAY BE REDUCED THE SECOND YEAR FROM JUNE 15TH TO OCTOBER 15TH, BUT MAINTAIN WEEKLY WATERING AND ADDITIONAL WATERING MAY BE NEEDED BASED ON MONITORING.
9. MAINTENANCE AND MONITORING TO TAKE PLACE ANNUALLY. TAG PLANTS WITH A RUST PROOF LABEL TO FACILITATE MONITORING. REPLACE DEAD OR DYING PLANT MATERIAL AS NEEDED.

General Planting Notes:

1. THE LANDSCAPE CONTRACTOR IS TO THOROUGHLY REVIEW THE SITE. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLAN AND EXISTING CONDITIONS THE LANDSCAPE ARCHITECT IS TO BE IMMEDIATELY NOTIFIED.
2. IF THE LANDSCAPE CONTRACTOR STARTS WORK BEFORE SITE CONDITIONS ARE READY, THEY WILL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS RELATING TO THE CONDITION.
3. PLANT MATERIALS SHALL BE FREE OF DISEASE, INJURY, AND INSECT INFESTATION. UNHEALTHY OR DAMAGED PLANTS SHALL BE REPLACED BY LANDSCAPE CONTRACTOR. ALL PLANT MATERIAL SHALL FOLLOW THE CURRENT AMERICAN STANDARD FOR NURSERY STOCK PUBLICATIONS INCLUDING ANSI A300 AND ANSI Z60.
4. PLANTER BEDS: ALL PLANTER BEDS SHALL HAVE A MINIMUM DEPTH OF 8" WORKABLE TOPSOIL, COMPACTED AT A MAXIMUM OF 85% STANDARD PROCTOR MAXIMUM DRY DENSITY. TOPSOIL SHALL BE OVER ROCK-FREE SUBGRADE. SUBGRADE TO BE RIPPED AND TILLED TO 6" DEPTH AND REMOVE ALL DEBRIS 2" OR LARGER. SMALL PLANTER AREAS MAY REQUIRE REMOVAL OF COMPACTED SOIL, ROCK, GRAVEL TO AT LEAST 18" DEEP. LOOSEN AND AMEND SOIL BEFORE REPLACING IN 6" LIFTS TO FINISH GRADE.
5. TOPSOIL MIX: AMEND EXISTING SOIL IN-SITU OR STOCK PILE. SOIL ON SITE. IMPORT TOPSOIL ONLY AS NECESSARY. CONDUCT A SOIL SAMPLE FOR EACH TYPE OF PLANTER AREA. SEND SAMPLES TO AN INDEPENDENT LABORATORY RECOGNIZED BY THE STATE DEPARTMENT OF AGRICULTURE AND SPECIALIZING IN AGRONOMIC SOIL ANALYSIS FOR TESTING AND AMENDMENT RECOMMENDATIONS.
6. SOIL AMENDMENTS: ADD A MINIMUM OF 3" CLEAN, MATURE COMPOST TO TOPSOIL. TILL IN. FOR ALL PLANTER BEDS, FOR BIDDING PURPOSES, ASSUME GENERAL SOIL AMENDMENTS AS FOLLOWS PER 1000' SF AT 6" LIFTS UNTIL SOIL ANALYSIS RECOMMENDATION IS COMPLETE, SEE ABOVE FOR COMPOST:
 - 25 LBS GYPSUM
 - 75 LBS LIME
 - 8 LBS SUPERPHOSPHATE
 - 3 LBS AMMONIUM NITRATE
 - 4 OZS ZINC SULFATE
 - 8 OZS MANGANESE SULFATE
7. MYCORRHIZAL FUNGI INOCULATE: USE A COMBINED ENDO AND ECTO MYCORRHIZAL FUNGI INOCULATE SUCH AS BIO-ORGANICS OR EQUAL AT A RATE OF:
 - 2" CAL. B&B TREE: 3 TEASPOONS
 - 5 GALLON: 2 TEASPOONS
 - 1-3 GALLON PLANT: 1 TEASPOON
 - 4" POT: 1/4 TEASPOON
 - SEED/TURF: 1 LB PER 2000 SFDO NOT USE ON RHODODENDRON/AZALEA, HUCKLEBERRY, SEDGE, RUSH, HEATH.
8. PLANTING: VERIFY SOIL IS APPROPRIATELY DRY FOR DIGGING. SEE DETAILS THIS SHEET FOR HOLE DEPTH, WIDTH AND BACKFILL. DEEP WATER IMMEDIATELY AFTER PLANTING.
9. MOUND PLANTING BED AREAS 3% FOR POSITIVE DRAINAGE AND AESTHETICS.
10. BARK MULCH: SPREAD 2" MAX. DEPTH AGED FIR MULCH IN ALL PLANTER BEDS AND OPEN LANDSCAPE AREAS. KEEP MULCH AWAY FROM PLANT BASE.
11. FERTILIZER: DO NOT USE ADDITIONAL FERTILIZERS ON NEWLY PLANTED TREES FOR FIRST YEAR.
12. TREES: TREE STAKES TO BE REMOVED AFTER 6 MONTHS.
13. PLANT QUANTITIES SHOWN ARE INTENDED TO ASSIST THE CONTRACTOR IN EVALUATING THEIR OWN TAKE-OFFS. IF THERE IS A DISCREPANCY BETWEEN PLANT QUANTITIES AND SYMBOLS SHOWN, USE THE LARGER OF THE TWO AMOUNTS. CONTRACTOR IS RESPONSIBLE FOR ALL FINAL QUANTITIES.
14. NOTIFY LANDSCAPE ARCHITECT OF PLANT SUBSTITUTIONS.

Legend:

- 2" TO 6" ROUND ROCK
- 1' TO 3' DIAMETER ROCK BOULDERS

General Notes:

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2. SEE ARCHITECTURAL DRAWINGS FOR SITE PLAN.
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4. PLANTING REQUIREMENTS SEE TABLE THIS SHEET.
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7. IRRIGATION PLAN SEE SHEETS L2.1 AND L2.2.

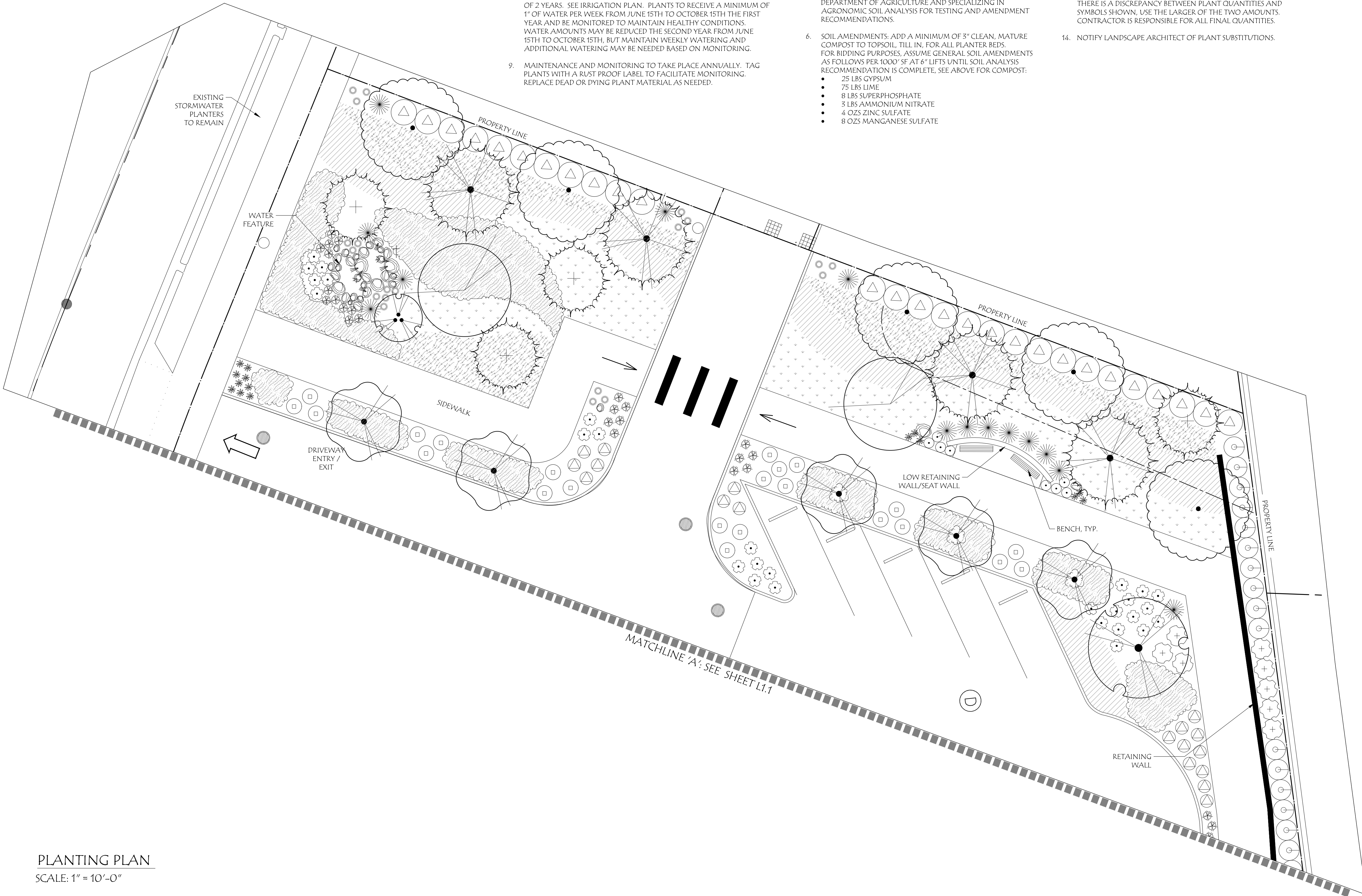
Laurus Designs, LLC



1012 Pine Street
Silverton, Oregon

503.784.6494

Project #: 1449C



PLANTING PLAN
SCALE: 1" = 10'-0"

REGISTERED
643



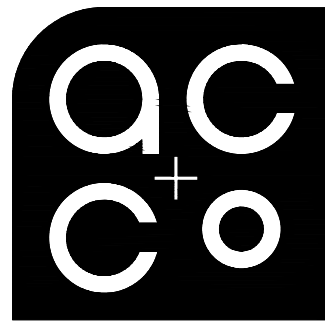
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SALEM, OREGON 97302

SHEET



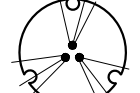
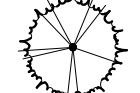


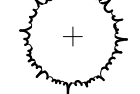
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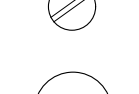


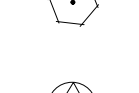
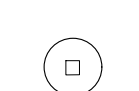
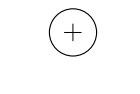



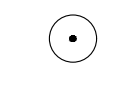



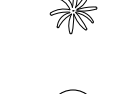


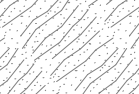
General Notes:

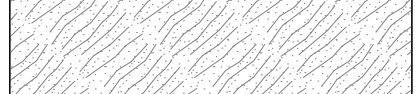


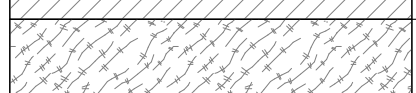
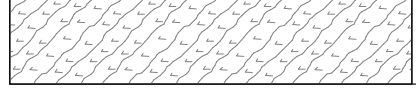

1. VERIFY ALL UTILITIES PRIOR TO CONSTRUCTION. SEE CIVIL DRAWINGS. CALL BEFORE YOU DIG. NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
2. SEE ARCHITECTURAL DRAWINGS FOR SITE PLAN.
3. SEE CIVIL DRAWINGS FOR GRADING, UTILITIES, AND STORMWATER FACILITY.
4. PLANTING PLAN SEE SHEETS L1.1 AND L1.2.
5. IRRIGATION PLAN SEE SHEETS L2.1 AND L2.2.

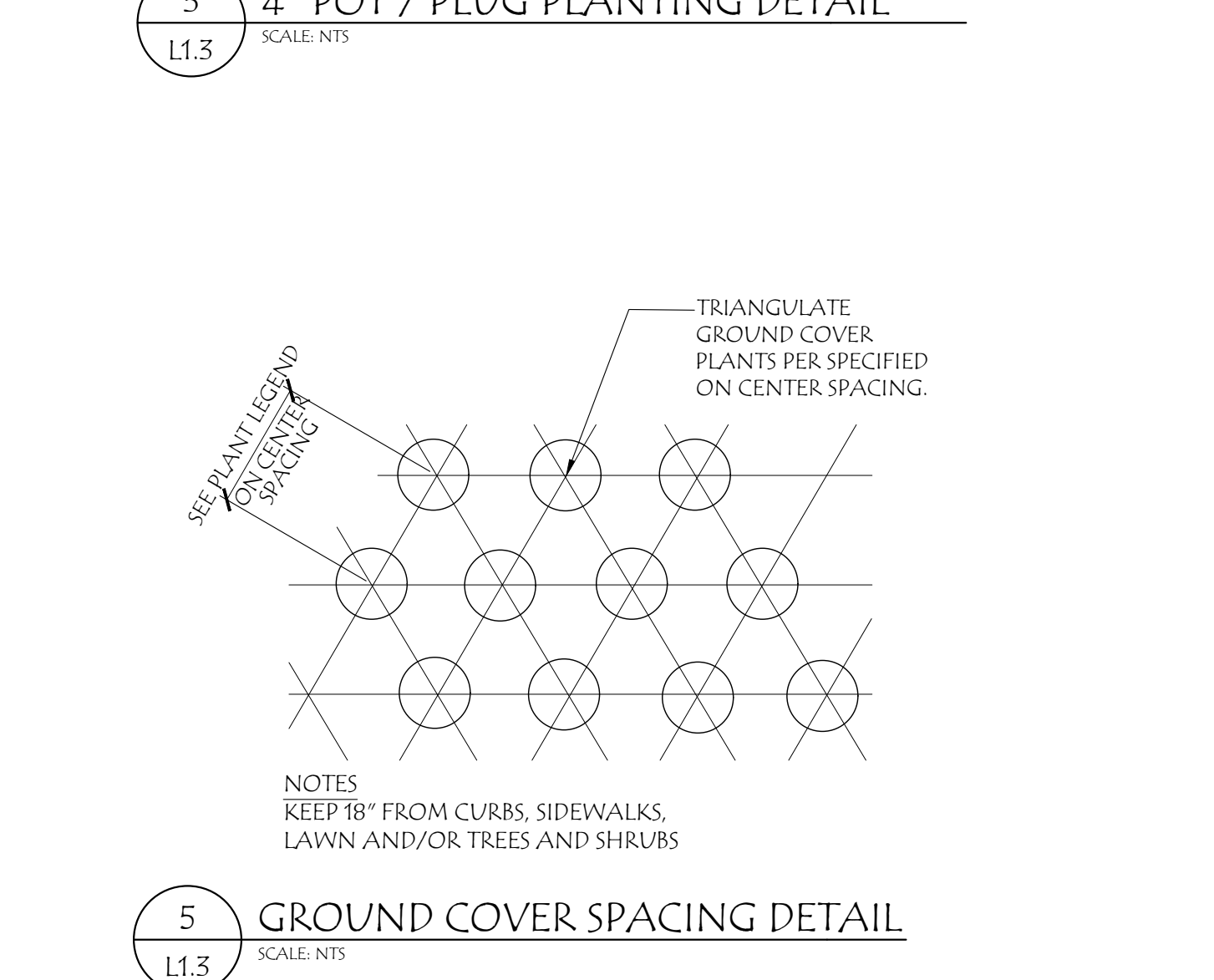
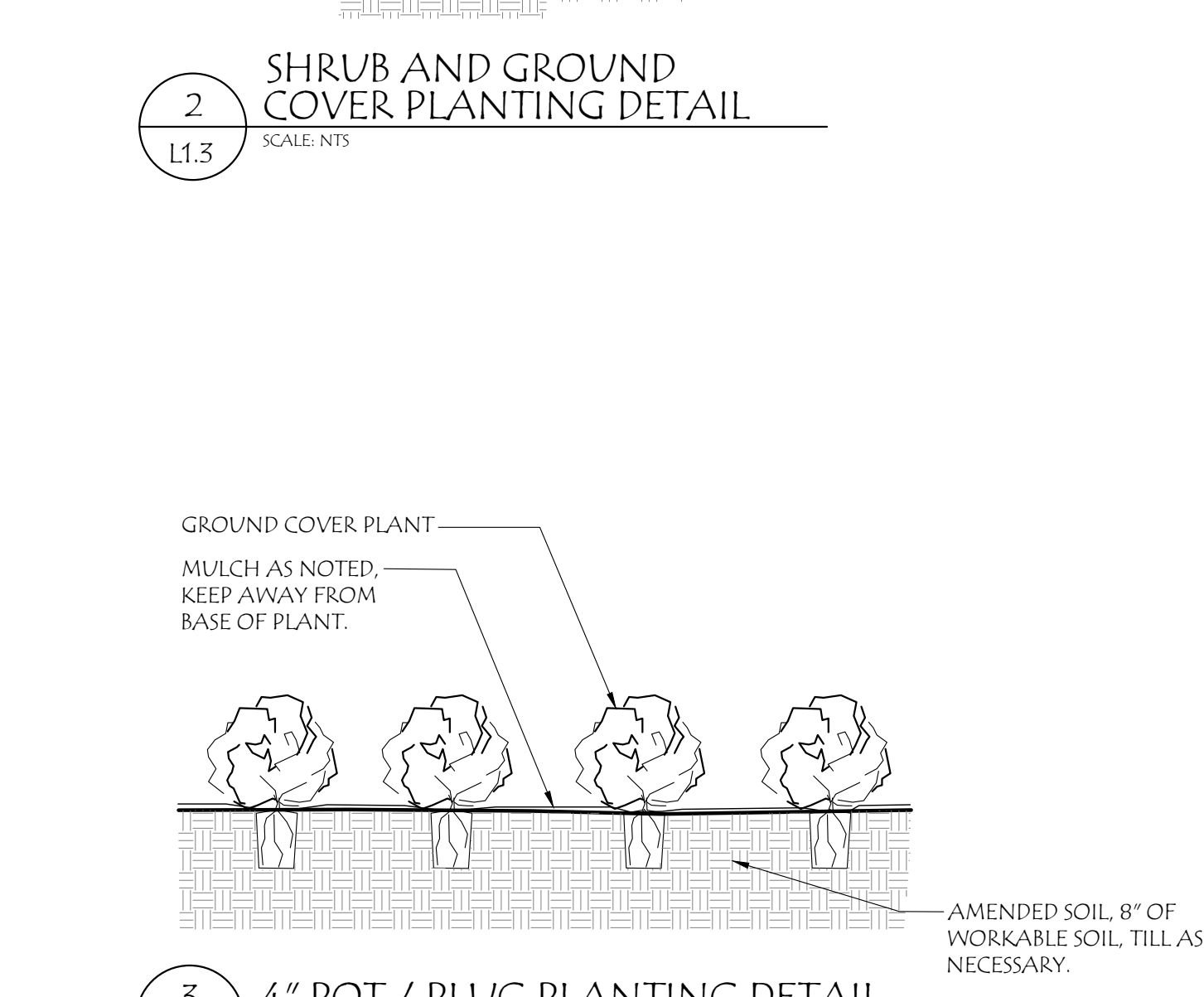
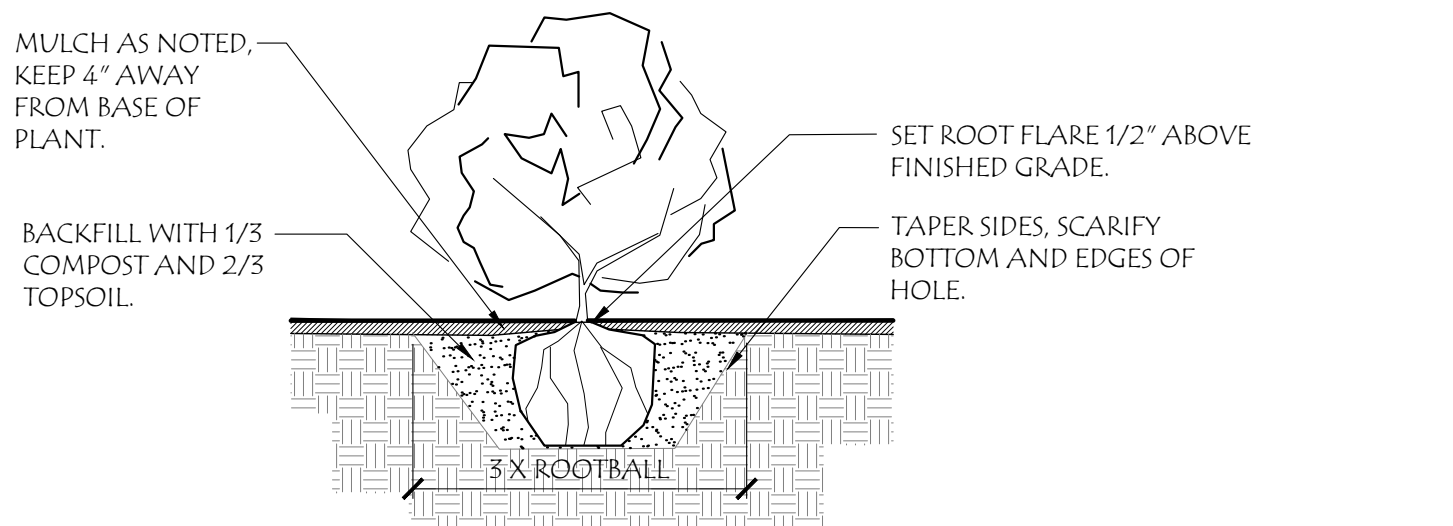
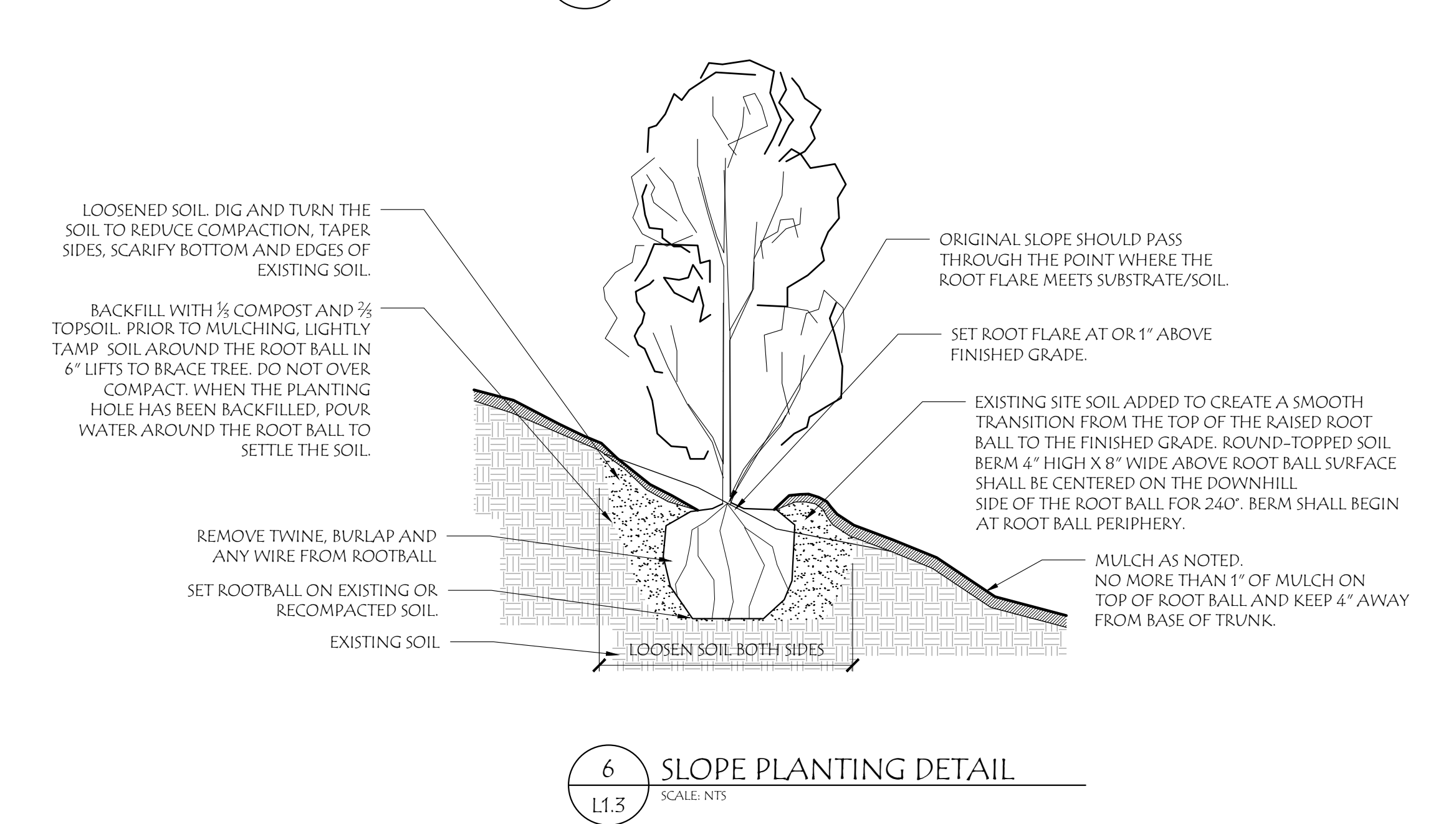
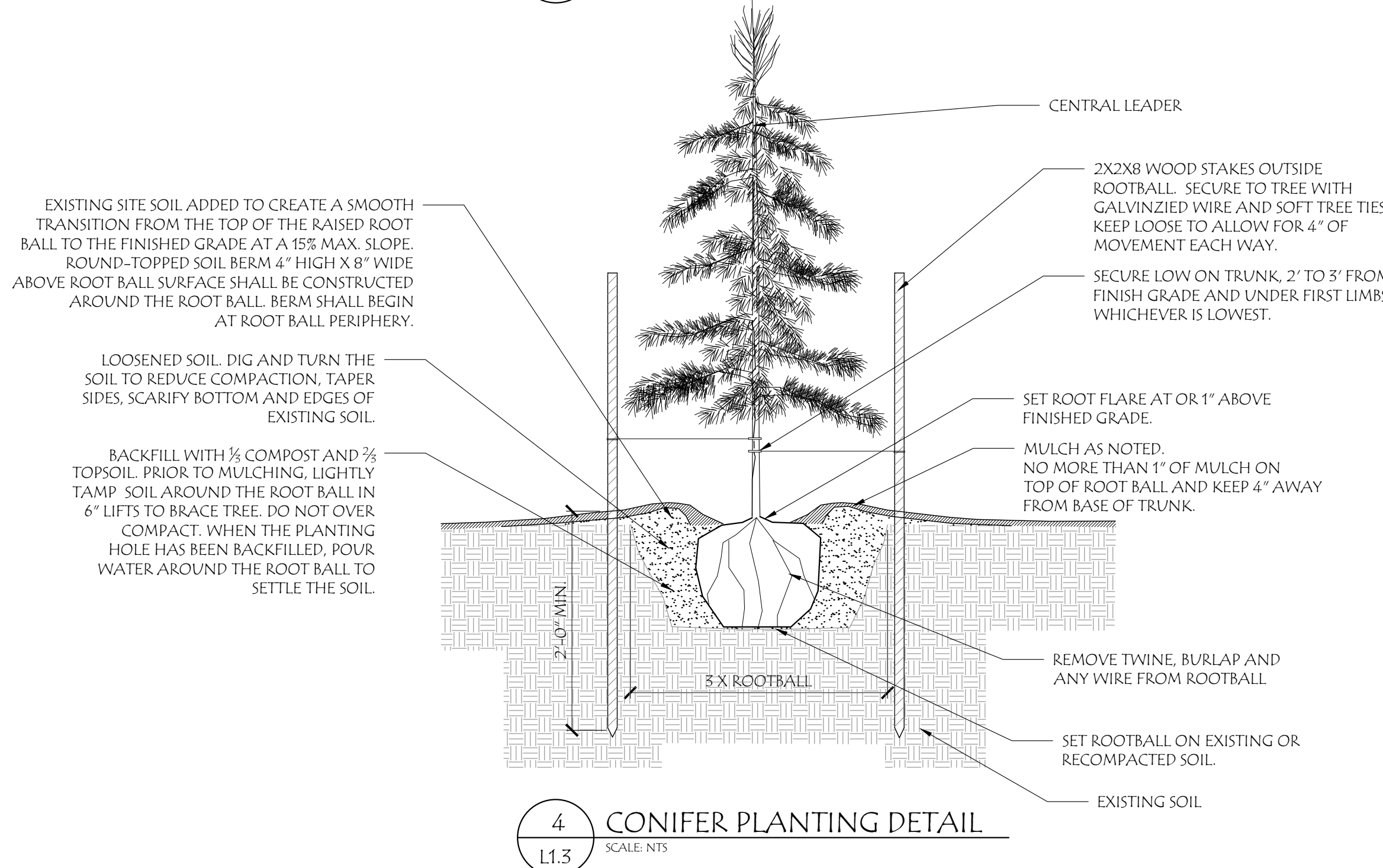
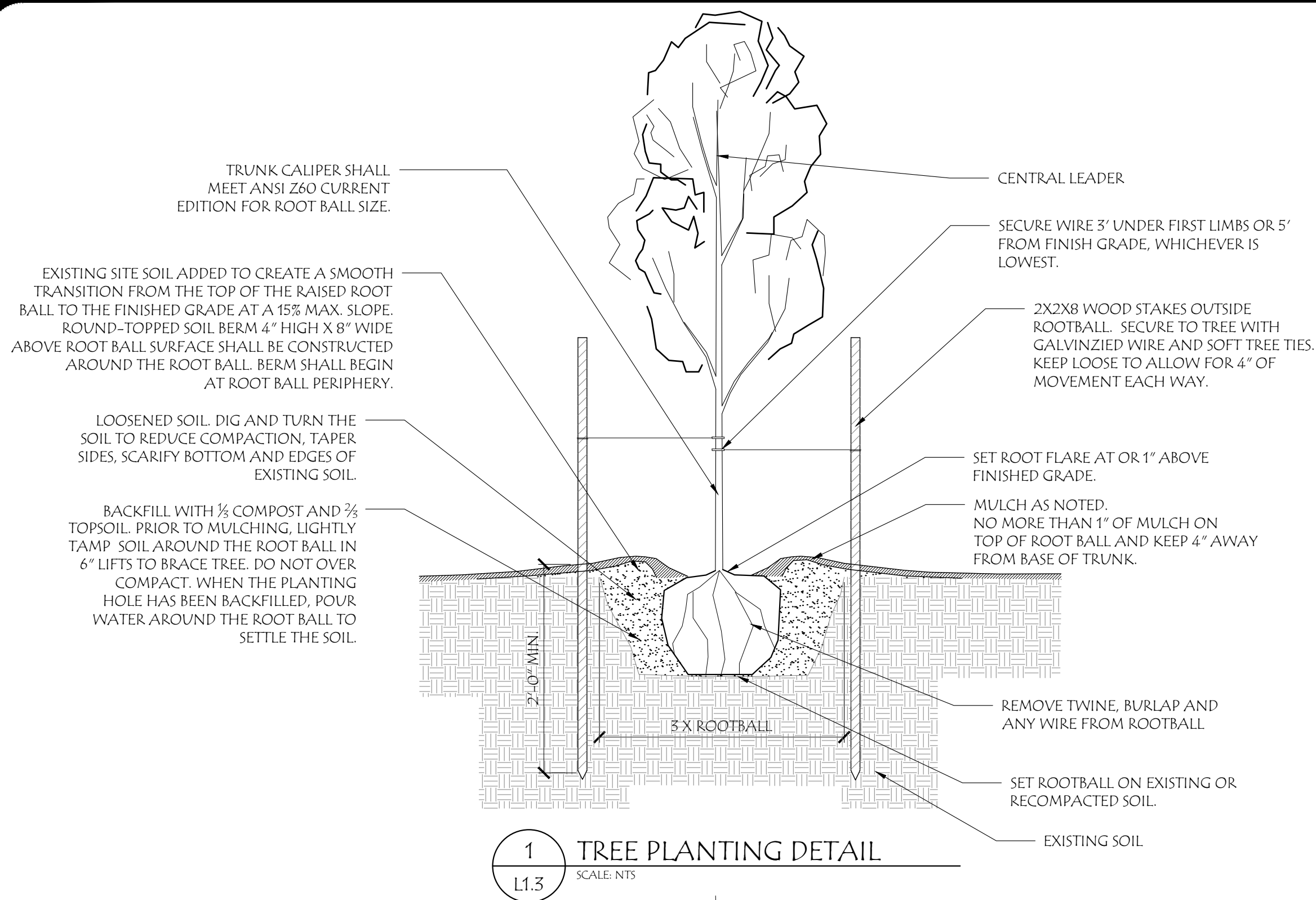
Plant Schedule

TREES	QTY	BOTANICAL / COMMON NAME	SIZE
	3	Acer palmatum 'Bloodgood' / Bloodgood Japanese Maple	1" Cal., B&B
	6	Cornus nuttallii x florida 'Eddie's White Wonder' / Eddie's White Wonder Dogwood	1 1/2" Cal., B&B
	3	Lagerstroemia indica x fauriei 'Zuni' / Lavender Crape Myrtle Multi-Trunk	1" Cal., B&B, Multi-trunk
	7	Nyssa sylvatica 'Zydeco Twist' / Zydeco Twist Tupelo	1 1/2" Cal., B&B
	4	Picea pungens 'Glauca' / Blue Colorado Spruce	6'-8' Ht., B&B
	4	Stewartia pseudocamellia / Japanese Stewartia	1 1/2" Cal., B&B
	14	Styrax japonicus / Japanese Snowbell	1 1/2" Cal., B&B
	4	Tsuga mertensiana / Mountain Hemlock	6'-8' Ht., B&B

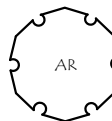





SHRUBS	QTY	BOTANICAL / COMMON NAME	SIZE
	81	Abelia x grandiflora 'Kaleidoscope' / Glossy Abelia	2 Gal.
	29	Berberis thunbergii 'Concorde' / Concorde Japanese Barberry	2 Gal.
	39	Choisya ternata / Mexican Orange	5 Gal.
	6	Cupressus macrocarpa 'Goldcrest' / Goldcrest Monterey Cypress	5 Gal.
	27	Escallonia x exoniensis 'Fradesii' / Pink Princess Escallonia	5 Gal.
	5	Ilex crenata 'Sky Pencil' / Sky Pencil Japanese Holly	24"-30" Ht.
	64	Ilex crenata 'Soft Touch' / Soft Touch Japanese Holly	2 Gal.
	31	Nandina domestica 'Gulf Stream' / Gulf Stream Heavenly Bamboo	2 Gal.
	94	Nandina domestica 'Nana' / Dwarf Heavenly Bamboo	2 Gal.
	62	Rhaphiolepis indica 'Conor' / Eleanor Tabor Indian Hawthorn	3 Gal.
	2	Rhododendron x 'P.J.M.' / P.J.M. Rhododendron	3 Gal.

GRASSES / PERENNIALS	QTY	BOTANICAL / COMMON NAME	SIZE
	56	Calamagrostis x acutiflora 'Karl Foerster' / Karl Foerster Feather Reed Grass	1 Gal.
	95	Carex oshimensis 'Evergold' / Evergold Japanese Sedge	1 Gal.
	56	Iberis sempervirens 'Alexander's White' / White Evergreen Candytuft	1 Gal.
	44	Miscanthus sinensis 'Morning Light' / Morning Light Eulalia Grass	2 Gal.
	179	Pennisetum alopecuroides 'Little Bunny' / Little Bunny Fountain Grass	1 Gal.

SHRUB AREAS	QTY	BOTANICAL / COMMON NAME	SIZE AND NOTES	SPACING
	200	Prunus laurocerasus 'Mount Vernon' / Mount Vernon English Laurel	1 Gal.	48" o.c.
GROUND COVERS	QTY	BOTANICAL / COMMON NAME	SIZE	SPACING
	192 (1,641 sf)	Arctostaphylos uva-ursi / Kinnikinnick	1 Gal.	36" o.c.
	28 (165 sf)	Iberis sempervirens 'Alexander's White' / White Evergreen Candytuft	1 Gal.	30" o.c.
	349 (1,323 sf)	Phlox subulata / Creeping Phlox	1 Gal.	24" o.c.
	179 (1,529 sf)	Rubus calycinoides 'Emerald Carpet' / Emerald Carpet Creeping Raspberry	1 Gal.	36" o.c.
	311 (296 sf)	Sedum rupestre 'Angelina' / Angelina Sedum	1 Gal.	12" o.c.



Water Quality Planter Planting Schedule

TREES	QTY	BOTANICAL / COMMON NAME	SIZE	
	2	Alnus rhombifolia / White Alder	1 1/2" Cal., B&B	
SMALL TREE/LARGE SHRUBS	QTY	BOTANICAL / COMMON NAME	SIZE	
	12	Oemleria cerasiformis / Indian Plum	5 Gal.	
SMALL SHRUBS	QTY	BOTANICAL / COMMON NAME	SIZE	
	55	Cornus sericea "Kelsey" / Kelsey Dogwood	1 Gal.	
	10	Spiraea douglasii / Western Spiraea	1 Gal.	
	3	Viburnum edule / Highbush Cranberry	1 Gal.	
GROUND COVERS	QTY	BOTANICAL / COMMON NAME	SIZE	SPACING
	745 745 745 745	Carex densa / Dense Sedge Juncus ensifolius / Dagger-leaf Rush Juncus tenuis / Slender Rush Scirpus microcarpus / Small Fruited Bulrush	Plug, 1" x 6" Min. Size	9" o.c., Plant Alternating Species in 8' wide Rows

REGISTERED
643

In the event conflicts are discovered between the original signed and sealed documents prepared by the Architect and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

JOB NO. 2022.0003

DATE JAN 30, 2023

DRAWN

REVISIONS



ARCHITECTURE
COMMUNITY
1100 Liberty St SE, Suite
200
Salem, OR 97302-5385
P: 503.581.4114
www.acccoc.com

REID
SAUNDERS3985 LINDBURG RD SE
SALEM, OREGON 97302

SHEET

L1.3

Attachment D

ELEVATION REFERENCE NOTES:

- 1

OFFCI SIGNAGE, COORDINATE SIZE AND LOCATION WITH OWNER
- 2

WINDOW PER PLAN
- 3

STEEL CANOPY, PAINT, REFER TO STRUCTURAL
- 4

FIBER CEMENT TRIM, PAINT
- 5

HORIZONTAL FIBER CEMENT LAP SIDING, PAINT
- 6

STANDING SEAM METAL ROOF
- 7

FIBER CEMENT FASCIA, PAINT
- 8

COLUMN PER STRUCTURAL, PAINT

9

STONE VENEER, STEP WITH GRADE

10

STONE SILL

11

PRE-FINISHED PARAPET FLASHING CAP FLASHING

12

DOOR PER PLAN

13

PRE-FINISHED SHEET METAL GUTTER AND DOWNSPOUT, AT SIM CONDITION, DAYLIGHT ONTO LOWER ROOF

14

METAL BUILDING ADDRESS NUMBER, 12" HEIGHT, REFER TO SPECIFICATIONS

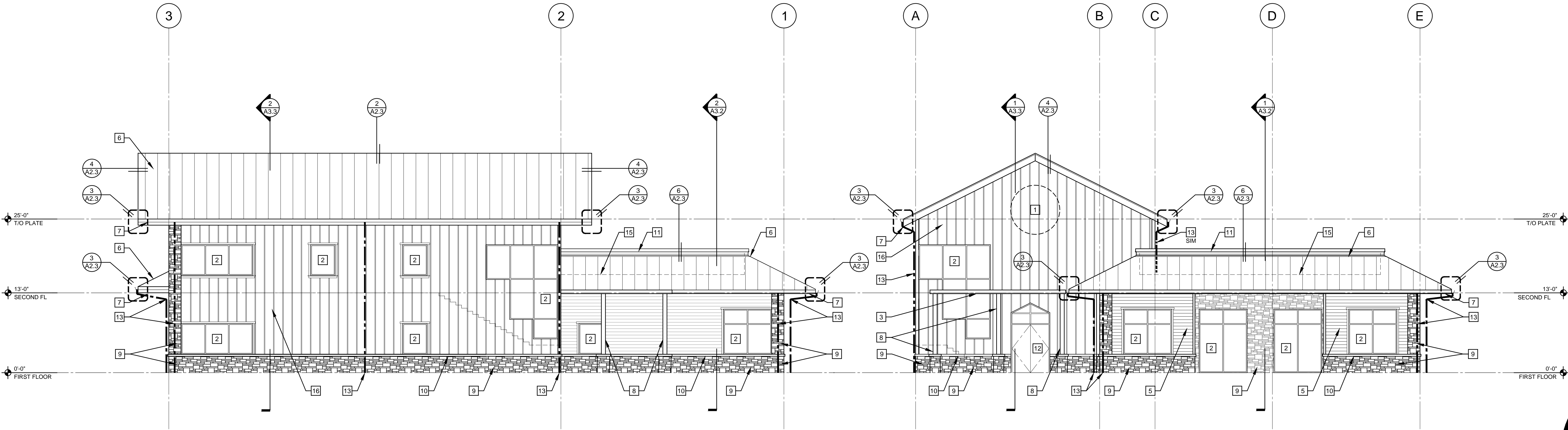
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PARAPET BEYOND

16

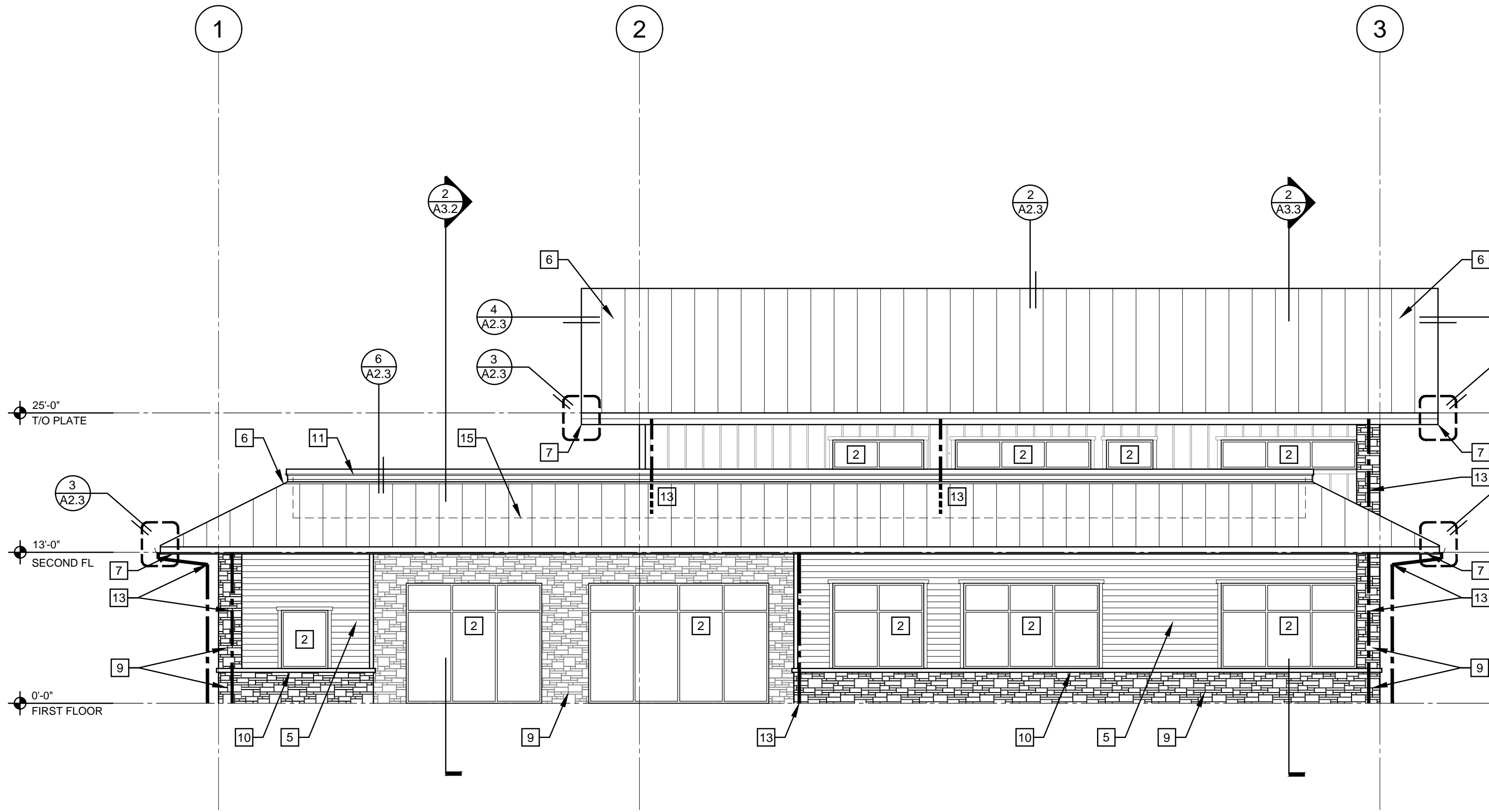
VERTICAL BOARD AND BATTEN SIDING, PAINT

- ARCHITECT SHALL IMMEDIATELY UP-ANY DISCREPANCIES OR UNUSUAL DIMENSIONS PRIOR TO PROCEEDING WITH AREA OF QUESTIONABLE WORK.
4. IN CASE OF ANY CONFLICTS IN THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, THE CONTRACTOR IS REQUIRED TO INCLUDE THE BETTER QUALITY AND LARGER QUANTITY OF THE WORK.
5. CONTRACTOR AND BIDDERS SHALL USE COMPLETE SETS OF CONTRACT DOCUMENTS; NEITHER THE OWNER NOR ARCHITECT ASSUMES RESPONSIBILITY FOR ERRORS OR MISINTERPRETATIONS RESULTING FROM THE USE OF INCOMPLETE SETS OF CONTRACT DOCUMENTS.
6. COORDINATE AND PROVIDE WOOD BACKING FOR ALL OFFCI ITEMS

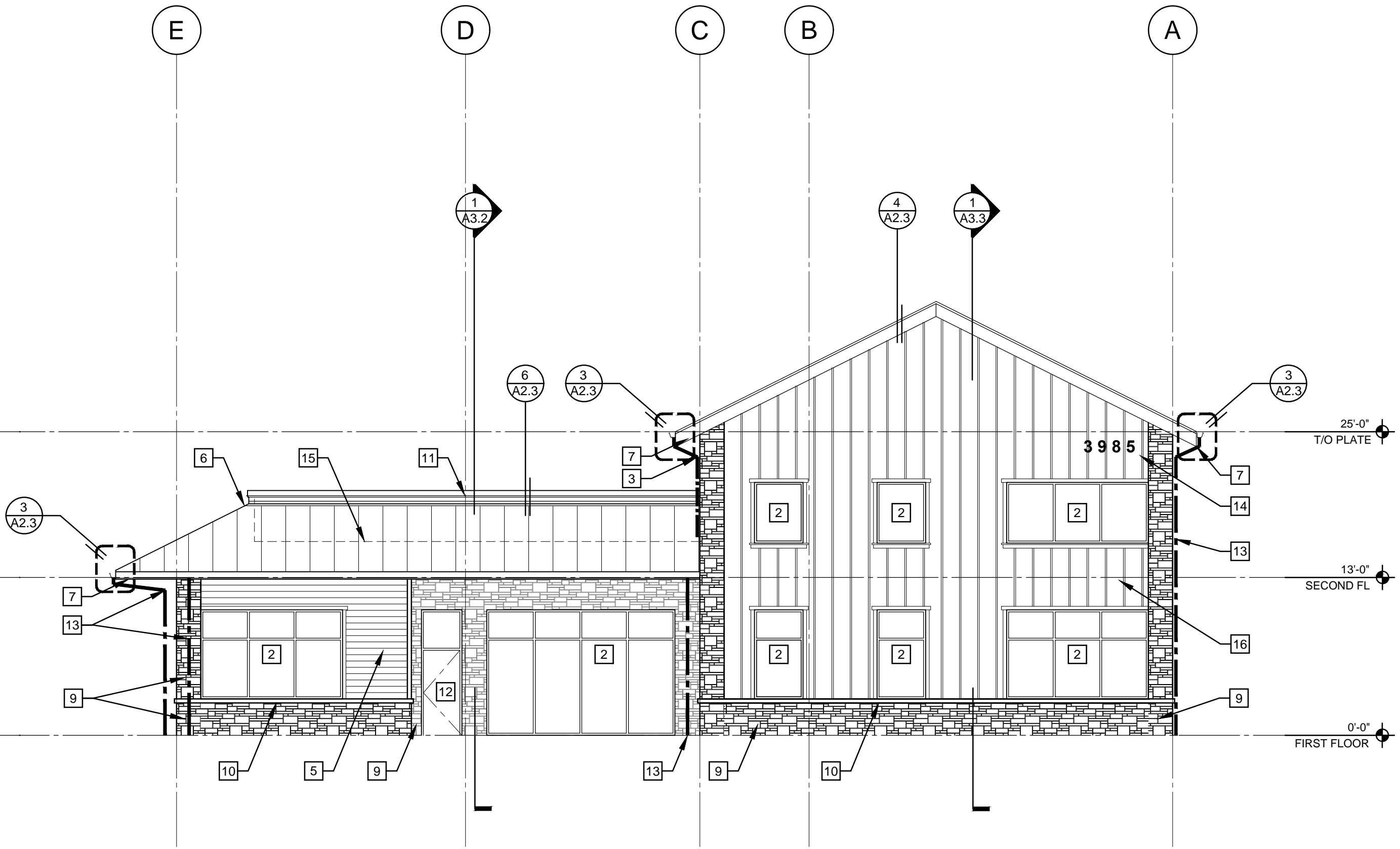


1 NORTH EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"

2 WEST EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"



3 SOUTH EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"



4 EAST EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"

CONSTRUCTION DOCUMENTS
NOT FOR CONSTRUCTION
AC+CO ARCHITECTURE | COMMUNITY JAN 30, 2023

In the event conflicts are discovered between the original signed and sealed documents prepared by the Architects and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

JOB NO. 2022.0003
DATE JAN 30, 2023
DRAWN CJA
REVISIONS



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REID
SAUNDERS

3985 LINDBURG RD SE
SALEM, OREGON 97302

SHEET

A3.1

May 05, 2023

Planning Manager
City of Salem Community Development Planning Division
555 Liberty St SE Room 305
Salem, OR 97301-3503

RE: Reid Saunders Association
Strong Rd SE & Lindburg Rd SE
Salem, OR 97302



To Whom it May Concern:

Our written statement to criteria found in SRC 250.005, d, 2 is as follows:

- A. *The purpose underlying the specific development standard proposed for adjustment is:*
 - i. *Clearly inapplicable to the proposed development; or*
 - ii. *Equally or better met by the proposed development.*
- a. Answer (Adjustment 1 Request: Setbacks):
 - i. Given the existing lot and its physical constraints our office does not believe the required 10'-20' setback to private drive and property lines can be obtained in all areas of the site. We have provided this along the east facade, although due to the site irregular shape and the planned private drive, this is unattainable on the north, south and west facades. Along the west, the private drive is between 22' to 59' from the façade. Along the north, the façade is 95' to 100' from the property line. Along the south, the façade is 10' to 22' from the property line. To provide a building that would comply with this standard is impossible on the north because the site is bisected with the private drive. Due to the angle of the private drive and location on the site, a building that would comply with the standard on the west would be an irregular shaped building with non-traditional construction, and therefore, detracting from the intent of the standard. The utility easement prohibits the SE corner of the building from complying. We feel the proposed development equally or better meets the intent of the setback requirements rather than an irregular and massive building on the site.
 - ii. Due to the angled private drive, the 20' minimum required setback from parking lots abutting streets cannot be met. There is not adequate room on the site to accommodate this setback along the private drive nor Lindburg Rd. There is a pinch point due to the angled private drive that the parking cannot accommodate. We have gotten the surface parking lot 20'-0" from the private road; however, the constraints of the site limit us to not be able to achieve 20'-0" from the right-of-way, which is inclusive of the entire street section of landscape strips and sidewalks. We believe our proposed development meets the intent of the setback requirements because the areas between the road and parking area are

heavily landscaped and will provide visual screening of the parking lot, which will offset the reduced setback depth.

- b. Answer (Adjustment 2 Request: Frontage): Due to the extensive frontage of this corner lot, a building that would comply with the frontage requirement of 70% along Strong Road SE and Lindburg Road SE is unfeasible. The building would need to be disproportionately long and narrow to meet this standard. This also would create additional site disturbance than necessary. The site is also bisected along both frontages with the private drive, making frontage unachievable. We have provided 37% frontage along Strong Rd SE and 25% frontage along Lindburg Road SE. We are proposing a building that has a first floor building height of at least 14 feet. We are providing large ground floor windows facing Strong Rd and Lindburg Road, totaling 41% of the façade. We are providing a primary building entrance adjacent to the intersection of Strong Road and Lindburg Road. We are also providing large overhanging soffits around the first floor building area and a canopy in the entry adjacent the surface parking lot and ADA stalls. These building elements we believe will offset the smaller sized building and reduced lot frontage by ensuring that those portions of the building which do occupy the required setbacks along the public street are designed to visually reinforce and support an active and inviting pedestrian environment at the intersection of Strong Road and Lindburg Road, which is the key most prominent and significant street intersection within the Fairview Training Site located in the core of the Village Center area. We feel we have met the intent of the standard to the best of our ability given the restrictions of the site.
- c. Answer (Adjustment 3 Request: FAR): Due to the large area of the site, a building that would meet the standard floor area ratio requirement of 0.75 would be enormous. This standard would require a 40,000 square foot building on the site. Also, because of the private drive, landscaping and sidewalk requirements and GSI detention, there is no room left on the site for a building this size. We have provided a 9,000 square foot building in a 52,093 square foot lot, resulting in a FAR of 0.17. We are deficient to the standard by 0.58. We believe a building meeting this standard would be infeasibly large and would result in failing all other required standards. We are proposing a building that has a first floor building height of at least 14 feet. We are providing large ground floor windows facing Strong Rd and Lindburg Road, totaling 41% of the façade. We are providing a primary building entrance adjacent to the intersection of Strong Road and Lindburg Road. We are also providing large overhanging soffits around the first floor building area and a canopy in the entry adjacent the surface parking lot and ADA stalls. These building elements we believe will offset the smaller sized building and reduced lot frontage by ensuring that those portions of the building which do occupy the required setbacks along the public street are designed to visually reinforce and support an active and inviting pedestrian environment at the intersection of Strong Road and Lindburg Road, which is the key most prominent and significant street intersection within the Fairview Training Site located in the core of the Village Center area. Therefore, we believe our proposed development equally or better meets the intent of this standard rather than a building that completely covers the site.
- d. Answer (Request for consideration under SRC 803.065 – Alternative street standards: Street Cross Section): The development standard requires a 10' wide multi-use path on one side of the private drive, a 5' wide sidewalk on the other

side of the private drive, and a 7' wide landscape strip on both sides of the private drive separating the walkways from the street. We have met this standard in almost all places of the site with the exception of the small area adjacent the ADA parking and along the north property line. At the parking, there is about a 20' length of sidewalk that does not allow for a 7' wide landscape buffer between the walkway and the private drive street parking. This is to provide a connection from the ADA parking stall to access the sidewalk and building. Along the north side of the property, the retaining wall needs to be offset from the property line due to grades. This moves the 10' walkway further south by about 4', which encroaches on the 7' wide landscape strip. This strip is now reduced to around 3' from the private drive. Because of the site constraints we request to be considered under SRC 803.065 for an alternative street standard.

B. *If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.*

a. Answer: Not applicable, the proposed development is not within a residential zone. Note, the FMU zone this and adjacent properties is within does support residential uses. The proposed development will not detract from the livability and appearance of the area.

C. *If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.*

a. Answer: Yes, the cumulative effect of all of the requested adjustments will result in a project that is still consistent with the intent of the FMU zone. The requested adjustments will not detract from the intent and overall purpose of the zone.

Thank you for taking the time to review our statement. Feel free to reach out if you have any questions.

Sincerely,



Sarah Rose, AIA

Enc.

cc: Lisa Fordyce, lisa@reidsaunders.org
Reid Saunders, reid@reidsaunders.org

REQUEST FOR COMMENTS

Si necesita ayuda para comprender esta información, por favor llame 503-588-6173

REGARDING: Class 3 Site Plan Review / Class 2 Adjustment Case No. SPR-ADJ23-17

PROJECT ADDRESS: 3985 Lindburg Rd SE, Salem OR 97302

AMANDA Application No.: 22-125120-PLN

COMMENT PERIOD ENDS: June 5, 2023

SUMMARY: Proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements.

REQUEST: A Class 3 Site Plan Review application for a proposed new 9,000 square-foot, two-story, office building with associated off-street parking and site improvements; together with a Class 2 Adjustment to the development standards of the Fairview Refinement Plan II refinement plan to:

- 1) Allow the southeast corner of the building to exceed the maximum allowed 20-foot setback abutting Strong Road SE and allow less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line;
- 2) Allow the proposed building to exceed the maximum 20-foot side street setback abutting the private internal street;
- 3) Allow the off-street parking area to the north of the proposed building to be setback less than the minimum required 20-foot setback abutting Lindburg Road SE and the private internal street; and
- 4) Allow the minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

The subject property is approximately 1.2 acres in size, zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and located at 3985 Lindburg Road SE (Marion County Assessor Map and Tax Lot Number: 083W11AB02900).

The Planning Division is interested in hearing from you about the attached proposal. Staff will prepare a Decision that includes consideration of comments received during this comment period. We are interested in receiving pertinent, factual information such as neighborhood association recommendations and comments of affected property owners or residents. The complete case file, including all materials submitted by the applicant and any applicable professional studies such as traffic impact analysis, geologic assessments, and stormwater reports, are available upon request.

Comments received by 5:00 p.m. Monday, June 5, 2023, will be considered in the decision process. Comments received after this date will be not considered. *Comments submitted are public record. This includes any personal information provided in your comment such as name, email, physical address and phone number. Mailed comments can take up to 7 calendar days to arrive at our office. To ensure that your comments are received by the deadline, we recommend that you e-mail your comments to the Case Manager listed below.*

CASE MANAGER: Bryce Bishop, Planner III, City of Salem, Planning Division; 555 Liberty St SE, Room 305, Salem, OR 97301; Phone: 503-540-2399; E-Mail: bbishop@cityofsalem.net.

For information about Planning in Salem, please visit: <http://www.cityofsalem.net/planning>

PLEASE CHECK THE FOLLOWING THAT APPLY:

☐ 1. I have reviewed the proposal and have no objections to it.

☒ 2. I have reviewed the proposal and have the following comments: We disapprove of this proposal. This would be bring too much traffic to our area. There is no room for more cars. The nearby apartments need the space to park. Please do not bring businesses to our quiet residential neighborhood.

Name/Agency: Ryjee Aguilar

Address: 4085 Outward Rd SE Apt 144 Salem OR 97302

Phone: 503-385-3300

Email: smartyckirt24@gmail.com

Date: 05/29/23

IMPORTANT: IF YOU MAIL COMMENTS, PLEASE FOLD AND RETURN THIS POSTAGE-PAID FORM

Bryce Bishop

From: Random Davis <random.davis@gmail.com>
Sent: Monday, June 5, 2023 11:30 AM
To: Bryce Bishop
Subject: Objection to Class 3 Site Plan Review / Class 2 Adjustment Case No. SPR-ADJ23-17 and Recommendations for Alternate Possibilities

Dear Mr. Bishop,

I am writing to express my strong objections to the proposed development detailed in the notice for Class 3 Site Plan Review / Class 2 Adjustment (Case No. SPR-ADJ23-17) for the property located at 3985 Lindburg Rd SE, Salem, OR, 97302. As a resident and homeowner living within a minute walk of the proposed site, I am deeply concerned about the potential negative impacts this development may have on our neighborhood, considering both the historical context of the site and the nature of the organization proposing the development.

The organization, which is primarily focused on aggressive evangelism such as "saturating communities" through festivals and events to spread their religious message, does not seem to be an appropriate choice for the development of this historically significant site. The land had previously housed an institution where individuals with mental health issues were held, which carries a complex history within our community. Considering the historical significance, it is disconcerting to contemplate how the organization's primary objectives fail to demonstrate sensitivity to the past and the surrounding community.

Regarding the requested Class 2 adjustments, I strongly believe that the justifications for these deviations are insufficient when considering the inherent purpose of the existing development standards:

1. Maximum allowed 20-foot setback abutting Strong Road SE and less than 70 percent of the lot frontage of the property abutting Strong Road SE and Lindburg Road SE occupied by buildings placed at the minimum setback line.

This restriction is intended to maintain consistency in landscape and neighborhood aesthetics. Allowing this deviation could disrupt these aesthetics and create a negative visual impact.

2. Maximum 20-foot side street setback abutting the private internal street.

This regulation helps to maintain adequate buffer zones between structures to ensure appropriate distances for fire safety concerns, natural light, and air circulation. Deviating from this standard could compromise these crucial concerns.

3. Minimum required 20-foot setback for the off-street parking area to the north of the proposed building abutting Lindburg Road SE and the private internal street.

This standard aims to minimize traffic congestion and maintain pedestrian safety.

If the proposed development is allowed to deviate from this setback, the safety of pedestrians and the flow of traffic might be compromised.

4. Minimum required floor-area-ratio (lot coverage) of the development to be less than 0.75 FAR.

This regulation ensures a balance between built-up areas and open spaces, as well as adequate provision for stormwater management and green spaces.

Allowing this deviation could result in environmental imbalances and increase the risk of flooding.

It is also clear that the proposed development's proximity to residential areas serves as a blatant infringement on the quality of life for residents living nearby. The organization's preference for large gatherings and festivals raises concerns about noise, privacy, and traffic patterns. Such activities could be invasive to the surrounding residents, highlighting the inadequacy of current zoning laws in providing protection and maintaining environmental quality for local residents.

I would also like to mention that every single neighbor and resident in the vicinity that I have discussed this proposal with has expressed shock and disbelief at the nature of the plans, the nature of the organization, the possibility of proselytization, the potential noisiness, and especially the extremely short notice provided for the comment period. This brief notice appears almost as a borderline insult and an afterthought, which further emphasizes the lack of consideration and respect for our community's input and the consequences that this development could impose on our neighborhood. Such unanimous concern should serve as a clear indication that a more thoughtful and inclusive approach must be (and should have been) taken in addressing the future of this site.

I strongly urge you to reconsider the current proposal and explore alternative possibilities that better respect the site's past and offer long-term benefits to our community while maintaining a high standard of living for the nearby residents. Rather than constructing an unrelated office building for an organization with aggressive evangelism tactics, the land could be better used for projects such as, say, preserving the history of the site by establishing a memorial, park, or community center with educational resources about mental health and the social history of the institution. Or even providing mental health resources and support services for the community, which would honor the site's legacy and promote learning and growth in our understanding of mental health care.

The very fact that the proposal has progressed to this stage seems tone-deaf to the concerns of local residents and the sensitivity required due to historical context. I strongly urge you to contemplate the implications of this development on the neighborhood and make a decision that respects the site's past and the interests of the community.

Thank you for taking the time to review our concerns, and please do not hesitate to reach out for further discussion or input.

Sincerely,

Random Davis
2087 Legacy Heights Drive SE, Salem, OR 97302
random.davis@gmail.com
(503) 515-1798

Bryce Bishop

From: nikki evans <novkid@yahoo.com>
Sent: Wednesday, May 24, 2023 7:43 PM
To: Bryce Bishop
Subject: Class 3 Site

Hello, I'm a current tenant at the grove apartments. I recently received a "notice of filing" request in the mail. I'd like to share my input. I, as well as other tenants in my household thoroughly enjoy the current empty space that you are requesting to build on. It is a very nice empty field with lots of walking trails, wildlife and nature sounds. My favorite thing about these apartments is how quiet and isolated the streets are. Please do not fill the land here as it would eliminate all these amazing qualities.

Sent from my iPhone

Bryce Bishop, Planner III
City of Salem Planning Division
555 Liberty St. SE Room 305
Salem OR 97301
bbishop@cityofsalem.net

Dear City of Salem Planning Division,

I am writing to object to the proposed variance on parking requirements for the new construction planned at 3985 Lindburg Rs SE, Salem OR 97302. As a resident of the Legacy Heights neighborhood adjacent to this site, I am concerned that reducing the amount of parking required will exacerbate an already difficult parking situation for local residents.

Our neighborhood currently experiences a significant deficiency in the provision of sufficient street parking along Lindburg Rd due to 435 apartment units without sufficient parking. Moreover, the situation exacerbates when vehicles are parked on both sides of Lindburg road, resulting in limited visibility at the curve located at the intersection of Lindburg Rd and Shall St. Consequently, I have personally observed instances where vehicles have been forced to suddenly come to a complete halt in order to prevent collisions with the parked cars.


I am also concerned with adjustment number 3 of allowing the off street parking area to the north to be setback less than the 20-foot setback required. This will create an unpleasant view from the street which is inconsistent with the existing neighborhood and negatively impact the walking experience for pedestrians.

Sincerely,
Evelyn Salinas



MEMO

TO: Bryce Bishop, Planner II
Community Development Department

FROM: Laurel Christian, Planner II
Public Works Department 

DATE: June 26, 2023

**SUBJECT: PUBLIC WORKS RECOMMENDATIONS
SPR-ADJ-DAP 23-17 (22-125120)
3985 LINDBURG ROAD SE
NEW OFFICE BUILDING**

PROPOSAL

A Class 3 Site Plan Review application for a proposed new 9,000-square-foot, two-story office building with associated off-street parking and site improvements; together with a Class 2 Adjustment to the development standards of the Fairview Refinement Plan II. The subject property is approximately 1.2 acres in size, zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and located at 3985 Lindburg Road SE (Marion County Assessor Map and Tax Lot Number: 083W11AB02900).

RECOMMENDED CONDITIONS OF APPROVAL

1. The applicant shall coordinate with Cherriots to locate and construct a transit stop conforming to applicable Salem Area Mass Transit District Standards and the Public Works Design Standards (PWDS) on Strong Road SE.
2. Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

FACTS

Streets

1. Lindburg Road SE
 - a. Standard This street is designated as a collector street in the Salem TSP and the Fairview Refinement Plan. The standard for this street classification is a 28-to36-foot-wide improvement within a 60-foot-wide right-of-way.

Code authority references are abbreviated in this document as follows: *Salem Revised Code* (SRC); *Public Works Design Standards* (PWDS); *Salem Transportation System Plan* (Salem TSP); and *Stormwater Management Plan* (SMP).

- b. Existing Conditions This street has an approximate 35-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

2. Strong Road SE

- a. Standard This street is designated as a collector street in the Salem TSP and the Fairview Refinement Plan. The standard for this street classification is a 28-to36-foot-wide improvement within a 60-foot-wide right-of-way.
- b. Existing Conditions This street has an approximate 35-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

Storm Drainage

1. Existing Conditions

- a. A 10-inch storm main is located on southwest side of the property within an easement.
- b. An 18-inch storm main is located in Strong Road SE.
- c. There are 12-inch and 48-inch storm mains located in Lindburg Road SE.

Water

1. Existing Conditions

- a. The subject property is located in the S-1 water service level.
- b. A 10-inch water main is located in Strong Road SE. Mains of this size generally convey flows of 1,500 to 3,400 gallons per minute.
- c. A 12-inch water main is located in Lindburg Road SE. Mains of this size generally convey flows of 2,100 to 4,900 gallons per minute.
- d. A 4-inch water main is located on the subject property in an easement. Mains of this size generally convey flows of 120 to 300 gallons per minute.

Sanitary Sewer

1. Existing Conditions

- a. An 8-inch sewer main is located in Strong Road SE.

- b. An 8-inch sewer main is located on the subject property in an easement.

CRITERIA AND FINDINGS—SITE PLAN REVIEW

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

Criteria: SRC 220.005(f)(3)(A) The application meets all applicable standards of the UDC (*Unified Development Code*)

Finding—The subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain; 802 – Public Improvements; 803 – Streets and Right-of-Way Improvements; 804 – Driveway Approaches; 805 – Vision Clearance; 809 – Wetlands; and 810 – Landslides.

Public Works staff has reviewed the Flood Insurance Study and Flood Insurance Rate Maps and has determined that no floodplain or floodway areas exist on the subject property.

According to the Salem-Keizer Local Wetland Inventory (LWI), the subject property does not contain any wetland areas or hydric soils.

According to the City's adopted landslide hazard susceptibility maps and SRC Chapter 810 (Landslide Hazards), there are no mapped landslide hazard areas on the subject property.

Criteria: SRC 220.005(f)(3)(B) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately

Finding—Lindburg Road SE and Strong Road SE are fully developed streets and meet the right-of-way width and pavement width standards pursuant to the Salem TSP and the Fairview Refinement Plan; therefore, no additional street improvements are required as a condition of the proposed development.

Cherriots submitted comments indicating that a new transit stop is needed in coordination with the proposed street improvements along Strong Road SE. The applicant shall be required to provide the bus stop as part of the development pursuant to SRC 803.035(r). The applicant shall coordinate with Cherriots to determine the appropriate type of stop and location.

Condition: The applicant shall coordinate with Cherriots to locate and construct

a transit stop conforming to applicable Salem Area Mass Transit District Standards and the Public Works Design Standards (PWDS) on Strong Road SE.

Criteria: SRC 220.005(f)(3)(C) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians

Finding—The site is served by two existing driveways; no changes to existing accesses are proposed. The driveway accesses onto Lindburg Road SE and Strong Road SE provide for safe turning movements into and out of the property and do not warrant modification.

Criteria: SRC 220.005(f)(3)(D) The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development

Finding—The subject property is located outside of the Urban Service Area. Because the proposed development does not precede construction of required facilities, an Urban Growth Area Development permit is not required. The Public Works Department has reviewed the applicant's preliminary plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that a sewer monitoring manhole may be required, and the trash area shall be designed in compliance with PWDS.

The applicant's engineer submitted a stormwater report as part of the application package. Public Works has reviewed the stormwater report and determined that the report does not demonstrate that design provides treatment and flow control for the developed site to meet the requirements of PWDS and SRC 71. A Design Exception has not been requested or approved for the stormwater design. At time of Building Permit Review, the applicant shall be required to demonstrate the stormwater system is designed in compliance with SRC Chapter 71 and PWDS, and at time of development, the stormwater system shall be constructed.

Condition: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Prepared by: Laurel Christian, Planner II
cc: File



RESPONSE TO REQUEST FOR COMMENTS

DATE: 06/01/2023

CASE/APP NUMBER: SPR-ADJ23-17

PROPERTY LOCATION: 3985 Lindburg Rd SE

CASE MANAGER: Bryce Bishop, Planner III, City of Salem

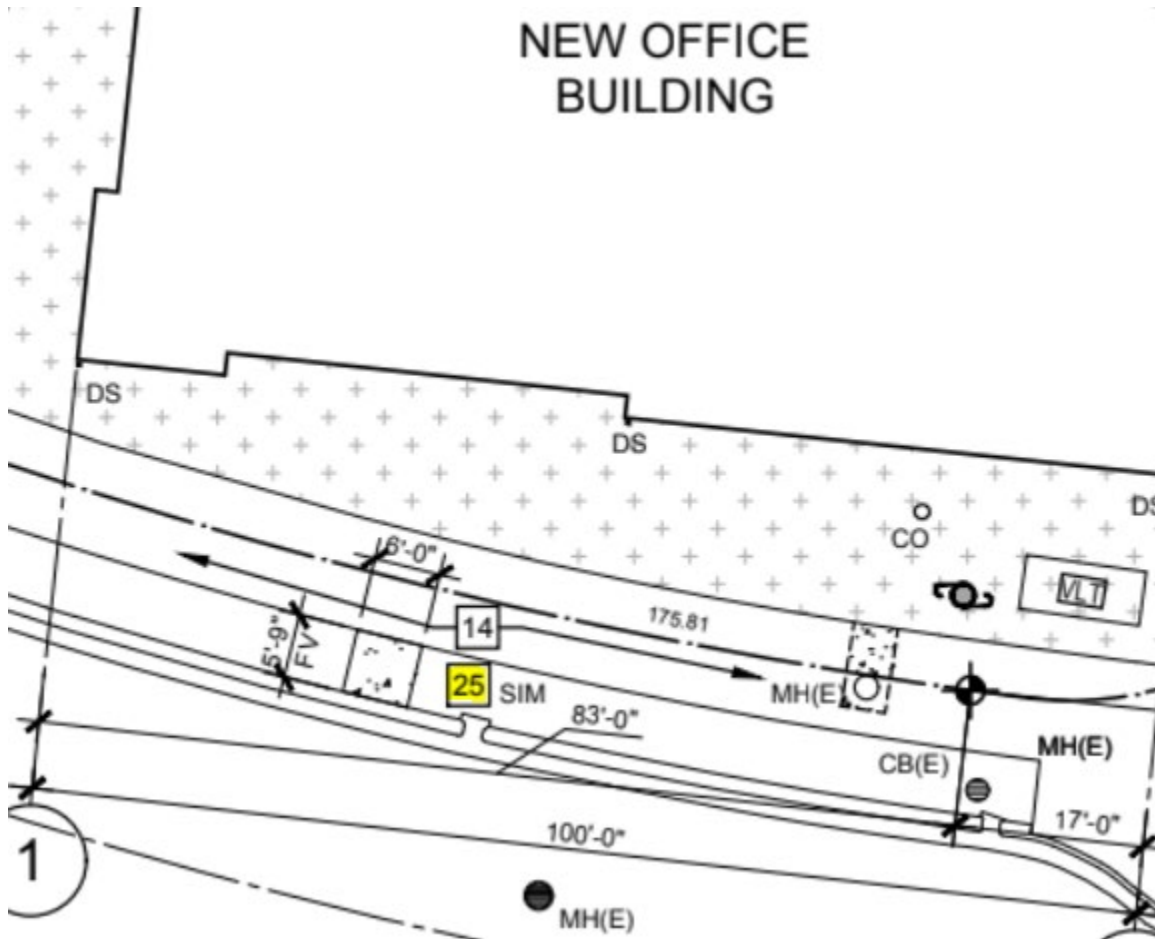
Email: bbishop@cityofsalem.net

COMMENTS FROM: Jolynn Franke, Transit Planner I, Cherriots

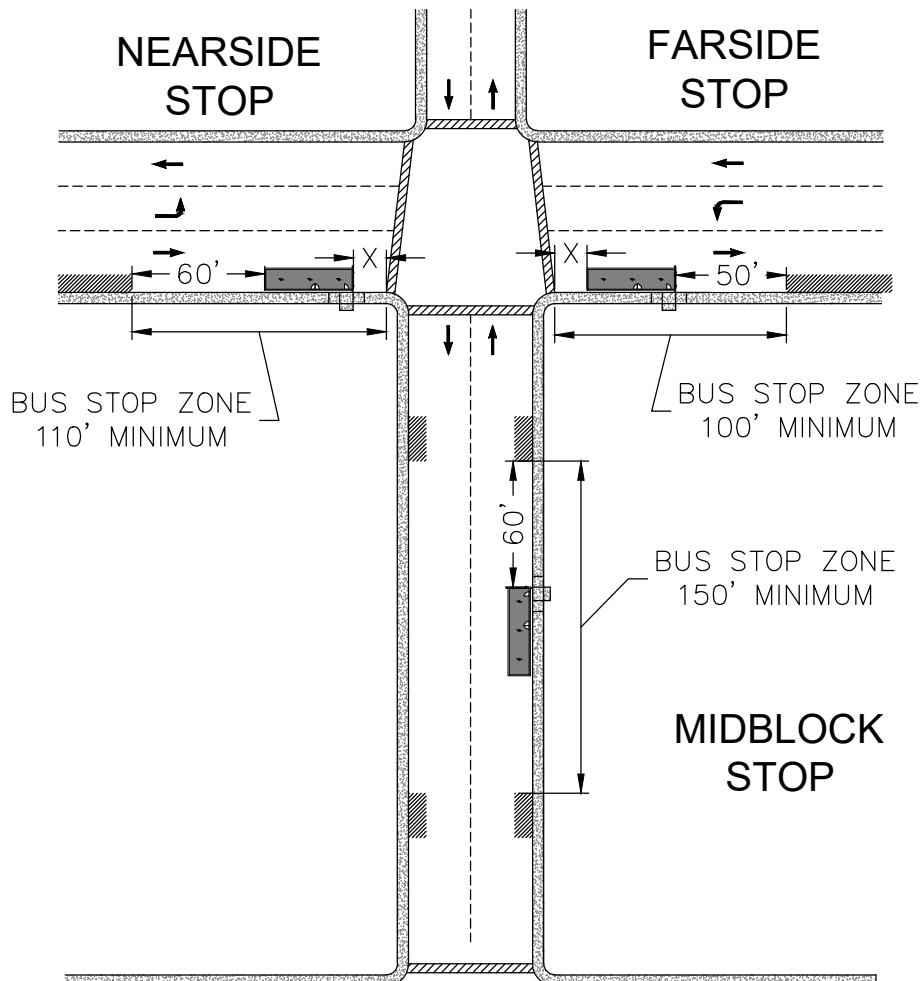
Email: planning@cherriots.org

COMMENTS: A transit stop has been identified as needed in connection with this proposed development. Cherriots requests a transit stop conforming to the applicable standards of the Salem Area Mass Transit District to be constructed and right-of-way dedication, if necessary, to be provided as part of this development. On-street parking shall be restricted in the area of the transit stop in order to ensure unobstructed access by transit. The minimum bus stop zone required is 100 feet as shown in the attached drawing - No Parking Zones at Intersections Detail (C8). The type, size, and location of the transit stop shall be as depicted in Reference Note 25 on Sheet A1.1 of the Site Plan included in the Notice of Filing distributed on May 22, 2023 (see screenshots on following page). AC+CO Architecture Community was provided with examples of swale bridges in an email from Cherriots staff on February 16, 2023 (also attached). Cherriots requests that final design of the transit stop be submitted to Cherriots staff at planning@cherriots.org for approval prior to construction.

NEW OFFICE BUILDING



- 25** NEW LOCATION FOR BUS TRANSIT STOP, AT SIM CONDITION
PROVIDE A 6'-0" WIDE BRIDGE OVER SWALE OF PVIOUS
MATERIAL

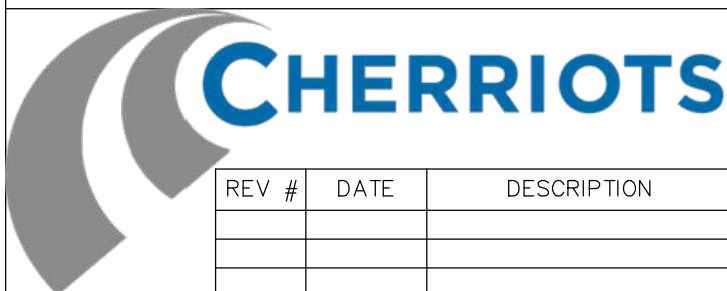


NOTES:

1. CHERRIOTS TO PROVIDE INFORMATION REGARDING BUS TYPE, LENGTH, AND QUANTITY OF BUSES TO BE SERVICED BY BUS STOP.
2. FOR MULTIPLE BUSES BEING SERVED AT ONE STOP:
 - 2.A. ADD 50 FEET FOR EACH ADDITIONAL STANDARD 40-FOOT BUS.
 - 2.B. ADD 70 FEET FOR EACH ADDITIONAL 60-FOOT ARTICULATED BUS.
3. BUS STOP ZONE SHALL BE SIGNED AS A NO PARKING ZONE PER STANDARDS OF LOCAL JURISDICTION.
4. X 10 MINIMUM FROM EDGE OF CROSSWALK OR END OF RADIUS, WHICHEVER IS FURTHER FROM THE INTERSECTION.

MINIMUM DISTANCE BETWEEN A BUS STOP AND LEFT TURN (FT)

POSTED SPEED LIMIT	LANE CHANGES			
	1	2	3	4
30 MPH OR LESS	430	610	790	970
35 MPH	625	875	1125	1375
40 MPH	780	1080	1380	1680
45 MPH	1080	1430	1780	2130
50 MPH	1415	1865	2135	2765
55 MPH	1830	2380	2930	3480



**NO PARKING ZONES
AT INTERSECTIONS
DETAIL**

C8

REV #	DATE	DESCRIPTION

BY EGW

DATE 09/22/22

CHECKED RDV

DATE 09/22/22





Peter & Margaret de Weert Center

Office Building

Room 100



Attachment I

ELEVATION REFERENCE NOTES:

- 1

OFFICI SIGNAGE, COORDINATE SIZE AND LOCATION WITH OWNER
- 2

WINDOW PER PLAN
- 3

STEEL CANOPY, PAINT, REFER TO STRUCTURAL
- 4

FIBER CEMENT TRIM, PAINT
- 5

HORIZONTAL FIBER CEMENT LAP SIDING, PAINT
- 6

STANDING SEAM METAL ROOF
- 7

FIBER CEMENT FASCIA, PAINT
- 8

COLUMN PER STRUCTURAL, PAINT

9

STONE VENEER, STEP WITH GRADE

10

STONE SILL

11

PRE-FINISHED PARAPET FLASHING CAP FLASHING

12

DOOR PER PLAN

13

PRE-FINISHED SHEET METAL GUTTER AND DOWNSPOUT, AT SIM CONDITION, DAYLIGHT ONTO LOWER ROOF

14

METAL BUILDING ADDRESS NUMBER, 12" HEIGHT, REFER TO SPECIFICATIONS

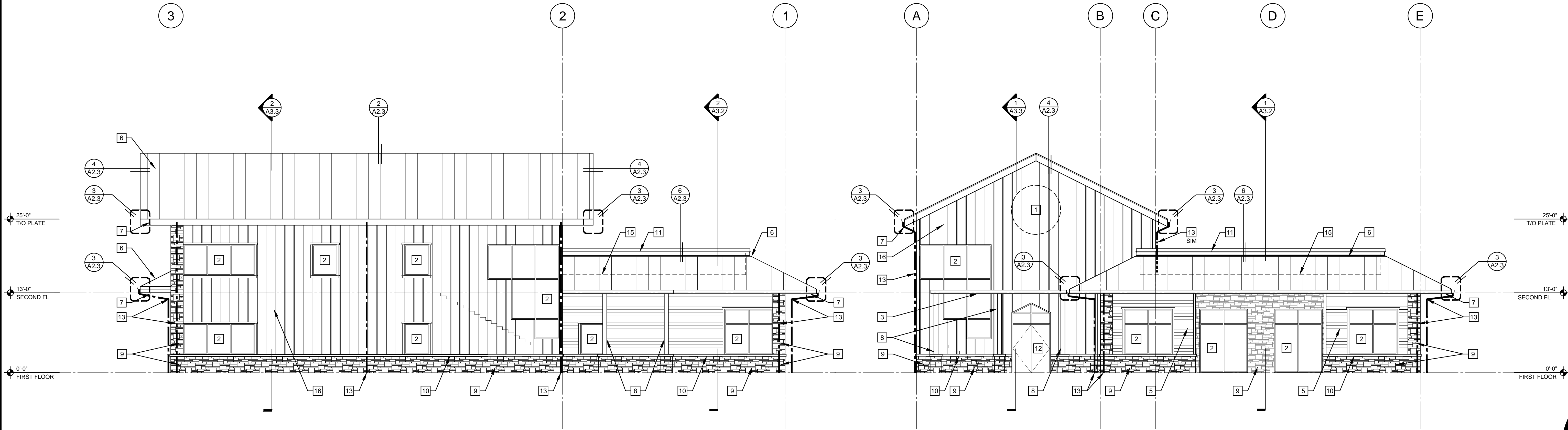
15

PARAPET BEYOND

16

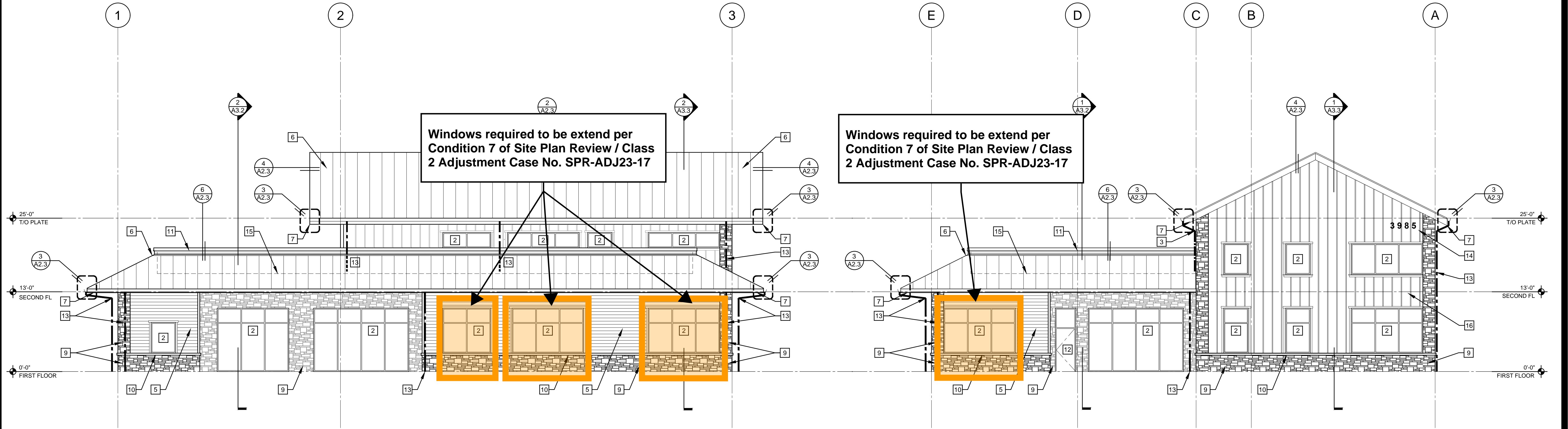
VERTICAL BOARD AND BATTEN SIDING, PAINT

3. DRAWINGS ARE UNLESS OTHERWISE SPECIFIED, AND SHOULD NOT BE SCALED. THE ARCHITECT ASSUMES RESPONSIBILITY FOR ANY DISCREPANCIES OR QUESTIONABLE DIMENSIONS PRIOR TO PROCEEDING WITH AREA OF QUESTIONABLE WORK.
4. IN CASE OF ANY CONFLICTS IN THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, THE CONTRACTOR IS REQUIRED TO INCLUDE THE BETTER QUALITY AND LARGER QUANTITY OF THE WORK.
5. CONTRACTOR AND BIDDERS SHALL USE COMPLETE SETS OF CONTRACT DOCUMENTS. NEITHER THE OWNER NOR ARCHITECT ASSUMES RESPONSIBILITY FOR ERRORS OR MISINTERPRETATIONS RESULTING FROM THE USE OF INCOMPLETE SETS OF CONTRACT DOCUMENTS.
6. COORDINATE AND PROVIDE WOOD BACKING FOR ALL OFFICI ITEMS



1 NORTH EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"

2 WEST EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"



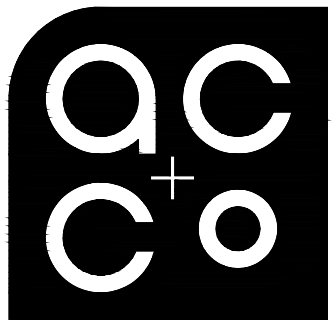
3 SOUTH EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"

4 EAST EXTERIOR ELEVATION
SCALE: 1/8" = 1'-0"

CONSTRUCTION DOCUMENTS
NOT FOR CONSTRUCTION
AC+CO ARCHITECTURE | COMMUNITY
JAN 30, 2023

In the event conflicts are discovered between the original signed and sealed documents prepared by the Architects and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

JOB NO. 2022.0003
DATE JAN 30, 2023
DRAWN CJA
REVISIONS



ARCHITECTURE
COMMUNITY
1100 Liberty St SE, Suite
200
Salem, OR 97302-5385
P: 503.581.4114
www.accoac.com

REID
SAUNDERS

3985 LINDBURG RD SE
SALEM, OREGON 97302

SHEET

A3.1