

NOTICE OF DECISION

PLANNING DIVISION
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*Si necesita ayuda para comprender esta información, por favor llame
503-588-6173*

PLANNING ADMINISTRATOR DECISION FOR SITE PLAN REVIEW

CASE TYPE: Class 2 Site Plan Review
AMANDA NO.: 21-114799-RP
DATE OF DECISION: March 1, 2022
PROPERTY LOCATION: 3995 Rickey St SE
APPLICANT: Magellan Architects (on behalf of McDonald's Corporation)
REPRESENTATIVE: Jamie Trendera
ZONE: CR (Commercial Retail)
COMPREHENSIVE PLAN: COM (Commercial)
OVERLAY: None

REQUEST

A Class 2 site plan review for exterior improvements to reconfigure the drive-through to add a second order lane with associated order canopy. The subject property is 0.88 acres in size, zoned CR (Commercial Retail), and located at 3995 Rickey Street SE (Marion County Assessor map and tax lot number(s): 072W31C00500).

FINDINGS

The purpose of site plan review is to provide a unified, consistent, and efficient means to review for development activity that requires a building permit, to ensure that such development meets all applicable standards of the UDC, including, but not limited to, standards related to access, pedestrian connectivity, setbacks, parking areas, external refuse storage areas, open areas, landscaping, and transportation and utility infrastructure. The proposed site plan is included as **Attachment A**.

Pursuant to SRC 220.005(b)(2), Class 2 site plan review is required for any development that requires a building permit, other than development subject to Class 1 site plan review, or prior to commencement of work for any of the activities identified under SRC 220.005(a)(1)(B) when a building permit is not otherwise required, and that does not involve a land use decision or limited land use decision, as those terms are defined in ORS 197.015.

1. Approval Criteria

SRC 220.005(f)(2) provides that an application for Class 2 site plan review shall be granted if:

- (a) Only clear and objective standards which do not require the exercise of discretion or legal judgment are applicable to the application.

Finding: Only clear and objective standards apply to the proposed development.

- (b) The application meets all the applicable standards of the UDC.

Finding: The following is a summary of the applicable development standards of the UDC.

Land Use

The subject property is zoned CR (Commercial Retail). Development of the property is subject to the use and development standards of the CR zone (SRC Chapter 522).

The proposed exterior improvements to reconfigure the drive-through to add a second order lane with associated order canopy. The existing *eating and drinking establishment* is proposed to continue and is a permitted use in the CR zone.

CR Zone (SRC Chapter 522)

- a. Lot Standards (SRC 522.010(a)):

There is no minimum lot area, no minimum lot width, and no minimum lot depth in the CR zone. The minimum street frontage requirement is 16 feet for all uses other than single family.

Finding: The existing lot complies with the minimum lot standards of the CR zone and no changes to the lot size or dimensions are proposed.

- b. Setbacks (SRC 522.010(b)):

Setbacks within the CR zone shall be provided as set forth in SRC Table 522-3 and 522-4.

Abutting Street

East: The subject property abuts Lancaster Drive SE to the east. Buildings and accessory structures have a minimum setback of 5 feet. Vehicle use areas shall be set back using one of the methods set forth in SRC 806.

Finding: No changes are proposed to the setbacks of the structure on the east property line; the structure is setback approximately 70 feet meeting the standard. The reconfiguration of the drive-through aisle(s) does not impact the existing, nonconforming vehicle use area setback of approximately five feet abutting Lancaster Drive SE; vehicle use area perimeter setbacks abutting street are found in SRC 806.035(c)(2). The standards are met or do not apply to this proposal.

South: The subject property abuts Rickey Street SE to the south. Buildings and accessory structures have a minimum setback of 5 feet. Vehicle use areas shall be set back using one of the methods set forth in SRC 806.

Finding: No changes are proposed to the setbacks of the structure on the south property line; the structure is setback approximately 26 feet meeting the standard. The reconfiguration of the drive-through aisle(s) does not impact the existing vehicle use area setback of approximately ten feet abutting Rickey Street SE meeting Method A as identified in SRC 806.035(c)(2)(A). A pedestrian connection is proposed to Rickey Street SE within the setback but does not impact the ten-foot setback. These standards are met.

Interior Side

West: The interior side setbacks require a zone-to-zone setbacks per SRC Table 522-4. The subject property is adjacent to a Marion County RS zoned, to the west. Pursuant 800.035(c)(1), *Zone-to-Zone setbacks abutting properties outside city limits*, the zone-to-zone setback is determined using the city equivalent zone identified under SRC Chapter 260; the City equivalent zone is RS. The minimum zone-to-zone setback for buildings, accessory structures and vehicle use areas when abutting residential zones in the CR zone is 15-feet with Type C landscaping as described in SRC 807.

Finding: No changes are proposed to structure that is setback approximately 48-53 feet on the west property line. The drive-through reconfiguration does not impact the existing, non-conforming vehicle use area setback of approximately five feet. The existing vehicle use area setback contains existing landscaping and the fenced screening of the Type C landscaping but is nonconforming in setback distance. The standards are met or are considered nonconforming.

Interior Rear

North: The interior rear setbacks require a zone-to-zone setbacks per SRC Table 522-4. The subject property is adjacent to a CR zoned property to the north. There is no minimum zone-to-zone setback for buildings and accessory structures to commercial zones, there is a vehicle use area setback of 5-feet with Type A landscaping.

Finding: No changes are proposed to the setbacks of the structure, the structure setback standards do not apply as there is no setback. Additionally, the vehicle use area is currently setback five-feet by an existing landscape strip. The proposal meets these standards.

- c. Lot Coverage, Height (SRC 522.010(c)):
In the CR zone, there is no maximum lot coverage for buildings and accessory structures. The maximum height of buildings and accessory structures, for all uses, is 50 feet.

Finding: The proposed development meets applicable lot coverage standards as there is no maximum lot coverage standard in the CR zone. No changes are proposed to the building height, therefore the maximum height standards do not apply. The proposal meets these standards as they do not apply to this proposal.

d. Landscaping (SRC 522.010(d)):

- 1) **Setbacks.** Required setbacks shall be landscaped. Landscaping shall conform to the standards set forth in SRC Chapter 807.
- 2) **Vehicle Use Areas.** Vehicle use areas shall be landscaped as provided under SRC Chapter 806 and SRC Chapter 807.
- 3) **Development site.** A minimum of 15 percent of the development site shall be landscaped. Landscaping shall meet the Type A standard set forth in SRC chapter 807. Other required landscaping under the UDC, such as landscaping required for setbacks or vehicle use areas, may count towards meeting this requirement.

Finding: As identified above, the setbacks and vehicle uses areas are either conforming or considered nonconforming in regard to setbacks and landscaping. The site is approximately 38,332 square feet requiring a minimum of 15 percent of landscaping, or 5,750 square feet of landscaping; per the site plan 5,961 square feet is proposed meeting the standard.

Adherence to the landscaping requirements set forth in SRC 806 & 807 will be reviewed again at the time of building permit.

General Development Standards (SRC Chapter 800)

Solid Waste Service Areas

Solid waste service areas are to provide for the safe and convenient collection of solid waste, recyclable and compostable materials by the local solid waste collection franchisee. Pursuant to SRC 800.055, solid waste service area design standards shall apply to all new solid waste, recycling and compostable service areas, where use of a solid waste, recycling and compostable receptacle one cubic yard or larger is proposed, and to any change to an existing solid waste service area for receptacles one cubic yard or larger that requires a building permit.

Finding: The proposed site plan does not indicate a new, or changes to, an existing solid waste service area. The standards of this section do not apply.

Pedestrian Access

SRC 800.065 – Pedestrian Access.

Except where pedestrian access standards are provided elsewhere under the UDC, all developments, other than *single family*, *two family*, *three family*, *four family*, and *multiple family* developments, shall include an on-site pedestrian circulation system developed in conformance with the standards in this section.

Finding: The pedestrian access standards of SRC Chapter 800 apply to the proposed development.

SRC 800.065(a)(1) – Connection between building entrances and streets.

A pedestrian connection shall be provided between the primary building entrance of each building on the development site and each adjacent street. Where a building has more than one primary building entrance, a single pedestrian connection from one of the building's primary entrances to each adjacent street is allowed; provided each of the building's primary entrances are connected, via a pedestrian connection, to the required connection to the street. Where an adjacent street is a transit route and there is an existing or planned transit stop along street frontage of the development site, at least one of the required pedestrian connections shall connect to the street within 20 feet of the transit stop.

Finding: As shown on the proposed site plan, the existing pedestrian walkway is shown to connect from the primary building entrance to Lancaster Drive SE. Additionally, a new connection is proposed from the primary building to Rickey Street SE. There is no transit stop, existing or planned, along the either street frontage of the development site (i.e. Lancaster Drive SE or Rickey Street SE). The proposal meets the standard.

SRC 800.065(a)(2) – Connection between buildings on the same development site.

Where there is more than one building on a development site, a pedestrian connection, or pedestrian connections, shall be provided to connect the primary building entrances of all of the buildings.

Finding: As shown on the site plan there is only one building on the development site. This standard is inapplicable.

SRC 800.065(a)(3)(A) – Connection through off-street parking areas.

Except as provided under subsection (a)(3)(A)(iii) of this section, off-street surface parking areas greater than 25,000 square feet in size or including four or more consecutive parallel drive aisles shall include pedestrian connections through the parking area to the primary building entrance or where there is no building, through the parking area as provided in this subsection.

Finding: The existing development site has no off-street parking areas greater than 25,000 square feet and/or with four parallel drive aisles, making these standards inapplicable to the proposal.

SRC 800.065(a)(4) – Connection to existing or planned paths and trails.

Where an existing or planned path or trail identified in the Salem Transportation System Plan (TSP) or the Salem Comprehensive Parks System Master Plan passes through a development site, the path or trail shall: Be constructed, and a public access easement or dedication provided;

or when no abutting section of the trail or path has been constructed on adjacent property, a public access easement or dedication shall be provided for future construction of the path or trail.

Finding: No existing or planned path or trail identified in the Salem TSP or the Salem Comprehensive Parks System Master Plan passes through the subject property. This standard does not apply to the proposed development.

SRC 800.065(a)(5) – Connection to abutting properties.

Except as otherwise provided within this section, whenever a vehicular connection is provided from a development site to an abutting property, a pedestrian connection shall also be provided.

Finding: A vehicular connection is provided from the subject property to the property abutting to the north. A recent site plan approval was approved for the abutting, northern property (Case No. 20-106727-RP) that outlined an existing *motor vehicle and manufactured dwelling and trailer sales* use. SRC 800.065(a)(5) provides for exceptions to this standard recognizing unique site characteristics that make pedestrian connections undesirable, unsafe or impractical. SRC 800.065(a)(5)(C) applies to the abutting property as the mixture of pedestrians with the neighboring use could create safety conflicts with pedestrians. Therefore this standard does not apply to the proposal.

SRC 800.065(b) – Design and materials.

Required pedestrian connections shall be in the form of a walkway or may be in the form of a plaza. Walkways shall be paved with a hard-surface material meeting the Public Works Design Standards, shall be a minimum of five feet in width, and shall meet the development standards required for walkways which are adjacent to auto travel lanes or cross over vehicle use areas set forth in SRC 800.065(b)(1)(b) and (c).

Finding: Proposed walkways are indicated on the site plan as being paved with a concrete surface and are proposed to be a minimum of five-feet in width. Full adherence to the pedestrian access requirements set forth in SRC Chapter 800 will be reviewed at the time of building permit application.

Off-Street Parking, Loading, and Driveways (SRC Chapter 806)

Vehicle Parking

SRC 806.005 – Off-Street Parking; When Required.

Off-street parking shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a parking ratio requiring a greater number of spaces than the previous use or activity; or for any intensification, expansion, or enlargement of a use or activity.

Finding: The proposal does not include a new use or activity, a change of use or activity resulting in a parking ratio requiring a greater number of spaces

than the previous use or activity, or for any intensification, expansion, or enlargement of the existing use or activity.

SRC 806.010 - Proximity of Off-Street Parking to Use or Activity Served.

Required off-street parking shall be located on the same development site as the use or activity it serves.

Finding: The proposed development includes an existing off-street parking and vehicle use are meeting this standard.

SRC 806.015 - Amount of Off-Street Parking.

Minimum Required Off-Street Parking. Pursuant SRC Table 806-1 *eating and drinking establishment* use requires a minimum of 1 space per 250 square feet of gross floor area.

Compact Parking. Up to 75 percent of the minimum off-street parking spaces required under this Chapter may be compact parking spaces.

Carpool and Vanpool Parking. New developments with 60 or more required off-street parking spaces and falling within the public services and industrial use classifications, and the business and professional services use category, shall designate a minimum of 5 percent of their total off-street parking spaces for carpool or vanpool parking.

Maximum Off-Street Parking. Unless otherwise provided under the UDC, off-street parking shall not exceed the amounts set forth in Table 806-2.

Finding: The existing *eating and drinking establishment* use is 4,233 square feet and requires 17 spaces (i.e. $4,233 / 250 = 16.93$); the existing site has 47 off-street parking spaces. No spaces are designated as compact and carpool or vanpool parking is not required. The maximum off-street parking for the site, pursuant to SRC Table 806-2, is 43 spaces. Currently, the proposal exceeds the maximum parking but is considered nonconforming and no changes are proposed to the off-street parking amounts/areas. The proposal meets these standards or it is considered nonconforming.

Off-Street Parking and Vehicle Use Area Development Standards

SRC 806.035(a) - General Applicability.

The off-street parking and vehicle use area development standards set forth in this section apply to the development of new off-street parking and vehicle use areas, expansion or alteration of existing off-street parking and vehicle use areas where existing paved surface is replaced with a new paved surface, or the paving of an unpaved area.

Finding: The proposed development does include an expansion or alteration of existing off-street parking and vehicle use areas where existing paved surface is replaced with a new paved surface, or the paving of an unpaved area. These standards apply.

SRC 806.035(b) - Location.

Off-street parking and vehicle use areas shall not be located within required setbacks. Carpool and vanpool parking shall be located so it is the closest employee parking to the building entrance normally used by employees; provided, however, it shall not be located closer than any parking designated for disabled parking.

Finding: The proposed off-street parking areas are not proposed within required setbacks and are on the same development site. As displayed, carpool and vanpool spaces are not proposed. The proposal meets these standards.

SRC 806.035(c) - Perimeter Setbacks and Landscaping.

Perimeter setbacks shall be required for off-street parking and vehicle use areas abutting streets, abutting interior front, side, and rear property lines, and adjacent to buildings and structures.

Finding: The setbacks are addressed in the above portion of the report under the CR Zone standards.

SRC 806.035(d) - Interior Landscaping.

Per SRC section 806.035(d), interior landscaping is required for off-street parking areas 5,000 square feet or greater. Interior landscaping shall be provided in amounts not less than those set forth in Table 806-5. Additionally, a minimum of one deciduous tree shall be planted for every 12 parking spaces within an off-street parking area; landscape islands and planters bays shall have a minimum planting area of 25 square feet and shall have a minimum width of five feet.

Finding: The parking area is existing with changes proposed to the interior landscaping. The northern portions of the off-street parking and vehicle use area, where the improvements are proposed, measures to approximately to 9,546 square feet; 5-percent of that area is 477 square feet (i.e. $9,764 \times .005 = 488.2$). As displayed in the proposed site plan (**Attachment A**) the interior landscaping standard is met with approximately 2, 037 square feet of interior landscaping. The proposal meets the standard.

Full adherence to the vehicle use area landscaping requirements set forth in SRC Chapter 806 and 807 will be reviewed at the time of building permit application.

SRC 806.035(e) - Off-Street Parking Area Dimensions. Off-street parking areas shall conform to the minimum dimensions set forth in Table 806-6.

Finding: The existing off-street parking spaces are shown to comply with the off-street parking space dimensions outlined in SRC Table 806-6 standards. The proposal meets the standard.

SRC 806.035(f-m) Additional Off-Street Parking Development Standards

Finding: The existing off-street parking area is developed consistent with the additional development standards for grade, surfacing, and drainage. The applicant's site plan does not indicate bumper guards/wheel stops on portions of the development site but not on the entire site. Wheel stops and/or bumper guards are required to ensure no portion of vehicles overhang or project into required setbacks and landscaped areas, pedestrian accessways, streets or alleys or, abutting properties. As shown on the site plan, extended curbs are displayed for the off-street parking spaces abutting pedestrian walkways. The remaining off-street parking spaces needing bumper guards/wheel stops are deficient as displayed in the site plan. The parking area striping, marking, signage, and lighting shall comply with the standards of SRC Chapter 806. Off-street parking area screening per SRC 806.035(m) is not required for the proposed parking area. Full adherence to the vehicle use area landscaping requirements set forth in SRC Chapter 806 and 807 will be reviewed at the time of building permit application.

Bicycle Parking

SRC 806.045(a) - General Applicability.

Bicycle parking shall be provided and maintained for each proposed new use or activity, any change of use or activity, when such change of use or activity results in a bicycle parking ratio requiring a greater number of spaces than the previous use or activity, or any intensification, expansion, or enlargement of a use or activity.

Finding: The proposal does not include a new use or activity, a change of use or activity resulting in a bicycle parking ratio requiring a greater number of spaces than the previous use or activity, or for any intensification, expansion, or enlargement of the existing use or activity. These standards are not applicable.

SRC 806.050 – Proximity of Bicycle Parking to use or Activity Served.

Bicycle parking shall be located on the same development site as the use or activity it serves.

SRC 806.055 - Amount of Bicycle Parking.

Bicycle parking shall be provided in amounts not less than those set forth in Table 806-8. The *eating and drinking establishments* use requires the greater of the following: 4 spaces; or 1 per 1,000 square feet.

Finding: The existing 4,233 square foot *eating and drinking* establishment requires a minimum of four bicycle parking spaces. As shown on the site plan, four bicycle parking spaces are shown abutting the main, western entrance. The bicycle parking spaces are on the same development site. These standards are met.

SRC 806.060 – Bicycle Parking Development Standards.

(a) *Location.* Except as otherwise provided in this section, bicycle parking shall be located outside a building.

(1) Bicycle parking located outside a building shall be located within a convenient distance of, and be clearly visible from, the primary building

entrance. In no event shall bicycle parking be located more than 50 feet from the primary building entrance, as measured along a direct pedestrian access route.

- (2) Where bicycle parking cannot be located outside a building, it may be located inside a building within a convenient distance of, and accessible from, the primary building entrance.

Finding: The proposed bicycle parking spaces are proposed approximately 36-45 feet from the primary entrance; the proposal meets this standard.

- (b) *Access.* Bicycle parking areas shall have direct and accessible access to the public right-of-way and the primary building entrance that is free of obstructions and any barriers, such as curbs or stairs, which would require users to lift their bikes in order to access the bicycle parking area.

Finding: As shown on the proposed site plan, the proposed bicycle parking areas have direct access to the public right-of-way through the existing pedestrian paths to Lancaster Drive SE that go through the existing the vehicle use area. Additionally, the bicycle parking areas have direct access to primary building entrances through the proposed pedestrian pathways in conformance with the requirements of SRC 806.060(b). The bicycle parking utilizes the ADA-ramps proposed on site removing any barriers that would require lifting of bikes. The proposal meets the standard.

- (c) *Dimensions.* Except as provided in subsection (f) of this section, bicycle parking areas shall meet the following dimension requirements:
 - (1) *Bicycle parking spaces.* Bicycle parking spaces shall be a minimum of six feet in length and two feet in width with the bicycle rack centered along the long edge of the bicycle parking space. Bicycle parking space width may be reduced, however, to a minimum of three feet between racks where the racks are located side-by-side.
 - (2) *Access aisles.* Bicycle parking spaces shall be served by a minimum four-foot-wide access aisle. Access aisles serving bicycle parking spaces may be located within the public right-of-way.

Finding: The proposed bicycle parking spaces meet the minimum dimensions for bicycle parking spaces and are adjacent to the proposed pedestrian walkway. The spaces are adjacent to pedestrian accessways, providing a minimum 4-foot-wide access aisle. The proposal meets the standard.

- (d) *Surfacing.* Where bicycle parking is located outside a building, the bicycle parking area shall consist of a hard surface material, such as concrete, asphalt pavement, pavers, or similar material, meeting the Public Works Design Standards.

Finding: The proposed bicycle parking spaces are placed on a hard surface material. The proposal meets the standard.

- (e) *Bicycle Racks.* Where bicycle parking is provided in racks, the racks may be floor, wall, or ceiling racks. Bicycle racks shall meet the following standards.

- (1) Racks must support the bicycle frame in a stable position, in two or more places a minimum of six inches horizontally apart, without damage to wheels, frame, or components.
- (2) Racks must allow the bicycle frame and at least one wheel to be locked to the rack with a high security, U-shaped shackle lock;
- (3) Racks shall be of a material that resists cutting, rusting, and bending or deformation; and
- (4) Racks shall be securely anchored.
- (5) Examples of types of bicycle racks that do, and do not, meet these standards are shown in Figure 806-10.

Finding: The applicant has proposed racks which appear to meet these standards. Full adherence with these standards will be ensured at the time of building permit review.

Off-Street Loading Areas

SRC 806.065 - General Applicability.

Off-street loading areas shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a greater number of required off-street loading spaces than the previous use or activity; or any intensification, expansion, or enlargement of a use or activity.

Finding: The proposal does not include a new use or activity, a change of use or activity resulting in an off-street loading area ratio requiring a greater number of loading areas than the previous use or activity, nor does the proposal include any intensification, expansion, or enlargement of the existing use or activity. The off-street loading area standards do not apply.

Historic Property Status

The subject property is not listed as a historic resource.

Previous Land Use Actions

VAR-SI19-02 - Denied request for two sign permits and a sign variance to allow construction of two freestanding vehicle viewing signs approximately 20 square feet and 13.7 square feet in size.

11-117761-RP - A Type I Site Plan Review for adding a roof cover to the existing trash enclosure on property.

City Department Comments

The Public Works Department reviewed the proposal and indicated no concerns.

The Fire Department reviewed the proposal and indicated no concerns.

The Building and Safety Division reviewed the proposal and indicated there are no issues.

2. Conclusion

Based on conformance with the preceding requirements, the Planning Administrator finds that the proposed site plan for property located at 3995 Rickey Street SE complies with approval criteria provided in SRC 220.005(f)(2).

DECISION

Final approval of the attached site plan is hereby GRANTED subject to SRC Chapter 220 and the applicable standards of the Salem Revised Code and providing compliance occurs with any applicable items noted above. This decision is a final land use decision.

Please Note: Findings included in this decision by the direction of the Salem Fire Department are based on non-discretionary standards. Fire Code related findings are intended to inform the applicant of the clear and objective Fire Prevention Code standards of SRC Chapter 58 that will apply to this development proposal on application for building permit(s). Additional or different Fire Prevention Code standards may apply based on the actual building permit application submitted.

If a building permit application has not already been submitted for this project, please submit a copy of this decision with your building permit application for the work proposed.

If you have any questions regarding items in this site plan review, please contact Kyle Kearns at 503-540-2313 or by email at kkearns@cityofsalem.net.



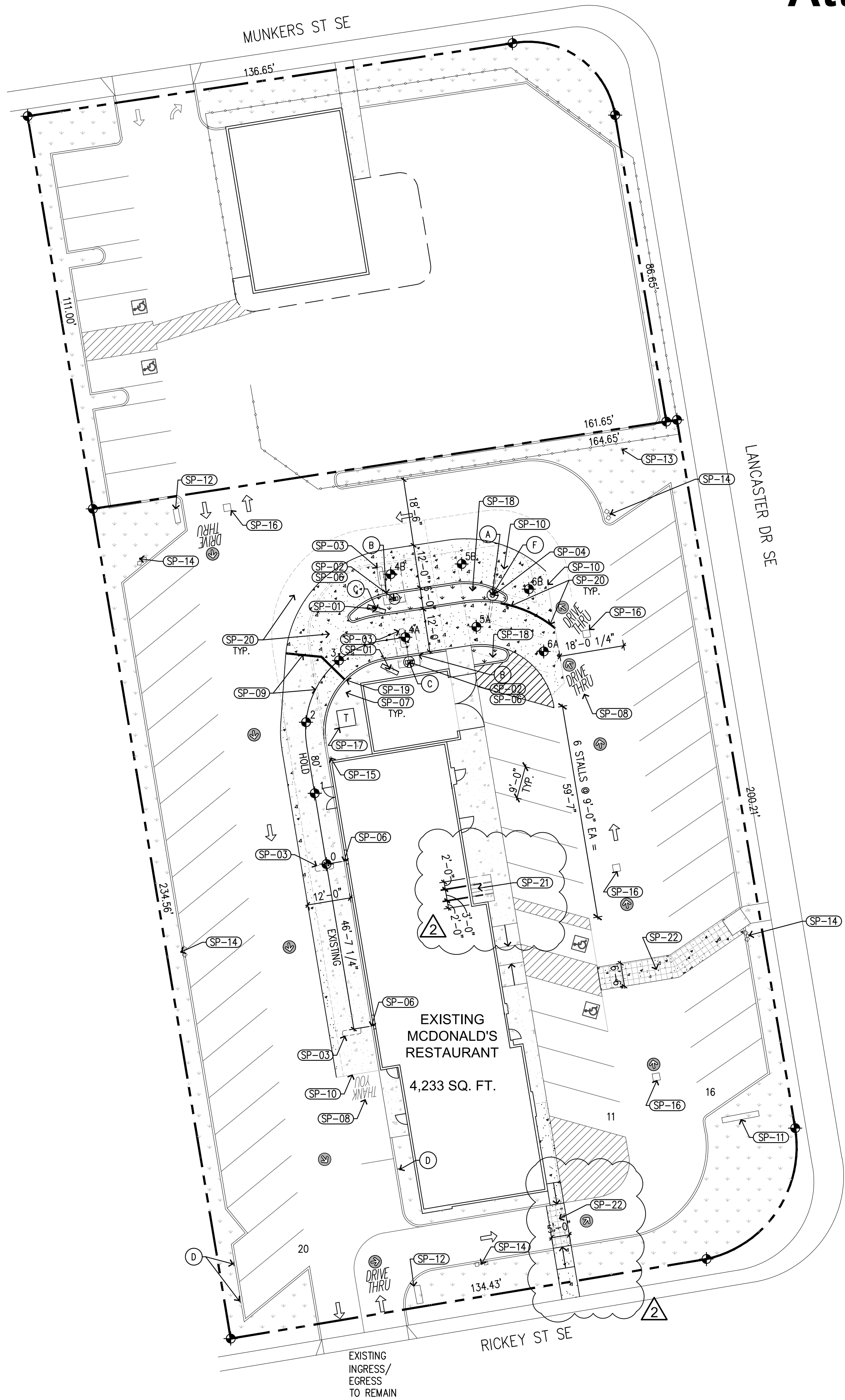
Kyle Kearns, Planner II, on behalf of
Lisa Anderson-Ogilvie, AICP
Planning Administrator

Application Deemed Complete:	<u>February 01, 2022</u>
Decision Mailing Date:	<u>March 1, 2022</u>
State Mandated Decision Date:	<u>June 1, 2022</u>

Attachments:

- A. Proposed Site Plan
- B. Existing Conditions Plan

Attachment A



SITE PLAN
SCALE: 1" = 20'-0"
0 5 10 20 40FT.

PROJECT DATA

SITE AREA:	38,332 SQ. FT.
EXISTING PARKING:	50 STALLS
PARKING REQUIRED:	1 STALL/ 250 SQ. FT. = 17 STALLS (BASED ON 4,233 SQ. FT.)
PARKING PROVIDED:	47 STALLS
ACCESSIBLE REQUIRED:	2 STALLS (BASED ON 47 PROVIDED)
ACCESSIBLE PROVIDED:	2 STALLS
ZONING:	RETAIL COMMERCIAL
BUILDING AREA:	4,233 EXISTING SQ. FT.(NO CHANGE)
OCCUPANCY :	A-2 (EXISTING)
TYPE OF CONSTRUCTION:	V-B (EXISTING)
LANDSCAPE AREA EXISTING:	6,723 SQ. FT.
LANDSCAPE AREA PROPOSED:	5,819 SQ. FT. (15.2%)

SITE PLAN KEYNOTES

- (SP-01) REMOVE EXISTING & INSTALL (2) NEW DIGITAL MENU BOARDS AS SHOWN. SEE DETAIL 2/D11.0 FOR EXACT LOCATIONS AND 10/D11.1. LOCATE EXISTING UTILITIES PRIOR TO EXCAVATING FOOTINGS.
- (SP-02) REMOVE EXISTING CUSTOMER ORDER DISPLAY & INSTALL (2) CUSTOMER ORDER CANOPIES AS SHOWN PER DETAIL 6/D11.2. SEE DETAIL 2/D11.0 FOR EXACT LOCATIONS. OVERALL CANOPY HEIGHT IS 10'-2" ABOVE PAVEMENT WITH CLEARANCE OF 9'-0" FOR VEHICLES BELOW. LOCATE EXISTING UTILITIES PRIOR TO EXCAVATING FOOTINGS.
- (SP-03) INSTALL NEW VEHICLE DETECTOR LOOPS AT NEW ORDER POINTS PER 2/D11.0. (OPTIONAL AT DRIVE THRU BOOTHS). PATCH & REPAIR CONCRETE PAD AT DRIVE THRU AS NECESSARY. SEE 18/D11.1 & 20/D11.1.
- (SP-04) REMOVE EXISTING & INSTALL NEW GATEWAY SIGN PACKAGE AS SHOWN. COORDINATE LOCATION WITH MCDONALD'S CONSTRUCTION MANAGER TO ENSURE BEST VISIBILITY FROM SITE ENTRANCE. SEE DETAIL 8/ D11.2 FOR MORE INFORMATION. LOCATE UNDERGROUND UTILITIES PRIOR TO EXCAVATION OF FOOTING.
- (SP-05) NOT USED
- (SP-06) INSTALL SPEECH/HEARING IMPAIRED SIGNAGE ON THE ORDER CANOPY & EACH SERVICE WINDOW. SEE DETAIL 4/SD1 & 5/SD1. PICTURE MENU, CLIPBOARD, AND PENCILS ARE TO BE PROVIDED AT EACH DRIVE-THRU WINDOW.
- (SP-07) EXISTING LANDSCAPE TO REMAIN. IF DAMAGED BY CONSTRUCTION, PATCH AND REPAIR AS NECESSARY TO ACCOMMODATE DEMOLITION AND NEW CONSTRUCTION. NEW LANDSCAPING TO MATCH EXISTING ADJACENT. ENSURE PLANTINGS DO NOT INHIBIT VIEW TO DRIVE THRU SIGNS.
- (SP-08) STRIPE PARKING STALLS AND SYMBOLS AS SHOWN. PAINTS TO BE TRAFFIC WHITE (2 COATS) AT PARKING AND TRAFFIC YELLOW (2 COATS) AT DRIVE-THRU. SEE DETAILS 1/SD1, 2/SD1, & 3/SD1.
- (SP-09) APPROXIMATE LINE OF NEW CONCRETE WORK TO EXISTING CONCRETE TO REMAIN.
- (SP-10) APPROXIMATE LINE OF CONCRETE PAD TO ASPHALT TRANSITION.
- (SP-11) EXISTING ROAD SIGN TO REMAIN; PROTECT FROM DAMAGE.
- (SP-12) EXISTING DIRECTIONAL SIGN TO REMAIN.
- (SP-13) EXISTING FLAG POLE TO REMAIN.
- (SP-14) EXISTING LOT LIGHTING TO REMAIN.
- (SP-15) EXISTING BOLLARD, PROTECT FROM DAMAGE
- (SP-16) EXISTING CATCH BASIN TO REMAIN.
- (SP-17) RELOCATE TRANSFORMER TO NEW LOCATION SHOWN. VERIFY EXACT LOCATION WITH MCDONALD'S CONSTRUCTION MANAGER & UTILITY COMPANY.
- (SP-18) NEW LANDSCAPING TO MATCH ADJACENT & AS REQUIRED BY LOCAL JURISDICTION REQUIREMENTS. ENSURE PLANTINGS WILL NOT INHIBIT VIEW OF DRIVE THRU & DIRECTIONAL SIGNAGE FROM CUSTOMERS.
- (SP-19) PROVIDE MERGE POINT MONITOR MOUNTED ON POLE WITH FOUNDATION; EXACT LOCATION TO BE COORDINATED WITH MCDONALD'S CONSTRUCTION MANAGER. PROVIDE 2" CONDUIT TO CASH BOOTH JUNCTION BOX. CONNECT TO ELECTRICAL & DATA SYSTEMS.
- (SP-20) REMOVE EXISTING ASPHALT, LANDSCAPING, CONCRETE CURBS, & CONCRETE DRIVE THRU PAD AS NECESSARY TO ACCOMMODATE NEW WORK. INSTALL NEW CONCRETE PAVING & CURBING THROUGHOUT DRIVE THRU LANES AS SHOWN, PER MCDONALD'S SPECIFICATIONS. SLOPE TO DRAIN AS NECESSARY. SEE DETAILS 1/SD1, 2/SD1.
- (SP-21) INSTALL NEW STAPLE, POST & RING, OR LOOP STYLE BICYCLE RACKS PER SRC 806.060 AT LOCATION SHOWN. PROVIDE 4 BICYCLE SPACES MIN.
- (SP-22) REMOVE EXISTING ASPHALT AS NECESSARY TO PROVIDE NEW STAMPED CONCRETE CROSSWALK AS SHOWN. ENSURE 5% MAXIMUM SLOPE IN DIRECTION OF TRAVEL & 1.5% MAXIMUM CROSS SLOPE. ENSURE NO GAPS OR CHANGES IN LEVEL GREATER THAN 1/4" OR 1/2" WITH BEVEL. PROVIDE FLUSH TRANSITION TO ADJACENT ASPHALT.
- (SP-23) REMOVE EXISTING LANDSCAPE & CONCRETE CURBS AS NECESSARY TO PROVIDE NEW BROOM-FINISH CONCRETE SIDEWALK & CURB RAMPS AS SHOWN. AT SIDEWALK, ENSURE 5% MAXIMUM SLOPE IN DIRECTION OF TRAVEL & 1.5% MAXIMUM CROSS SLOPE. SEE DETAILS 6/SD1 & 7/SD1. AT IN-LINE CURB RAMPS, SLOPE IN DIRECTION OF TRAVEL TO BE 8% MAX WITH 1.5% MAX CROSS SLOPE. PROVIDE 5'-0" MIN DEEP X FULL RAMP WIDTH TOP & BOTTOM CURB RAMP LANDINGS WITH 1.5% MAX SLOPE IN ALL DIRECTIONS & 5% MAX SLOPE IN DIRECTION OF TRAVEL AT GUTTER PAN. ENSURE TOP AND BOTTOM CURB RAMP TRANSITIONS ARE FLUSH. SEE DETAIL 8/SD1. ENSURE NO GAPS OR CHANGES IN LEVEL GREATER THAN 1/4" OR 1/2" WITH BEVEL.

LEGEND

	EXISTING CONCRETE CURBING TO REMAIN/ REPAIR
	NEW CONCRETE CURBING
	EXISTING CONSTRUCTION TO BE DEMOLISHED
	NEW STRIPING
	EXISTING LANDSCAPING TO REMAIN
	NEW LANDSCAPING
	EXISTING CONCRETE TO REMAIN
	NEW CONCRETE
	EXISTING LOT LIGHT
	EXISTING STRIPING
	EXISTING CATCH BASIN

GENERAL NOTES

1. PROPOSED UTILITIES ARE SHOWN IN SCHEMATIC ONLY. EXACT LOCATIONS SHALL BE DETERMINED TO ALLOW FOR THE MOST ECONOMICAL INSTALLATION.
2. THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES TO DETERMINE EXACT POINT OF SERVICE CONNECTION AT EXISTING UTILITY. REFER TO THE BUILDING ELECTRICAL AND PLUMBING DRAWINGS FOR UTILITY SERVICE ENTRANCE LOCATIONS, SIZES, AND CIRCUITING.
3. FINISH WALK AND CURB ELEVATIONS SHALL BE 6" ABOVE FINISH PAVEMENT.

PAVING SPECIFICATION
(MINIMUM 3" TOTAL COMPACTED ASPHALT THICKNESS)

NOTE: MCDONALD'S ENGINEER RESERVES THE RIGHT TO REQUEST A COMPACTION TEST AND/OR A CORE SAMPLE. IF TESTS PROVE CORRECT, PER ABOVE SPECIFICATION, TESTS WILL BE AT THE EXPENSE OF MCDONALD'S, OTHERWISE, G.C. WILL BE CHARGED.

LOT LIGHTING RECOMMENDATION

EXISTING LOT LIGHTS TO REMAIN. VERIFY WITH MCDONALD'S CONSTRUCTION MANAGER IF CLEAN/RELAMP OR NO WORK DURING THIS PROJECT.

PARKING INFORMATION

TOTAL SPACES	25	9'-0" X 20'-0" SPACES @ 65'
	20	9'-0" X 20'-0" SPACES @ 60'
	2	ADA 9'-0" X 20'-0" SPACES @ 65'
47		

UTILITY INFORMATION

	SIZE	TYPE	LOCATION
SANITARY SEWER		EXISTING	
WATER		EXISTING	
STORM SEWER		EXISTING	
ELECTRIC		EXISTING	
GAS		EXISTING	

SURVEY INFORMATION

PLAN SCALE: 1" = 20'	
STREET ADDRESS 3995 RICKEY ST. SE	
CITY SALEM	STATE OR
COUNTY MARION	

REGIONAL DWG. NO. 36-0189	CORPORATE DWG. NO. 14118
------------------------------	-----------------------------

REGISTERED ARCHITECT
MAGELLAN ARCHITECTS
KIRKLAND, WA

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8333 158th Avenue Northeast, Suite 280
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M. McDonald's USA, LLC
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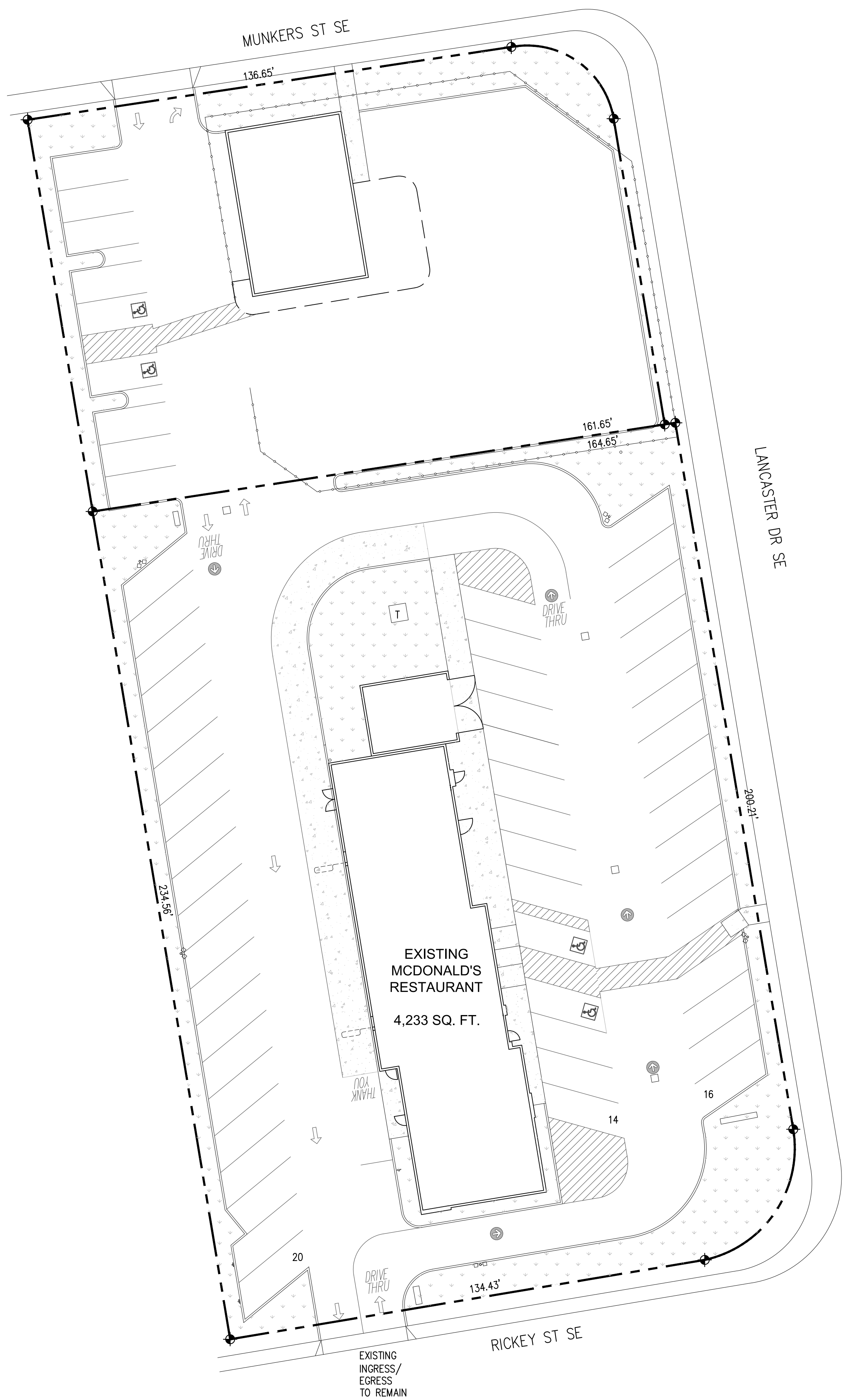
TITLE	SIDE-BY-SIDE DRIVE THRU REMODEL
DESCRIPTION	NEW SBS DRIVE THRU AT EXISTING BUILDING
SHEET NO.	PP21-210 SP-2 SITE PLAN

DATE	REV	DATE	BY
02/01/22	2	02/01/22	MA
07/31/22	1	07/31/22	MA
	-		MA
			BY

1. THE LOCATION AND ORIENTATION OF MENU BOARDS AND PRE-SELLS WERE DETERMINED BY THE SIGHT LINES OF THE CARS SHOWN. THE CARS WERE POSITIONED ACCORDING TO THE MAX. AMOUNT OF CARS DURING A PEAK PERIOD. ALL DIMENSIONS SHOWN ARE BASED UPON PROTOTYPICAL LAYOUTS SHOWN. ACTUAL DIMENSIONS TO BE BASED ON SITE SPECIFIC CONFIGURATIONS AND THE REQUIREMENTS OF NOTES 2, 3 & 4.
2. MENU BOARDS ARE ORIENTATED FOR VIEWING BY CARS 4A & 4B (PRIMARY USERS) & CARS 5A & 5B (SECONDARY USERS).
3. PRE-BROWSE MENU BOARDS ARE ORIENTED FOR VIEWING BY CARS 5A & 5B.
4. PROVIDE TWO CIRCUITS PER MENU BOARD. ONE CIRCUIT FOR LIGHTS AND ONE ONE CIRCUIT WITH LOCK FOR MOTOR AND CONTROLLER.
5. ALL GALVANIZED ANCHOR BOLTS TO BE SUPPLIED AND INSTALLED BY THE CONTRACTOR.
6. CONTRACTOR & SIGN INSTALLER TO INSTALL ALL SIGNS PER MANUFACTURER'S INSTALLATION INSTRUCTIONS.

DT1.0 SCALE: - DT_GENERAL_NOTES.DWG

[illegible]



EXISTING SITE PLAN

SCALE: 1"= 20'-0"
0 5 10 20 40FT.

LEGEND

- EXISTING CONCRETE CURBING
- EXISTING LANDSCAPING
- EXISTING CONCRETE
- EXISTING LOT LIGHT
- EXISTING STRIPING
- EXISTING CATCH BASIN

PROJECT DATA

SITE AREA: 38,332 SQ. FT.
EXISTING PARKING: 50 STALLS
ACCESSIBLE PROVIDED: 2 STALLS
ZONING: RETAIL COMMERCIAL
BUILDING AREA: 4,233 EXISTING SQ. FT.
OCCUPANCY : A-2 (EXISTING)
TYPE OF CONSTRUCTION: V-B (EXISTING)
LANDSCAPE AREA: 6,723 SQ. FT. (17.5%)

PARKING INFORMATIION

TOTAL SPACES	28	9'-0" X 20'-0" SPACES @ 65"
	20	9'-0" X 20'-0" SPACES @ 60"
	2	ADA 9'-0" X 20'-0" SPACES @ 65"
	50	

UTILITY INFORMATION

	SIZE	TYPE	LOCATION
SANITARY SEWER	EXISTING		
WATER	EXISTING		
STORM SEWER	EXISTING		
ELECTRIC	EXISTING		
GAS	EXISTING		

SURVEY INFORMATION

PLAN SCALE: 1" = 20'

STREET ADDRESS
3995 RICKEY ST. SE

CITY SALEM STATE OR

COUNTY
MARION

REGIONAL DWG. NO. 36-0189
CORPORATE DWG. NO. 14118



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DRAWN BY	STD ISSUE DATE	REVIEWED BY	DATE ISSUED
	JUL 2021	SEM	JUL 2021
TITLE	SIDE-BY-SIDE DRIVE THRU REMODEL		
DESCRIPTION	NEW SBS DRIVE THRU AT EXISTING BUILDING		
SITE ID	3995 Rickey St. SE Salem, OR 97301		

SHEET NO. PP21-210
SP-1
SITE PLAN