

Coburn Apartments

Adjustment Class-2 Application

Proposal:

The subject property is 8.60 acres in size, zoned RMII, and located east of Battlecreek Road (08 3W 11D/Tax Lot 601). The subject property is Parcel 1 of recorded Plat P.P. 2019-036.

There are required streets that run through the development. However, the entire development will be owned by the same owner and will share all amenities.

The applicant is proposing a development consisting of 200-apartment units as shown on the site plans.

The applicant is requesting an adjustment greater than 20% adjustment to Sec. 702.020(e)(4) Façade and building design:

“(4) On sites with 75 feet or more of buildable width, a minimum of 40 percent of the buildable width shall be occupied by building placed at the setback line to enhance visual interest and activity along the street. Accessory structures shall not apply towards meeting the required percentage.”

Adjustment Criteria-SRC 250.005(d)(2) Criteria:

(A) The purpose underlying the specific development standard proposed for adjustment is:

- (i) Clearly inapplicable to the proposed development; or***
- (ii) Equally or better met by the proposed development.***

(B) If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.

(C) If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.

Applicant Findings:

- (A) The purpose of this requirement is to provide a pedestrian friendly development with buildings located close to the sidewalks instead of parking areas.***

Cordon Road

The subject property has 685 feet of buildable width (this excludes required side setbacks) along Cordon Road. Code requires a minimum of 40% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, due to the Cordon Road being a high traffic road and a required 10-foot PUE, the buildings are not set within required 5-foot setback. Therefore, occupying 0% of the buildable width of street frontage along Boone Road. Due to the location and need for the detention pond along Cordon Road, additional buildings could not be provided along the setback line of Cordon Road.

A Street (East Side)

The subject property has 1065 feet of buildable width (this excludes required side setbacks and driveway) along A Street. Buildings 1, 2, 3, 4, 5, 6, and 11 are located within 20 feet of the east side A Street. Code requires a minimum of 40% of the buildable width be occupied by buildings. As shown on the site plan, due to the required 10-foot PUE, the buildings are not set within required 5 foot setback. However, as shown on the site plan, the buildings are setback 20 feet and total 615 feet of the buildable width along the street frontage. Therefore, occupying 58% of the buildable width of street frontage along A Street.

A Street (West Side)

The subject property has 1065 feet of buildable width (this excludes required side setbacks and driveway) along A Street. Buildings 29, 32, 33, 34, 35, and 36 are located within 20 feet of the east side A Street. As shown on the site plan, due to the required 10-foot PUE, the buildings are not set within required 5 foot setback. However, as shown on the site plan, the buildings are setback 20 feet and total 660 feet of the buildable width along the street frontage. Code requires a minimum of 40% of the buildable width be occupied by buildings. Therefore, occupying 61% of the buildable width of street frontage along A Street.

All buildings within the development are located 20 feet from the street and have direct pedestrian access onto sidewalks. Proposed and existing sidewalks will further enhance the pedestrian connections and circulation to and from the site. Therefore, creating a pedestrian friendly development.

(B) The apartment development will provide additional landscaped areas throughout the site along with pedestrian paths/sidewalks through, along with visual appealing buildings. All of which will create a pedestrian friendly development. The reduction of buildings along the setback line will have no effect on the proposed use or surrounding uses.

(C) Only one adjustment is being requested for this proposal.

Hawks Ridge Apartments-Phase 3

Conditional Use

Applicant:

KCH Enterprises, LLC
10355 Liberty Road S
Salem, Oregon 97306

Representative:

Multi/Tech Engineering
Brandie Dalton, Land-Use Consultant
1155 SE 13th Street
Salem, Oregon 97302
(503) 363-9227
bdalton@mtengineering.net

SITE HISTORY:

The site is located at the NW corner of the intersection of Highway 22 and Cordon Road SE (082W05/200, 300, 400, 401, and 500). The subject property is zoned IC (Industrial Commercial). The subject property is designated as "Industrial Commercial" on the Salem Area Comprehensive Plan (SACP) Map.

In 2012, an Urban Growth Area Permit (UGA12-01), was issued to determine the public facilities required to develop an apartment complex and industrial and commercial development on property approximately 45.67 acres in size located at the northwest corner of the intersection of Cordon Road SE and Highway 22. The subject property was part of this UGA permit.

On January 10, 2013, CPC-ZC-PAR12-08 was approved. The CPC-ZC-PAR12-08 approval was a consolidated application to change the comprehensive plan designation from Industrial to Industrial Commercial and Multi-Family Residential and change the zone district from IBC (Industrial Business Campus) and IP (Industrial Park) to IC (Industrial Commercial) and RM-II Multiple Family Residential) and a 3-parcel partition on property approximately 45.42 acres in size and located at the northwest corner of the intersection of Highway 22 and Cordon Road SE. The subject property was part of the CPC-ZC-PAR12-08 approval.

The City held a pre-application conference with the applicant on August 30, 2021, for the purpose of discussing redevelopment of the subject property (PRE-AP21-92).

A Design Review Pre-App was held on January 20, 2021 (PRE-AP22-03), to discuss the development of the subject property as apartments.

PROPOSAL:

Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.

The subject property is 23.3 acres in size and zoned IC (Commercial Retail). The applicant is applying for a Conditional Use for the construction of a 396-unit apartment complex.

CONDITIONAL USE CRITERIA:

SRC 240.005(d) - An application for a Conditional Use permit shall be granted if all of the following criteria are met:

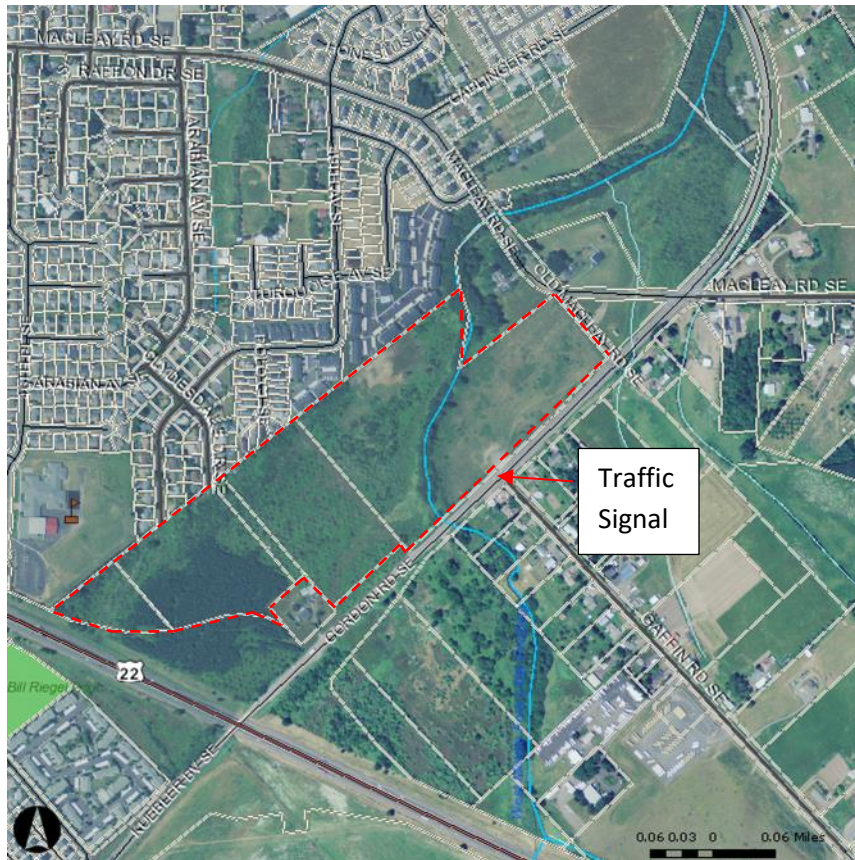
- (1) The proposed use is allowed as a conditional use in the zone;*
- (2) The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions; and*
- (3) The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.*

APPLICANT'S REASONS ADDRESSING CHAPTER 240.005(d):

- (1) Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.
- (2) The proposed apartments will have little to no impact on the neighborhood. The property to the north is zoned RS and has existing single-family dwellings, the property to the east is zoned IC and developed with multi-family dwellings, storage facilities, and a gas station. The proposal for multi-family dwellings on the site will be consistent with the surrounding area.

The proposed apartments will have less an impact on the area then an industrial use that is allowed within the IC zone. The applicant's Traffic Engineer is working on providing an analysis of the site. A Traffic Study is currently being worked on and will be submitted to the City when complete.

The original TIA was done as part of CPC-ZC-PAR12-08 for the subject property and the properties to the east.



In order to mitigate any impacts, the developers were required to put in a signal at Gaffin Road. Furthermore, the intent of the applicant is to mitigate impacts by providing adequate circulation for vehicle, multi-modal / pedestrian impacts to the transportation system.

(3) The subject property is surrounded by the following uses:

North:	RS zoned property; existing single-family dwelling
East:	Across IC zoned property; existing multi-family dwellings, storage units, gas station
South:	Across Cordon Road-Marion County Industrial zone; vacant land
West:	Across Hwy 22-RMII (Multi-Family Residential) and RA (Residential Agriculture) zone; existing apartments and Bill Riegall Park

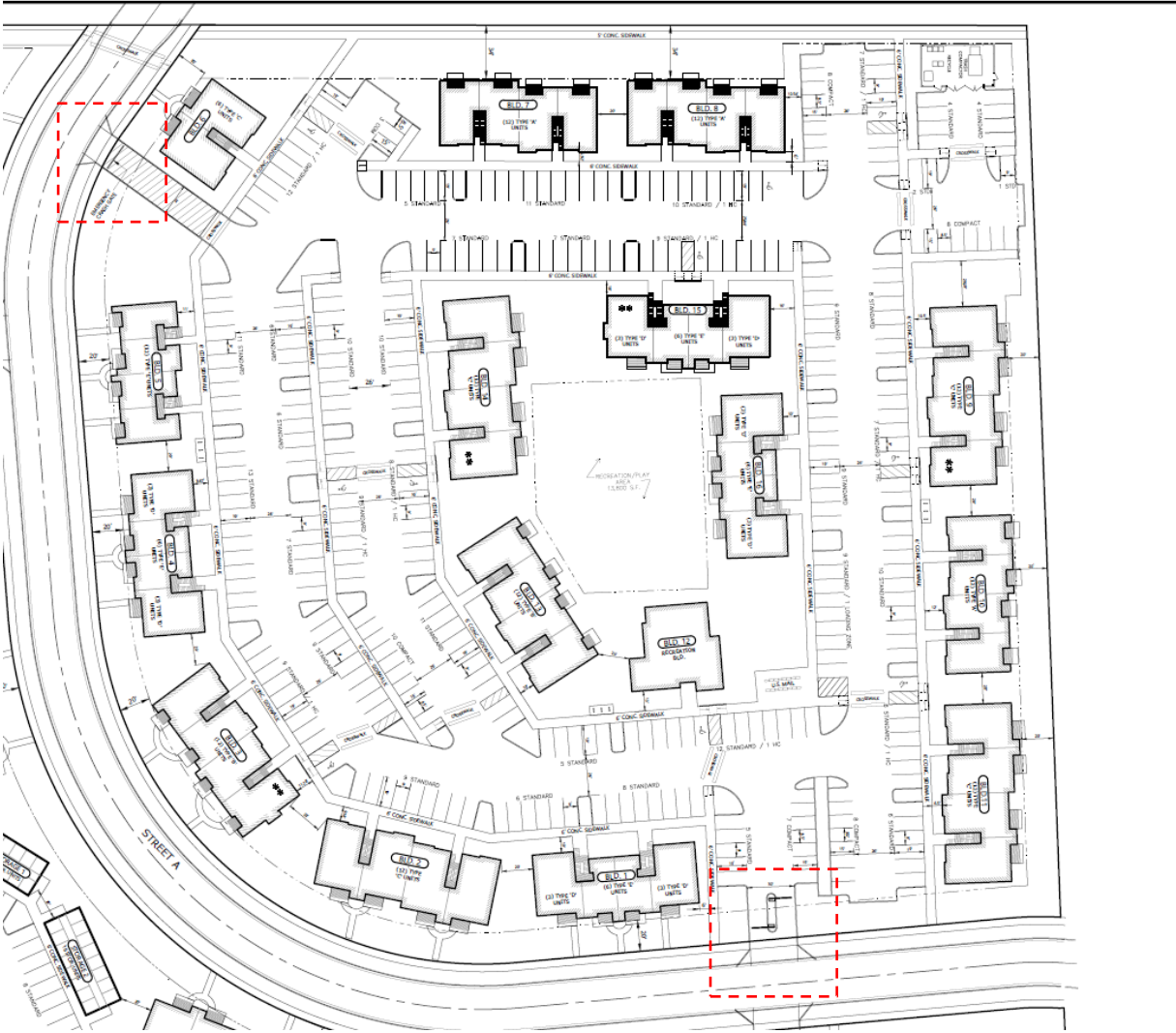
The apartments will be required to go through Site Plan/Design Review, which requires open space and landscaping at a higher percent than what a commercial use would be required to provide. Amenities like landscaped open space will help with the visual appeal of this area and reduce impacts on the neighborhood. The design standards are in place to help ensure compatibility with adjacent uses.

The proposed apartment development will provide pedestrian paths throughout the site and to Clydesdale Road (north), Whitaker Road (east), and Cordon Road (south). The pedestrian paths will provide access and circulation to the surrounding neighborhoods. Therefore, increasing their livability as well as providing needed housing.

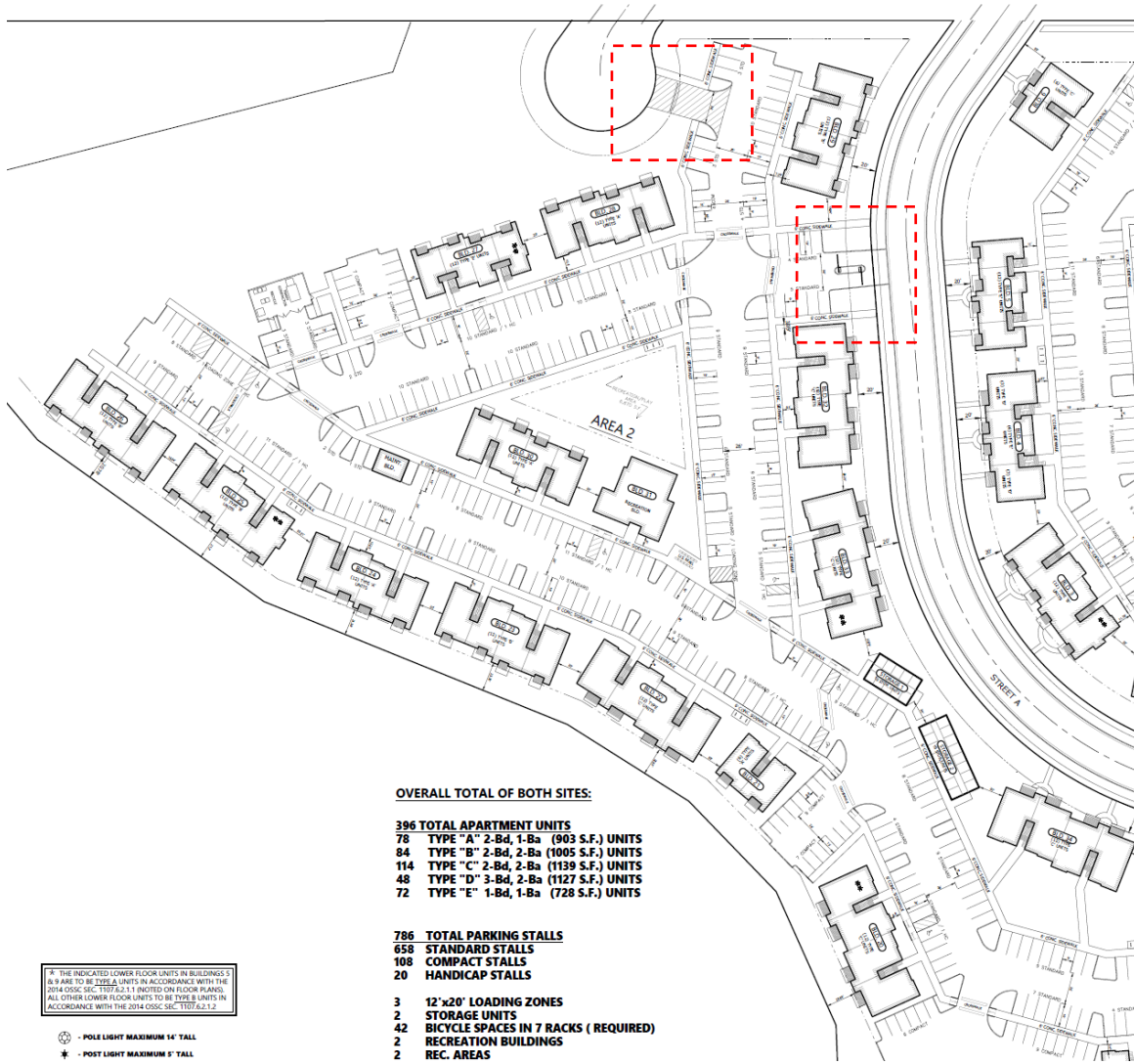
There for this condition will be complied with through the Site Plan/Design Review process.

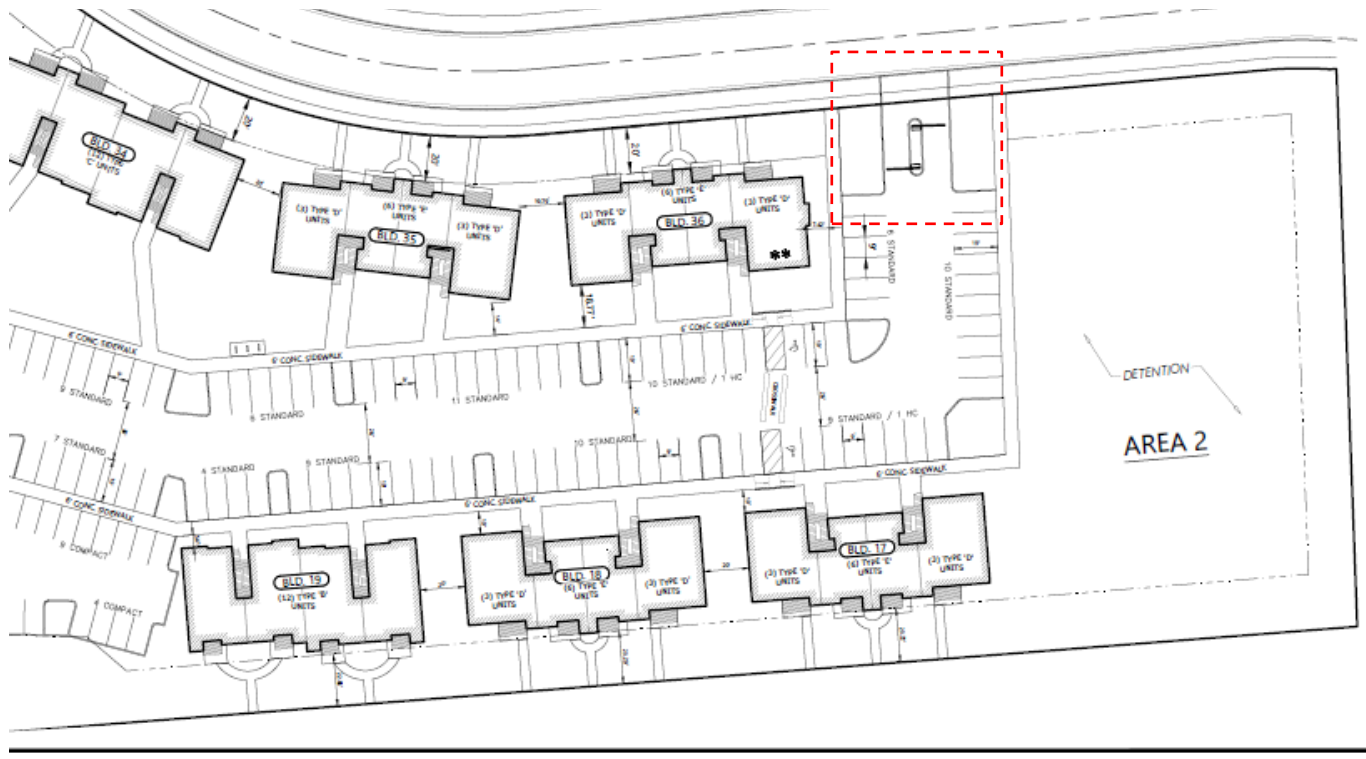
Class 2-Driveway Approach Permit
January 24, 2022

Area 1



Area 2





SRC 804.025 (d) Criteria. A Class 2 Driveway Approach Permit shall be granted if:

(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;

Applicant Response: The subject property is 23.3 acres in size and zoned IC. The site is located at the NW corner of the intersection of Highway 22 and Cordon Road SE (082W05/200, 300, 400, 401, and 500). Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.

The applicant is applying for a Conditional Use, Design Review, and Site Plan Review for the construction of a 396-unit apartment complex.

The required street extension/connection divides the property into Area 1 and Area 2. However, the complex will be developed as one with shared amenities for all 396-units.

Area 1: 174 Apartment Units
Area 2: 222 Apartment Units

The proposed development will have access onto A Street that runs through the property and connects to Whitaker Drive to the east. The development will also have access to the proposed cul-de-sac to the north. A Street and the cul-de-sac are designated as a 'local' streets on the

Salem Transportation System Plan. There are five (5) driveways. One of the driveways located in Area 1 is for emergency access only. All five (5) driveways will meet Public Works design standards. As shown on the site plan the driveways are required for access to the site and are in compliance with design standards.

(2) No site conditions prevent placing the driveway approach in the required location;

Applicant Response: The location of the driveways were taken into consideration prior to laying the site out. Access onto Cordon Road to the south is not allowed, so all access for the development will be taken from A Street and the cul-de-sac. The location of the proposed driveways take into consideration the location of internal streets adjacent to the site and access onto the local streets. Therefore, all factors were taken into consideration and there are no conditions on the site that prevent the driveway approach.

(3) The number of driveway approaches onto an arterial are minimized;

Applicant Response: There are no driveways proposed onto an arterial. Therefore, this criterion is not applicable.

(4) The proposed driveway approach, where possible:

(A) Is shared with an adjacent property; or

(B) Takes access from the lowest classification of street abutting the property;

Applicant Response to (4)(B): A Street is proposed to run through the development to Whitaker Drive to the east. All access will be onto A Street and the cul-de-sac, both of which are classified as local streets. There are four (4) driveways proposed onto A Street and one (1) driveway proposed onto the cul-de-sac. One of the driveways onto A Street is for emergency access only. There are no adjacent driveways to share access with the development. Therefore, there is no lower classified streets abutting the property. Therefore, this criterion has been met.

(5) The proposed driveway approach meets vision clearance standards;

Applicant Response: Through the pre-app process, the applicant has been working with Public Works to ensure that the driveway approach is in the required location and meets vision clearance standards. As shown on the site plan, this criterion has been met.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Applicant Response: The driveway approach does not create traffic hazards. As shown on the site plan, this criterion has been met.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Applicant Response: Public Works has had the opportunity to review the site plan for any adverse impacts. No adverse impacts to the vicinity have been identified. As shown on the site plan, the location of the driveway will not have any impacts on the subject property or adjacent properties. This criterion has been met.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required locations to minimize impacts to adjacent streets and intersections. As shown on the site plan, this criterion has been met.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required location to help balance the adverse impacts to residentially zoned property. The location of the proposed driveways takes into consideration the location of the streets adjacent to the site. As shown on the site plan, this criterion has been met.

Hawk Ridge Apartments

Class 3-Site Plan Review

February 21, 2022

SRC 220.005(f)(3) Class 3 Site Plan Review Criteria:

(A) The application meets all applicable standards of the UDC;

Applicant Findings: The applicant is requesting to meet all Design Review Standards and Guidelines as outline in the submitted application.

The subject property is 23.3 acres in size and zoned IC. The site is located at the NW corner of the intersection of Highway 22 and Cordon Road SE (082W05/200, 300, 400, 401, and 500).

Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.

The applicant is applying for a Conditional Use, Design Review, and Site Plan Review for the construction of a 396-unit apartment complex.

The required street extension/connection divides the property into Area 1 and Area 2. However, the complex will be developed as one with shared amenities for all 396-units.

Area 1: 174 Apartment Units

Area 2: 222 Apartment Units

Adjustments Requested:

SRC 702.020(e)(4) Façade and Building Design

SRC 702.020(e)(5) Façade and Building Design

All applicable standards and guidelines have been outlined below and on the attached site plans.

Industrial Commercial (IC)-SRC Chapter 551

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Therefore, all setback requirements have been met. Setbacks are shown on the tentative plan.

Northeast:	34-foot setback (Buildings 6, 7, 8, and 29); RS zoned-existing single-family dwelling
Northwest:	48 to 165-foot setback (Buildings 26, 27, 28); IC zoned-vacant land
East:	20-foot setback (Buildings 9, 10, 11); RMII zoned-existing apartments
South:	20-foot setback (Buildings 17, 18, 19); Cordon Road
West:	20-foot setback (Buildings 20-26); HWY 22 right-of-way
A Street:	20-foot setback (Buildings 1, 2, 3, 4, 5, 6, 11, 29, 32, 33, 34, 35, 36)

Maximum Height: Maximum building height allowed in the IC zone is 70'. All proposed buildings are in compliance with the requirements of the Code.

*Building 1 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

*Building 2 (Type C Units) is 34.8 feet in height (measured to the highest point)

*Building 3 (Type B Units) is 38.8 feet in height (measured to the highest point)

*Building 4 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

*Building 5 (Type E Units) is 37.6 feet in height (measured to the highest point)

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*Building 8 (Type A Units) is 38.2 feet in height (measured to the highest point)

*Building 9 (Type C Units) is 38.4 feet in height (measured to the highest point)

*Building 10 (Type A Units) is 38.2 feet in height (measured to the highest point)

*Building 11 (Type C Units) is 34.8 feet in height (measured to the highest point)

*Building 12 (Recreation Building) is 23.2 feet in height (measured to the highest point)

*Building 13 (Type B Units) is 38.8 feet in height (measured to the highest point)

*Building 14 (Type C Units) is 38.4 feet in height (measured to the highest point)

*Building 15 (Type D Units) is 37.10 feet in height (measured to the highest point)

*Building 16 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

*Building 17 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

*Building 18 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

*Building 19 (Type B Units) is 38.8 feet in height (measured to the highest point)

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*Building 21 (Type A Units) is 38.2 feet in height (measured to the highest point)

*Building 22 (Type C Units) is 34.8 feet in height (measured to the highest point)

*Building 23 (Type B Units) is 38.8 feet in height (measured to the highest point)

- *Building 24 (Type A Units) is 38.2 feet in height (measured to the highest point).
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- *Building 34 (Type C Units) is 34.8 feet in height (measured to the highest point)
- *Building 35 (Type D and E Units) is 37.10 feet in height (measured to the highest point)
- *Building 36 (Type D Units) is 37.10 feet in height (measured to the highest point)
- *Storage Buildings (2 Buildings) are 13.8 feet in height (measured to the roof line)

Therefore, the buildings are in compliance with the building height requirement.

Stormwater: As shown on the Grading and Drainage Plan, the proposal is treating at least 80% hard surface with Green Water Infrastructure. A Preliminary Drainage Report is currently being worked on and will be submitted when complete. Therefore, meeting the requirements of the Public Works Department.

(B) *The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;*

Applicant Findings: The subject property has street frontage on Cordon Road (south), A Street that runs through the site (connects to Whitaker Drive to the east of the site). A Traffic Impact Analysis (TIA) is currently being worked on and will be submitted to the City when complete.

As shown on the site plan, safe and efficient access and circulation has been provided into and throughout the development. The proposed development has 26-foot-wide driveways throughout the site. The driveways provide circulation throughout the site and onto the surrounding street system.

The design of on-site circulation is clearly identifiable, safe, pedestrian friendly and interconnected.

The subject property is located in a developing area where improved streets and sidewalks continue as required by the City. Improved access is required by code. Approval does not adversely affect the safe and healthful development of any adjoining land or access thereto.

(C) *Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and*

Applicant Findings: The development is for a 396-unit apartment complex. Code requires 1 vehicle parking spaces per every studio or 1-bedroom dwelling unit and 1.5 vehicle parking spaces per every 2 or more bedrooms. The applicant is required to provide a minimum of 558 on-site vehicle parking spaces and is allowed a maximum of 961 on-site vehicle parking spaces. As shown on the site plan, 777 on-site parking spaces are being provided throughout the development.

Area 1: 243 parking spaces required

291 Standard Parking Stalls

44 Compact Parking Stalls

12 Handicap Parking Stalls

347 Total Parking Stalls Provided

Area 2: 315 parking spaces required

352 Standard Parking Stalls

64 Compact Parking Stalls

14 Handicap Parking Stalls

430 Total Parking Stalls Provided

Adequate parking has been provided throughout the development with 1.96 parking spaces per dwelling unit. Loading spaces have been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Three (3) loadings spaces have been provided throughout the site.

Bike Parking: Bike racks have been provided on the site and located in a convenient location for the residents.

Bicycle parking is also required on site. The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 42 bicycle parking spaces have been provided on-site.

Area 1: (18 required)

18 Bicycle Spaces

Area 2: (22 required)

24 Bicycle Spaces

The design of pedestrian circulation systems shall provide clear and identifiable connections within the multiple family development and to adjacent uses and public streets/sidewalks. The proposed development provides safe and convenient bicycle and pedestrian access from within the development to adjacent residential areas. Therefore, this standard has been met.

(D) *The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.*

Applicant Findings: Utility plans have been provided that show how the site will be served with City water, sewer, storm water facilities, and other utilities appropriate to the development.

Stormwater: As shown on the Grading and Drainage Plan, the proposal is treating at least 80% hard surface with Green Water Infrastructure. A Preliminary Drainage Report is currently being worked on and will be submitted to the City when complete. Therefore, meeting the requirements of the Public Works Department.

Hawks Ridge 3-Apartments

Design Review

February 23, 2022

Applicant:

KCH Enterprises, LLC
10355 Liberty Road S
Salem, Oregon 97306

Representative:

Multi/Tech Engineering
Brandie Dalton, Land-Use Consultant
1155 SE 13th Street
Salem, Oregon 97302
(503) 363-9227
bdalton@mtengineering.net

Site History:

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The City held a pre-application conference with the applicant on August 30, 2021, for the purpose of discussing redevelopment of the subject property (PRE-AP21-92).

A Design Review Pre-App was held on January 20, 2021 (PRE-AP22-03), to discuss the development of the subject property as apartments.

Proposal:

The following statement addresses the applicable Design Review Standards in the SRC Chapter 702 (Multiple Family Design Review) and the requirements under the IC Zone District. Information provided on the site plans for the Design Review application further address applicable code requirements.

The subject property is 23.3 acres in size and zoned IC. The site is located at the NW corner of the intersection of Highway 22 and Cordon Road SE (082W05/200, 300, 400, 401, and 500). Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.

The applicant is applying for a Conditional Use, Design Review, and Site Plan Review for the construction of a 396-unit apartment complex.

The required street extension/connection divides the property into Area 1 and Area 2. However, the complex will be developed as one with shared amenities for all 396-units.

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Area 2: 222 Apartment Units

Adjustments Requested:

SRC 702.020(e)(4) Façade and Building Design

Industrial Commercial (IC)-SRC Chapter 551

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Northwest: 48 to 165-foot setback (Buildings 26, 27, 28); IC zoned-vacant land

East: 20-foot setback (Buildings 9, 10, 11); RMII zoned-existing apartments

South: 20-foot setback (Buildings 17, 18, 19); Cordon Road

West: 20-foot setback (Buildings 20-26); HWY 22 right-of-way

A Street: 20-foot setback (Buildings 1, 2, 3, 4, 5, 6, 11, 29, 32, 33, 34, 35, 36)

Maximum Height: Maximum building height allowed in the IC zone is 70'. All proposed buildings are in compliance with the requirements of the Code.

*Building 1 (Type D and E Units) is 37.10 feet in height (measured to the highest point)

- *Building 2 (Type C Units) is 34.8 feet in height (measured to the highest point)
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- *Storage Buildings (2 Buildings) are 13.8 feet in height (measured to the roof line)

Therefore, the buildings are in compliance with the building height requirement.

Parking (Sheet SDR3): The development is for a 396-unit apartment complex. Code requires 1 vehicle parking spaces per every studio or 1-bedroom dwelling unit and 1.5 vehicle parking spaces per every 2 or more bedrooms. The applicant is required to provide a minimum of 558 on-site vehicle parking spaces and is allowed a maximum of 961 on-site vehicle parking spaces. As shown on the site plan, 777 on-site parking spaces are being provided throughout the development.

Area 1: 243 parking spaces required

291 Standard Parking Stalls

44 Compact Parking Stalls

12 Handicap Parking Stalls
347 Total Parking Stalls Provided

Area 2: 315 parking spaces required
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64 Compact Parking Stalls
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430 Total Parking Stalls Provided

Adequate parking has been provided throughout the development with 1.96 parking spaces per dwelling unit. Loading spaces have been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Three (3) loading zones have been provided throughout the site as well.

Bicycle parking is also required on site. The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 42 bicycle parking spaces have been provided on-site.

Area 1: (18 required)	Area 2: (22 required)
18 Bicycle Spaces	24 Bicycle Spaces

Recycling (Sheet SDR3): There are two trash/recycle area provided within the development. One is located in Area 1 and one is located in Area 2 of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: As shown on the Grading and Drainage Plan, the proposal is treating at least 80% hard surface with Green Water Infrastructure. A Preliminary Drainage Report is currently being worked on and will be submitted to the City when complete. Therefore, meeting the requirements of the Public Works Department.

Landscaping: The IC zone requires that 15% of the site be landscaped. The net developable area is 958,324 square feet in size with 523,967 square feet of landscaping throughout. Therefore, the development provides 55% landscaping throughout the site.

SITE PLAN AREA 1:

BOUNDARY	343,482 S.F.
OPEN SPACE	132,604 S.F.
SIDEWALK	28,901 S.F.
BUILDINGS	59,772 S.F.
PARKING	122,205 S.F.
RECREATION	13,353 S.F.

SITE PLAN AREA 2:

* ENTIRE SITE 2 AREA TOTALS

BOUNDARY	614,842 S.F.
OPEN SPACE	391,363 S.F.
SIDEWALK	39,822 S.F.
BUILDINGS	79,056 S.F.
PARKING	152,751 S.F.
RECREATION	6,870 S.F.
WATER QUALITY	23,000 S.F.

Traffic: A Traffic Impact Analysis (TIA) is currently being worked on and will be submitted when complete.

The original TIA was done as part of CPC-ZC-PAR12-08 for the subject property and the properties to the east. In order to mitigate any impacts, the developers were required to put in a signal at Gaffin Road. Furthermore, the intent of the applicant is to mitigate impacts by providing adequate circulation for vehicle, multi-modal / pedestrian impacts to the transportation system.

Trees: There are 55 trees located within the boundary of the development (Area 1 and Area 2). Eighteen (18) trees are designated for removal. Therefore, preserving 67% of the street on the site.

SITE PLAN AREA 1:

10 TREES
10 TREES TO BE REMOVED
0 REMAIN

SITE PLAN AREA 2:

45 TREES
8 TREES TO BE REMOVED
37 REMAIN

There are no significant Oregon White Oaks on the site.

Multiple Family Design Review Standards- Chapter 702

702.020(a):

702.020(a)(1) Open Space (Sheet SDR4): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The standards are also intended to ensure that open space is an integral part of the overall development design.

Per the code the proposed development shall provide a minimum 30% open space. The minimum open space area required for this development is 287,497 square feet of the net developable area. The portion of the subject property being developed is zoned IC and is 958,324 (22 acres) square feet in size with 544,190 square feet of open space, including landscaped areas, a tot lot, and recreation building. Therefore, totaling 57% open space.

The existing street to the north, A Street, runs north/south through the site. Therefore, there are two sides of the development (Area 1 and Area 2). Both sides will share open space amenities as shown on the site plan.

Area 1 (174-units): 103,045 square feet of open space required

Site Area-343,482 square feet (7.88 acres)

Open Space-129,958s.f. (39%/Including Rec. Building)

Recreation Building-2,646s.f.

Area 2 (222-units): 184,453 square feet of open space required

Site Area-614,842 square feet (14.11 acres)

Open Space-388,717s.f. (64%/Including Rec. Building)

Recreation Building-2,646s.f.

The proposed development provides two 2,646 square foot recreation buildings, one in Area 1 and one in Area 2. Along with two fenced play areas, with one located Area 1 (13,353s.f.) and one located in Area 2 (6,870s.f.); and landscaped open space areas throughout the site. The recreation building, open space areas, and play areas are available to all 396 dwelling units and accessible via the proposed pedestrian pathways. The interior pathways are 5-foot wide paved sidewalks and all sidewalks throughout the development are 6-foot wide paved sidewalks. The recreation building, play areas, and open space areas are located in convenient and safe areas for the residents. The proposed development provides approximately 544,190 square feet of total common open space throughout the site, that includes the recreation building and pool.

Therefore, this standard has been met. See attached site plans and open space plan.

702.020(a)(1)(A) Common Open Space Area Size and Dimensions (Sheet SDR4):

The applicant is proposing a development consisting of 396-apartment units as shown on the site plans. The apartments will be developed with the IC zoned portion of the site that consists of 22 acres.

Per Table 702-3, The minimum open space area required for this development is 287,497 square feet of the net developable area. The portion of the subject property being developed is zoned IC and is 958,324 (22 acres) square feet in size with 544,190 square feet of open space, including landscaped areas, a tot lot, and recreation building. Therefore, totaling 57% open space.

702.020(a)(1)(B): As shown on the site plan all open space is usable open space. The existing conditions plan, and grading plan identify all the slopes throughout the site.

702.020(a)(1)(C) and (D): Private Open Space: Each unit will have private open space as required by code. Ground floor units will have patio areas that are 96 square feet in size, with no dimension less than 6 feet. All second and third story units will have balconies/decks that are a minimum 60 square feet in size. All private open space areas are located contiguous to the dwelling unit and will be screened with 5-foot-high landscaping. This private open space includes the patios and balconies/decks. Therefore, this standard has been met.

702.020(a)(1)(E): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The standards are also intended to ensure that usable open space is an integral part of the overall development design. As shown on the site plan, there is a usable open space area located within the development.

702.020(a)(1)(F): Bill Eiegel Park is located directly across Hwy 22 from the site, which is less than 1/2 from the subject property. However, the proposed development will have still be providing more than adequate open space areas on-site.

702.020(b)(1) and 702.020(b)(2) Landscaping Standards: The subject property does not abut RS zoned properties. Landscaping is being provided adjacent all property lines and within the interior of the development. Landscaping has been provided throughout the site as identified on the landscape plans. A minimum of 1 tree will be planted for every 2,000 square feet of the site. Trees and vegetation have been provided throughout the development as shown on the landscape plans. There is 523,967 square feet of landscaped area throughout the site. Therefore, 55% of the site is landscaped. Landscape plans have been provided and demonstrate how the landscape standards have been met. See attached landscape plans.

A permanent underground irrigation system will be provided when development plans are final.

There are 55 trees located within the boundary of the development (Area 1 and Area 2). Eighteen (18) trees are designated for removal. Therefore, preserving 67% of the street on the site.

702.020(b)(3), (4), (5): Landscaping has been provided around the proposed buildings as shown on the plans.

There are 55 trees located within the boundary of the development (Area 1 and Area 2). Eighteen (18) trees are designated for removal. Therefore, preserving 67% of the street on the site.

702.020(b)(6): All private ground level open space areas are located contiguous to the dwelling unit and will be screened with 5-foot-high landscaping. This will help to ensure privacy for the patio areas designated as private open space for residents.

702.020(b)(7) and (8): All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds. There landscaping within the parking areas. This does not include the landscaped areas surrounding the parking areas.

Interior Parking Lot Landscaping: The proposal is for 396-units; therefore, the development is exempt from the requirements of SRC 806.035(d)(2).

702.020(c)(1) and (2) Site Safety and Security Standards: All buildings have windows provided in habitable rooms and windows that face the parking lots and open space areas. This helps provide an eye on the development. Lighting on the buildings and along the sidewalks will be provided as well.

702.020(c)(3) and (4): Safety of the residents is very important, and all requirements are met to assure safety and compliance with code. There are no fences or plant materials located in areas within the development that obstruct visibility. All landscaping adjacent to open space areas will not exceed 3 feet in height.

702.020(d)(1), (2), and (4) Parking and Site Design Standards: The subject property has street frontage on A Street and Cordon Road. Internal accessways are proposed within the development.

Per SRC 702.020(d)(1): *"To minimize large expanses of continuous pavement, parking areas greater than 6,700 square feet in area shall be physically and visually separated with landscaped planter bays that are a minimum of nine feet in width. Individual parking areas may*

be connected by an aisle or driveway.”

As shown on Sheet SDR5, there is only one parking area that is greater than 6,700 square feet in area, which is allowed when broken up by landscape planter bays. Planter bays a minimum of 9 feet in width have been provided adjacent all parking areas. Therefore, in order to minimize large expanses of continuous pavement, 9-foot-wide planter islands have been provided a maximum of every 12 parking spaces. Therefore, meeting the code.

All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. The layout of the parking areas has been taken into consideration and provides for safe and efficient circulation throughout the development. In order to take into consideration circulation, pedestrian access, landscaping, and the requirements of the code, the parking areas have been carefully designed. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds. There is landscaping within the parking areas. This does not include the landscaped areas surrounding the parking areas.

SRC 702.020(d)(2): “To minimize the visual impact of on-site parking and to enhance the pedestrian experience, off-street surface parking areas and vehicle maneuvering areas shall be located behind or beside buildings and structures. Off-street surface parking areas and vehicle maneuvering areas shall not be located between a building or structure and a street.”

All other buildings and parking areas on the site are in compliance with this standard.

The development does provide setbacks between the buildings and pathways. All the pathways connect the buildings, open space, parking areas, and surrounding uses.

Therefore, meeting this standard.

702.020(e)(1) through (8) Façade and Building Design: These guidelines are intended to promote building and site design that contributes positively to a sense of neighborhood and to the overall streetscape by carefully relating building mass, entries and yards to public streets.

(e)(1)-All other buildings within the development will not exceed 150 feet in length. The height and length of the buildings and structures conform to the measuring requirements in code.

Therefore, the applicant has requested an adjustment to this standard.

(e)(2) & (3)-All buildings face the interior of the lot. The rear side of Buildings 1, 2, 3, 4, 5, 6, 29, 32, 33, 34, 35, and 36 face A Street that runs through the site. The rear side of Buildings

17, 18, and 19 face Cordon Road. The street side of these buildings (rear) will be designed to be visually appealing, by providing similar design as is being provided for the front building facade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions of the building facing the right-of-way. The rear side of Buildings 7 and 8 do face residential zoned property (RS). However, they are setback over 34 feet from the property. All of which will help to provide privacy.

In order to provide safety and convenience for the residents, both buildings will face the interior the lot. This provides more eyes on the site, which provides greater safety for residents. This also provides convenience since all parking is located within the interior of the site.

(e)(4)-Cordon Road

The subject property has 685 feet of buildable width (this excludes required side setbacks) along Cordon Road. Code requires a minimum of 40% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, due to the Cordon Road being a high traffic road and a required 10-foot PUE, the buildings are not set within required 5 feet. Therefore, occupying 0% of the buildable width of street frontage along Boone Road. Due to the location and need for the detention pond along Cordon Road, additional buildings could not be provided along the setback line of Cordon Road.

An adjustment to this standard has been requested.

A Street (East Side)

The subject property has 1065 feet of buildable width (this excludes required side setbacks and driveway) along A Street. Buildings 1, 2, 3, 4, 5, 6, and 11 are located within 20 feet of the east side A Street. Code requires a minimum of 40% of the buildable width be occupied by buildings. As shown on the site plan, due to the required 10-foot PUE, the buildings are not set within required 5 feet. However, as shown on the site plan, the buildings are setback 20 feet and total 615 feet of the buildable width along the street frontage. Therefore, occupying 58% of the buildable width of street frontage along A Street.

An adjustment to this standard has been requested.

A Street (West Side)

The subject property has 1065 feet of buildable width (this excludes required side setbacks and driveway) along A Street. Buildings 29, 32, 33, 34, 35, and 36 are located within 20 feet of the east side A Street. As shown on the site plan, due to the required 10-foot PUE, the buildings are not set within required 5 feet. However, as shown on the site plan, the buildings are setback 20 feet and total 660 feet of the buildable width along the street frontage. Code requires a minimum of 40% of the buildable width be occupied by buildings. Therefore,

occupying 61% of the buildable width of street frontage along A Street.

An adjustment to this standard has been requested.

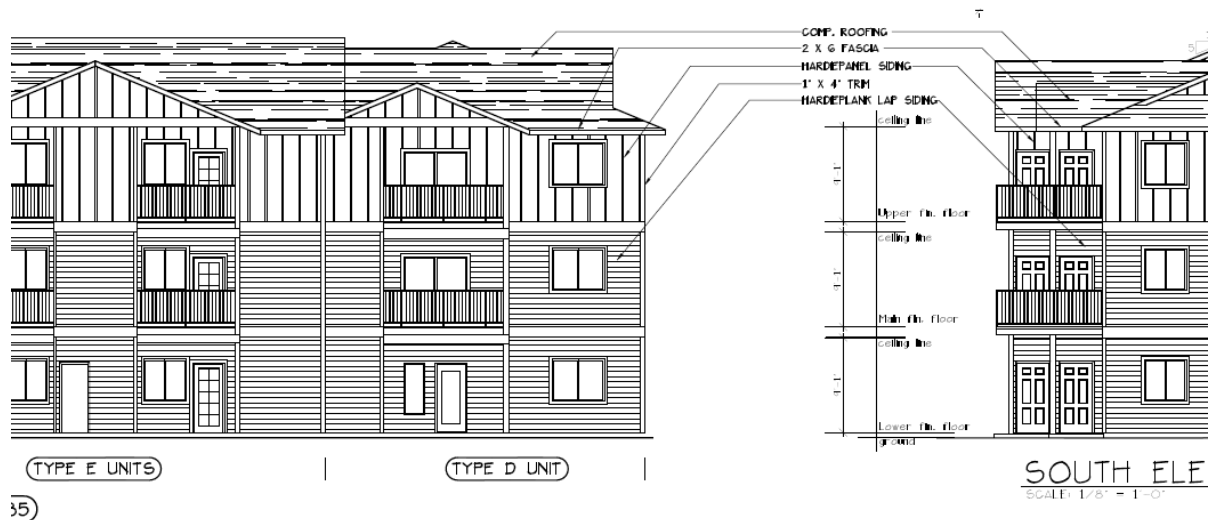
(e)(5) & (6)-All buildings have entrances physically and visually connected to the internal public sidewalk system and the parking lots. All external stairways are recessed into the buildings. Therefore, physically and visually incorporating them into the building's architecture design. The primary entrances for each individual unit are provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood. All roof-mounted equipment will be screened and integrated into the building design. Further review of this requirement will take place at the time of building permits.

A direct pedestrian path from a building entrance to the street has been provided to Cordon Road, the cul-de-sac, and A Street as shown on the site plan.

(e)(7)-All roof mounted equipment will be screened as required by code.

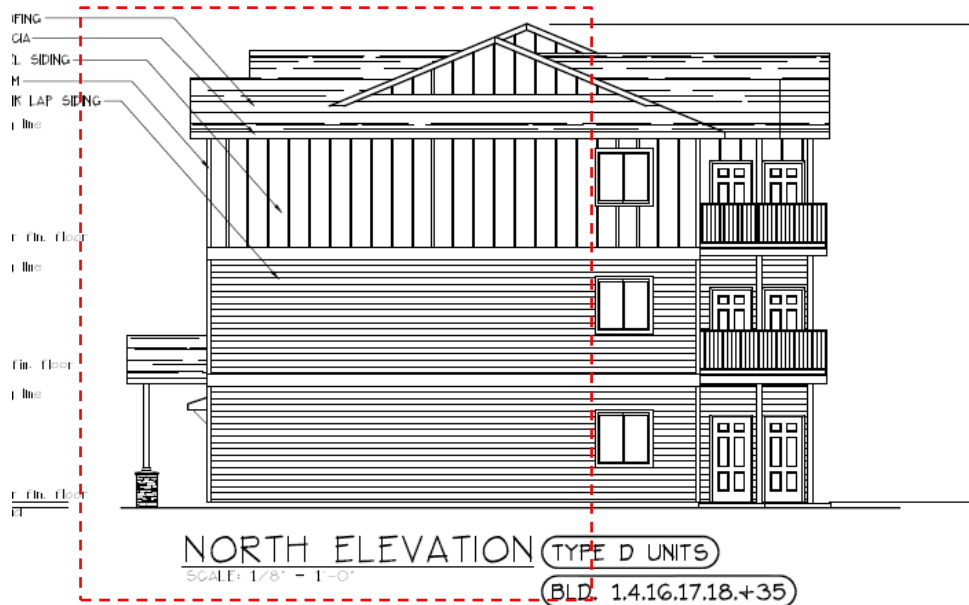
(e)(8)-The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. Balconies (decks) and dormers are incorporated in the building design to add some visual element to the buildings. Therefore, breaking up the long building length.

702.020(e)(9) and (10) (See Building Elevations): Varied materials, textures, and colors are being used on the buildings. The applicant has provided building elevations to show how this is being complied with. The materials used on the front, rear, and sides of the apartments are the same; shake siding, trim board, and lap siding. See attached building elevations.



The proposed buildings proved the required offsets and design elements as shown on the building elevations.

The proposed buildings provide offsets:



Covered Decks and Recessed Balconies:



Covered Entrance:

