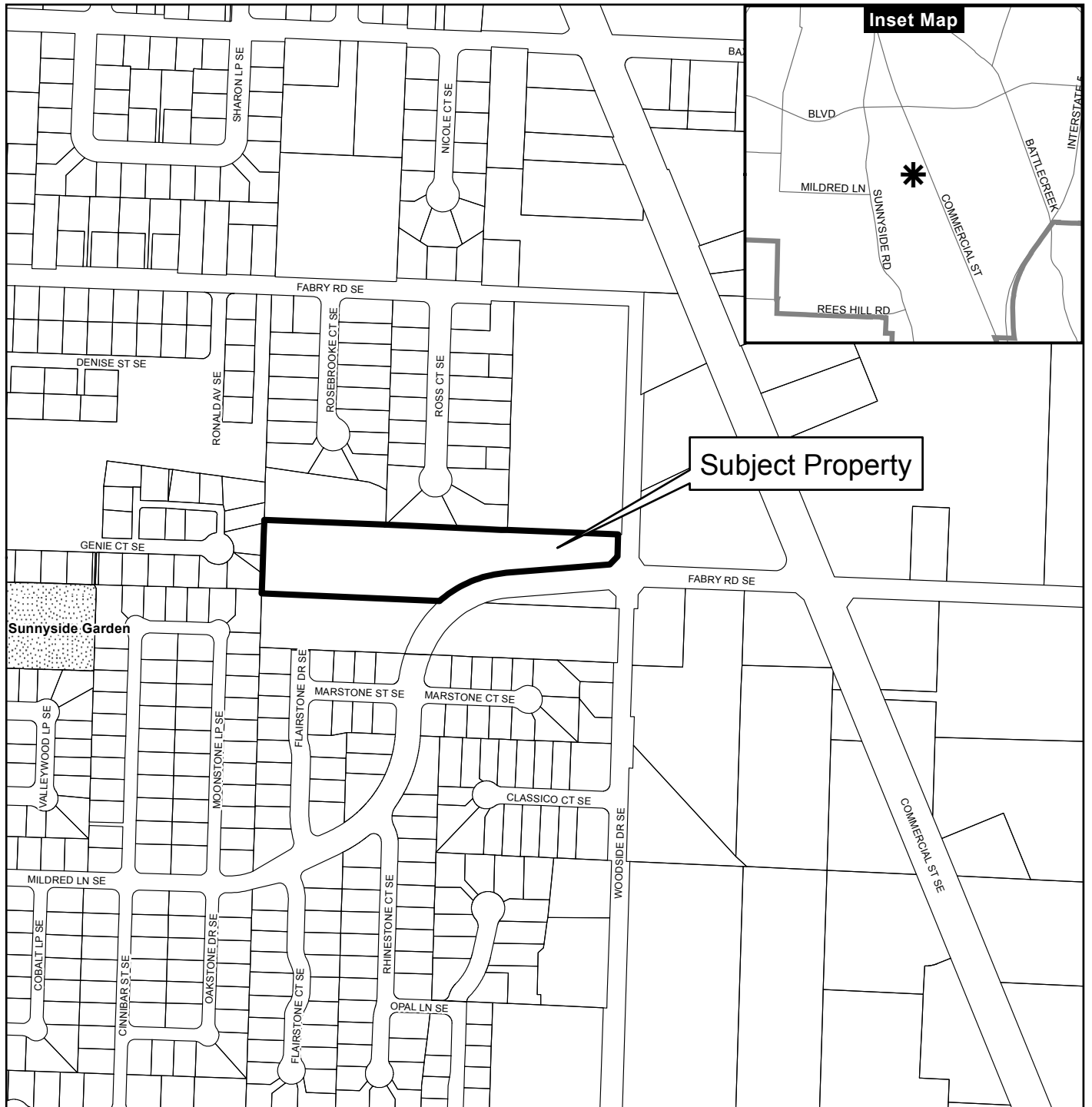


Vicinity Map 5611 Woodside Drive SE



Legend

- Taxlots
- Urban Growth Boundary
- City Limits
- Outside Salem City Limits
- Historic District
- Schools

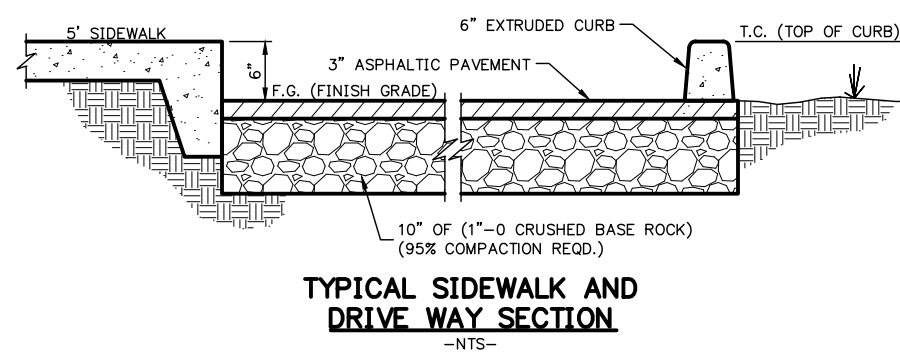
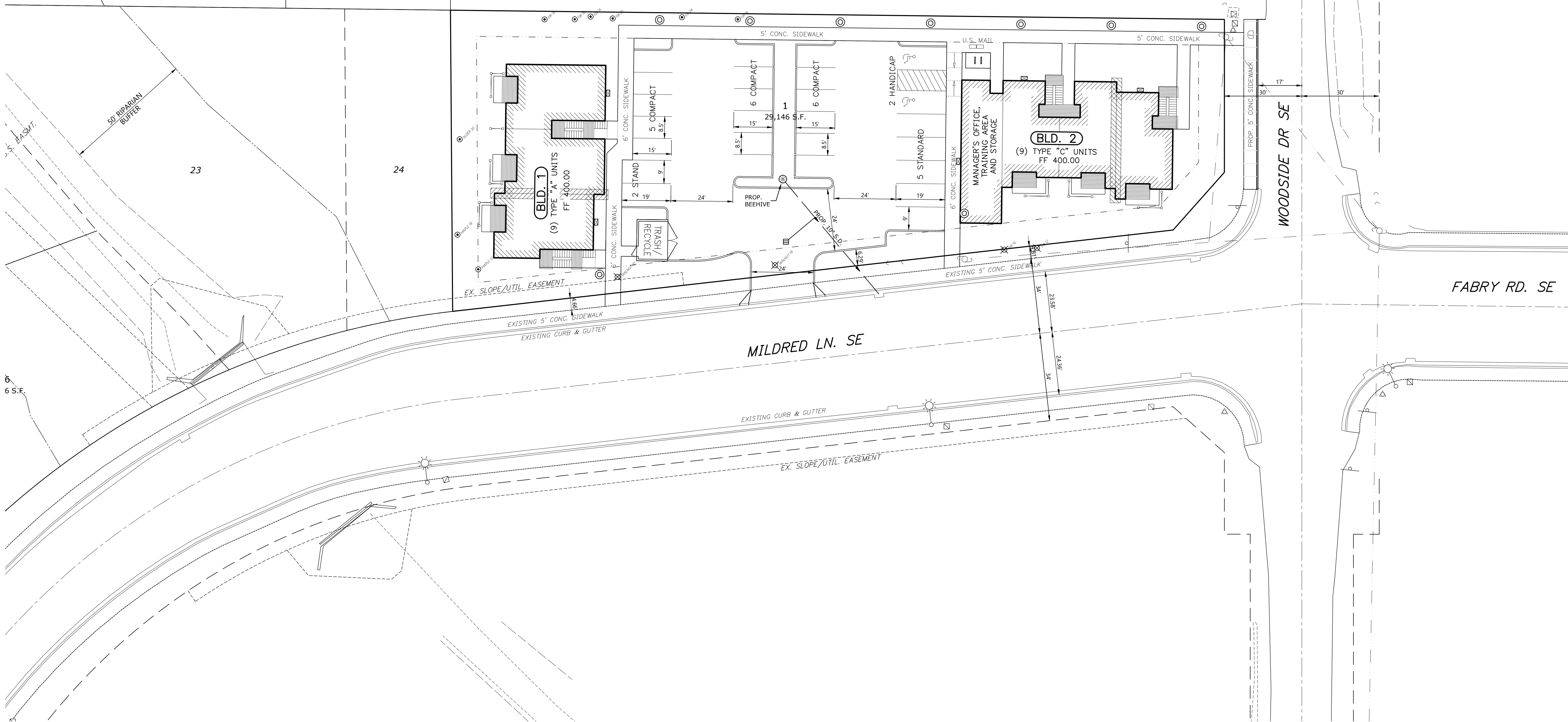
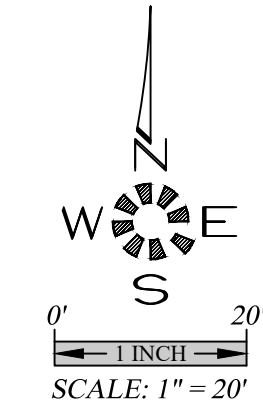
- Parks

CITY OF Salem
AT YOUR SERVICE
Community Development Dept.

0 100 200 400 Feet



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LIMITS OF DETENTION
10 YEAR STORM EVENT

- ⊙ 19 = EXISTING TREES
⊙ 19 = EXISTING TREE MAY BE REMOVED

18 TOTAL APARTMENT UNITS
9 TYPE "A" 2-Bd, 1-Ba (844 S.F.) UNITS
9 TYPE "C" 1-Bd, 1-Ba (728 S.F.) UNITS

26 TOTAL PARKING STALLS
7 STANDARD STALLS
17 COMPACT STALLS
2 HANDICAP STALLS

1 MANAGER'S OFFICE / STORAGE
1 TRASH / RECYCLE
1 PLAY AREA
1 U.S. MAIL BOX AREA

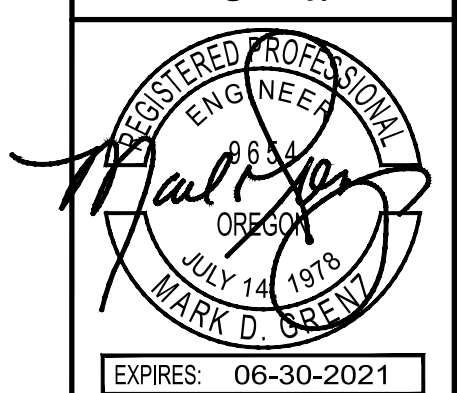
- ▨ - WALL PACK LIGHTS MOUNTED ON BUILDINGS
⊙ - POLE LIGHT MAXIMUM 14' TALL
⊙ - POST LIGHT MAXIMUM 5' TALL
▨ - LOCATION OF ELECTRICAL SEPARATION WALL
① - MAXIMUM 1:12 SLOPE ON SIDEWALK END RAMPS
▨ - 4 BICYCLE SPACES

PRELIMINARY SITE PLAN

CHARLENE'S HOUSE
APARTMENTS

NO CHANGES, MODIFICATIONS
OR REPRODUCTIONS TO BE
MADE TO THESE DRAWINGS
WITHOUT WRITTEN
AUTHORIZATION FROM THE
DESIGN ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER
GRAPHICAL REPRESENTATION.

DESIGN: M.D.G.
DRAWN: C.D.S.
CHECKED: J.J.G.
DATE: SEPT 19
SCALE: AS SHOWN

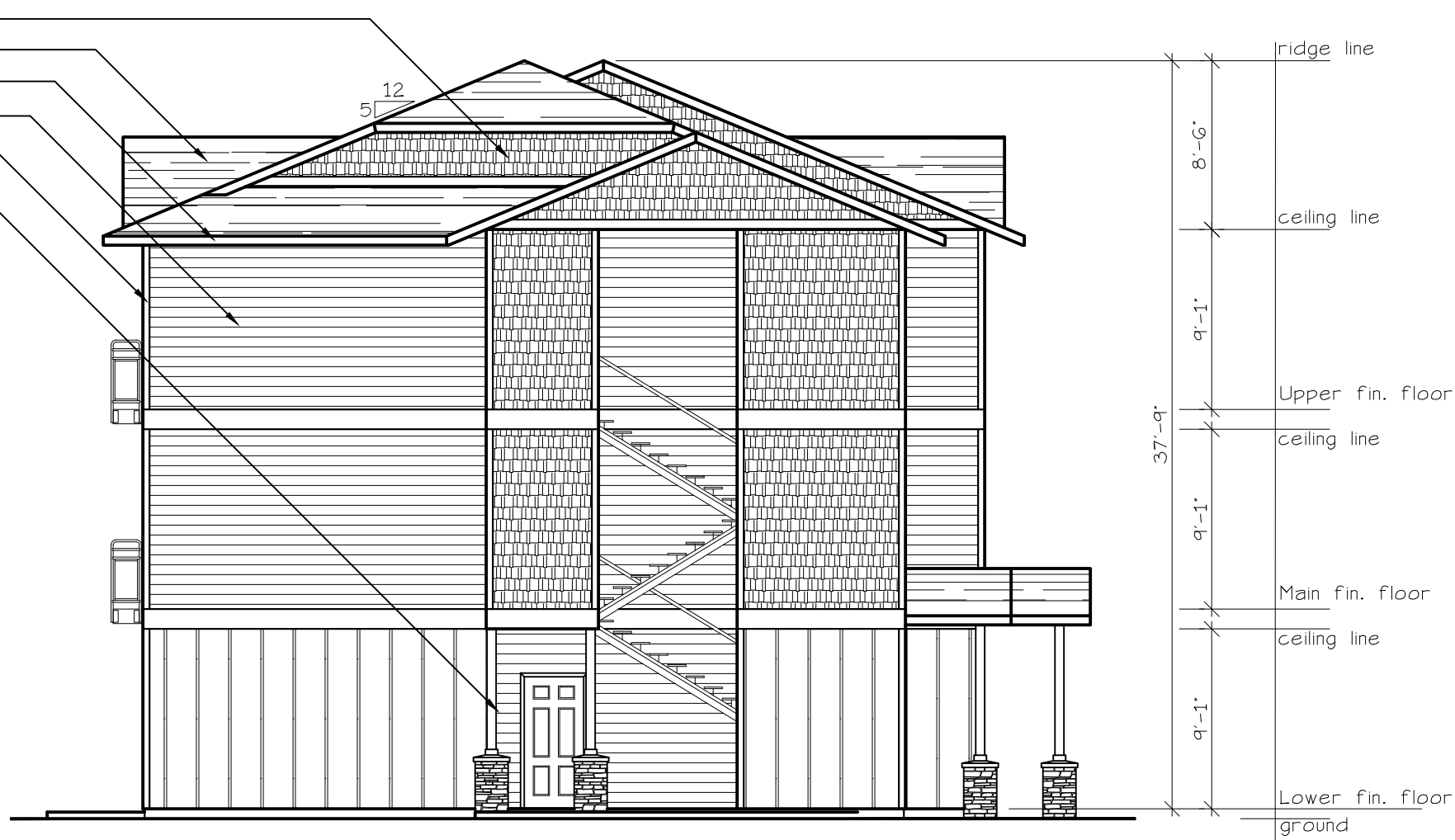


EXPIRES: 06-30-2021

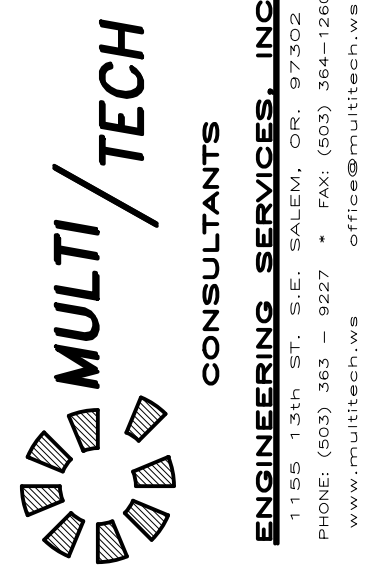
JOB # 6818

SDR3

J:\Box\6818-Charlene House\Drawings\A1.90ELEV.dwg 2/20/2020 11:41:39 AM. D:\wylor



ATTACHMENT B,
CONTINUED



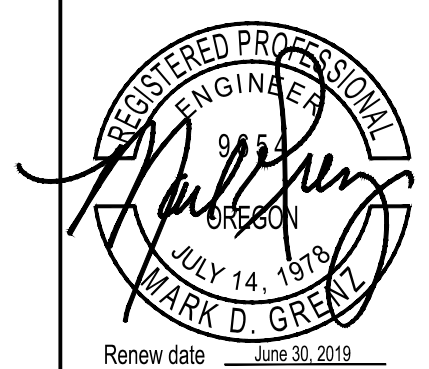
BUILDING
ELEVATIONS

CHARLENE'S HOUSE
APARTMENT COMPLEX

NO CHANGES, MODIFICATIONS OR
REVISIONS TO BE MADE TO
THIS DRAWING WITHOUT THE
AUTHORIZATION FROM THE DESIGN
ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER GRAPHICAL
REPRESENTATION.

Design: P.L.M.
Drawn: G.L.D.
Checked: M.D.G.
Date: Jan-20
Scale: AS SHOWN

JOB # 6818



A1.90



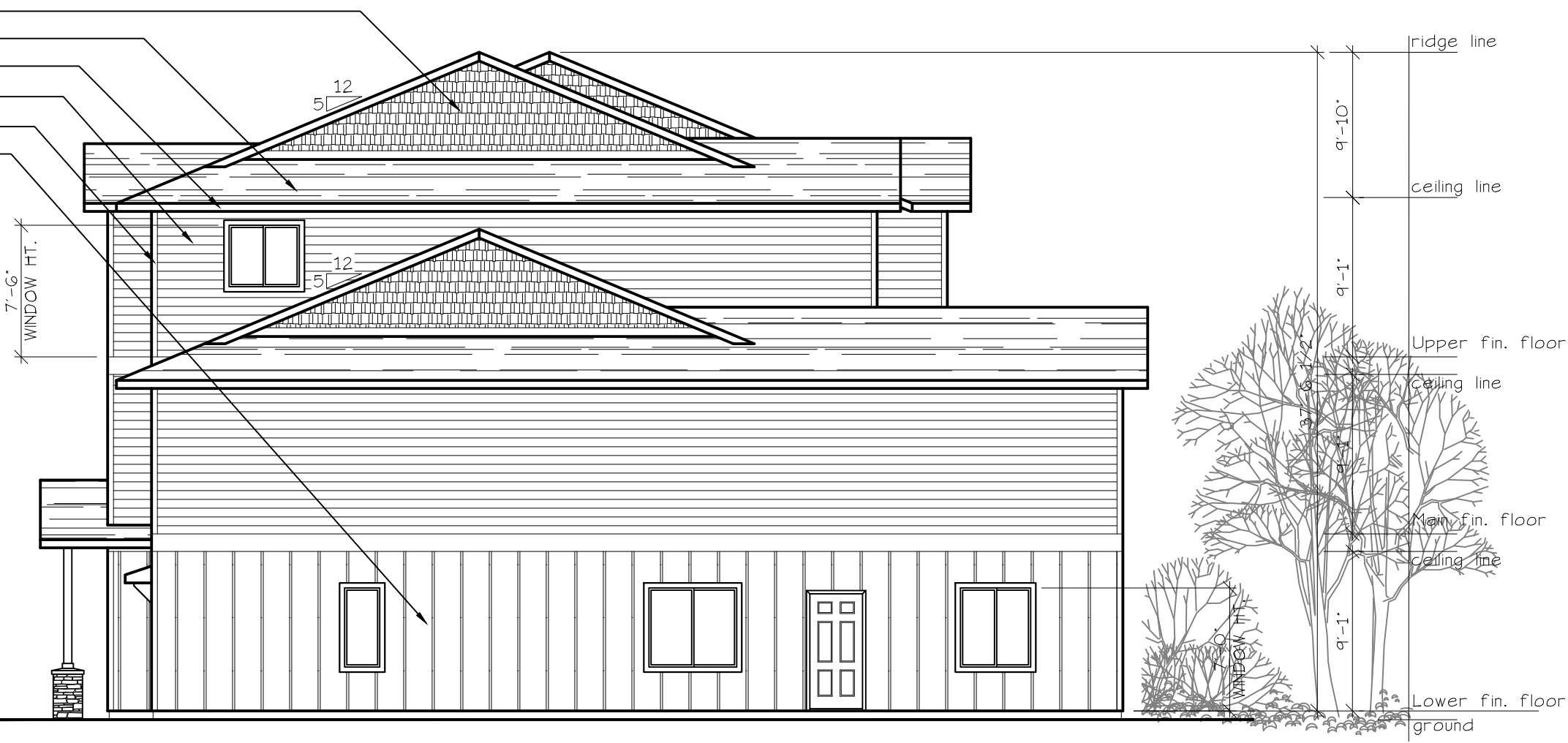
SOUTH ELEVATION (TYPE C UNITS)
SCALE: 1/8" = 1'-0"
BLD. 2



EAST ELEVATION (TYPE C UNITS)
SCALE: 1/8" = 1'-0"
BLD. 2



NORTH ELEVATION (TYPE C UNITS)
SCALE: 1/8" = 1'-0"
BLD. 2



WEST ELEVATION (TYPE C UNITS)
SCALE: 1/8" = 1'-0"
BLD. 2

NO CHANGES, MODIFICATIONS OR
REVISIONS TO BE MADE TO
DRAWINGS WITHOUT THE
AUTHORIZATION FROM THE DESIGN
ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER GRAPHICAL
REPRESENTATION.

Design: P.L.M.
Drawn: G.L.D.
Checked: M.D.G.
Date: Jan-20
Scale: AS SHOWN

JOB # 6818

REGISTERED PROFESSIONAL
ENGINEER
6818
JAN 14 1978
MARK D. GARET
Renew date 06-30-2019

Charlene's House-Apartments

Design Review

Revised-May 14, 2020

The following statement addresses the applicable Design Review **Guidelines** in the SRC Chapter 702 (Multiple Family Design Review Guidelines and Design Review Guidelines) and the requirements under the IC Zone District. Information provided on the site plans for the Design Review application further address applicable code requirements.

On March 4, 2019, a Design Review Pre-Application Conference (PRE-AP19-13) was held with the applicant and City staff to discuss the development of the subject property.

Proposal (Sheet SDR3):

The subject property is 0.67 acres in size, zoned IC, and located at 5611 Woodside Drive (083W14CB/Tax Lot 2400). The subject property is Parcel 2 of Partition Case No. 19-11, dated August 29, 2019.

The applicant is proposing a development consisting of 18-apartment units as shown on the site plans.

The applicant is requesting to meet all Design Review Guidelines.

Industrial Commercial (IC)-SRC Chapter 551

Setbacks (Sheet SDR3): Setbacks are shown on the tentative plan.

Northwest:	20-foot (Building 1) setback; (RM2 zoned/existing residential uses)
Northeast:	24-foot (Building 2) setback; (RM2 zoned/existing residential uses)
East:	Adjacent Woodside Drive, 18-foot (building)
Southeast:	Adjacent Mildred Lane, 10-foot (Building 2) setback
Southwest:	Adjacent Mildred Lane, 10-foot (Building 1) setback
West:	15-foot (Building 1) setback; (RA zoned/vacant land)

Maximum Height (Sheets A1.8 and A2.8): Maximum building height allowed in the IC zone is 70'. Both proposed buildings are in compliance with the requirements of the Code.

*Building 1 is 37.9 feet in height (measured to the highest point)

*Building 2 is 37.6 feet in height (measured to the highest point)

Therefore, the buildings are in compliance with the building height requirement.

Stormwater (Sheet SDR5): As stated on the Grading and Drainage Plan, the proposal is treating at least 80% hard surface with Green Water Infrastructure. Therefore, meeting the requirements of the Public Works Department.

Multiple Family Design Review Guidelines- Chapter 702

702.015 Common Open Space Guidelines (Sheet SDR4): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The guidelines are also intended to ensure that open space is an integral part of the overall development design.

The minimum open space area required for this development is 30% of the site. The portion of the subject property being developed is 29,145 (0.67 acres) square feet in size with 9,991 square feet of landscaped open space. Therefore, totaling 34% open space.

Therefore, this guideline has been met. See attached site plans and open space plan.

702.015 Private Open Space Guidelines (Sheets A1.3, A1.4, A2.3, and A2.4): Each unit will have private open space as required by code. Ground floor units will have patio areas that are 96 square feet in size, with no dimension less than 6 feet. All second and third story units will have balconies/decks that are a minimum 60 square feet in size. All private open space areas are located contiguous to the dwelling unit and will be screened with a 5-foot-high sight obscuring wood fence or landscaping. This private open space includes the patios and balconies/decks. Therefore, this guideline has been met.

702.020 Landscaping Guideline (Sheets SDR2, SDR3, L1.1 and L1.2): The subject property does abut RA zoned property to the west. Landscaping is being provided adjacent all property lines and within the interior of the development. Landscaping has been provided throughout the site as identified on the landscape plans. A minimum of 1 tree will be planted for every 2,000 square feet of the site. Trees and vegetation have been provided throughout the development as shown on the landscape plans. There is 9,991 square feet of landscaped area throughout the site. Therefore, 34% of the site is landscaped. Landscape plans have been provided and demonstrate how the landscape guidelines have been met.

A permanent underground irrigation system will be provided when development plans are final.

There are 19 trees located on the subject property. Due to the size of the site and the location of the trees, all nineteen (19) trees are proposed to be removed. There are no significant trees located on the site.

New trees will be provided through the site as shown on the landscape plans.

702.020 Street Frontage Guidelines (Sheet L1.1 and L1.2): The landscape plans identify how this standard is met. Trees will be provided along the street frontage with one canopy tree per 50 linear feet. See attached landscaped plans. Therefore, this standard has been met.

702.020 Building Exterior Guidelines (Sheet L1.1 and L1.2): The exterior of the buildings will be landscaped to provide a visually appealing development. Trees and shrubs will be planted in front of and around all buildings as shown on the landscape plans. This will help to provide shading and privacy for residents. Therefore, this standard has been met.

702.020 Privacy Guidelines (Sheet L1.1, L1.2, A1.3 and A2.3): All ground level private open space areas (patios) will be screened and separated with fencing. This will help to provide privacy for ground level residents. Therefore, this standard has been met.

702.020 Landscape Parking Guidelines (Sheet SDR3, L1.1, and L1.2): In order to take into consideration circulation, pedestrian access, landscaping, and the requirements of the code, the parking areas have been carefully designed. All parking areas are landscaped as required, and separated by landscaped bays that are a minimum of 18-feet in width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds.

Interior Parking Lot Landscaping: SRC 806.035(d)(2) requires a minimum of 5 percent landscaping within parking areas less than 50,000 square feet in size and a minimum of 8 percent landscaping within parking areas 50,000 square feet and greater in size. The parking area within the development is 9,639 (parking and driveways) square feet in size with 1,466 square feet (15%) of landscaping.

Therefore, this standard has been met. See attached site plans.

702.025 Crime Prevention Guidelines (Sheet SDR3, A1.3, A1.4, A1.8, A2.3, A2.4, and A2.8): Safety of the residents is very important, and all requirements are met to assure safety and compliance with code. There are no fences or plant materials located in areas within the development that obstruct visibility. All landscaping adjacent to open space areas will not exceed 3 feet in height.

All buildings have windows provided in habitable rooms and windows that face the parking lots and open space areas. This helps provide an eye on the development. Lighting on the buildings and along the sidewalks will be provided as well.

Therefore, this standard has been met. See attached site plans.

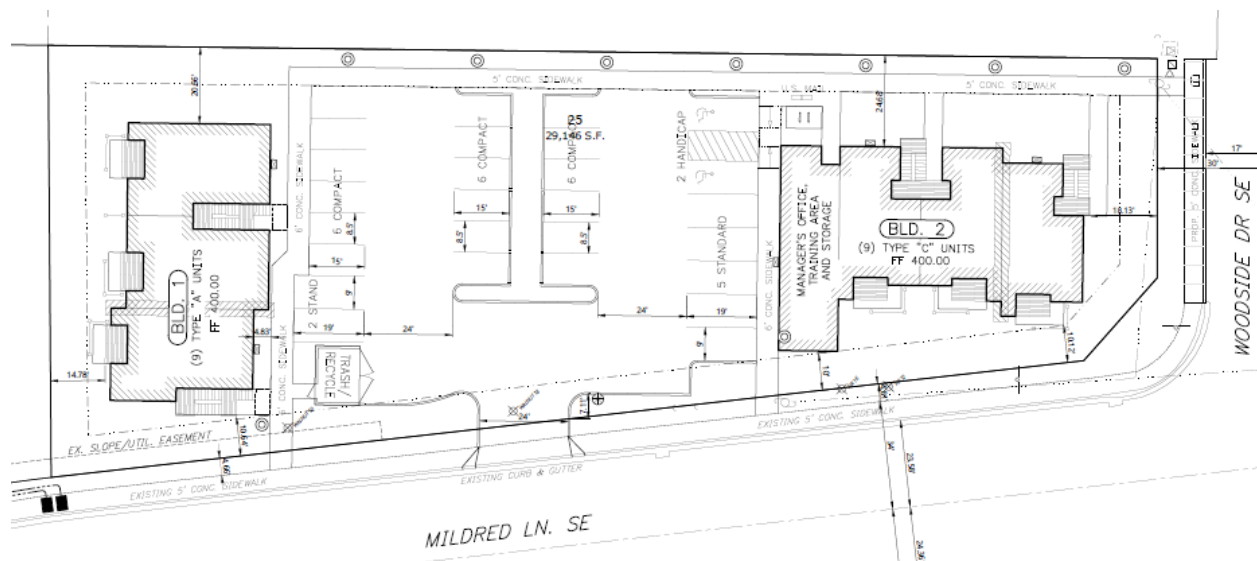
702.030 Parking, Site Access, and Circulation Guidelines (Sheet SDR3):

The subject property has street frontage on Woodside Drive (east) and Mildred Lane (south) along the property lines. Internal accessways are proposed within the development.

All parking areas greater than 6,700 square feet in area are within the requirements of the code and are separated by planter bays that are a minimum of 18 feet in width. The layout of the parking areas has been taken into consideration and provides for safe and efficient circulation throughout the development.

As shown on the site plan, all buildings are not separated from all pathways by a minimum 10-foot setback. The intent of this standard is to provide privacy for residents. However, due to the site of the site, this standard cannot be met. The development does provide at 5-foot setbacks between the buildings and pathways. All the pathways connect the buildings, open space, parking areas, and surrounding uses. Therefore, providing privacy and meeting the intent of the Code and the Guidelines.

The parking areas along Mildred Lane do not meet the 20-foot setback standard. The intent of this standard is to provide an adequate setback for safety and visual reasons. Due to the required 5-foot landscape strip and sidewalk along Mildred Lane, a 20-foot setback on-site is difficult to provide while adequately developing the site. However, as shown on the site plan, the parking is setback at least 20 feet when including the setback, 5-foot landscape strip, and the 5-foot sidewalk.



Therefore, this guideline has been met.

Parking: The development is for an 18-unit apartment complex. Code requires 1.5 vehicle parking spaces per every 1 dwelling units. The applicant is required to provide a minimum of 27 on-site vehicle parking spaces. As shown on the site plan, 27 on-site parking spaces are being provided. Required setbacks and landscaping requirements on a lot this size make it difficult to provided additional parking.

Total:

7	Standard Parking Stalls
18	Compact Parking Stalls
2	Handicap Parking Stalls
27	Total Parking Stalls

Adequate parking has been provided throughout the development with 1.5 parking spaces per dwelling unit.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle parking is also required on site. The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents.

702.030 Pedestrian Site Access Guidelines (Sheet SDR3): The internal pedestrian circulation system consists of hard 6-foot wide surfaced sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, the trash disposal area, and adjacent properties. The pedestrian system connects the buildings to the public sidewalk system, adjacent properties, and to the future park to the north as required.

The sidewalks are raised above the surface of the travel lanes. This provides a clear separation between vehicles and pedestrians. Any pedestrian pathways that cross the parking area or driveways will be marked and a minimum of 6 feet wide. The pedestrian pathways will be lighted. Proposed pedestrian sidewalk connections are illustrated on the tentative site plan.

The design of pedestrian circulation systems shall provide clear and identifiable connections within the multiple family development and to adjacent uses and public streets/sidewalks. The proposed development provides safe and convenient bicycle and pedestrian access from within the development to adjacent residential areas.

Therefore, this standard has been met.

702.035 Building Mass and Façade Design Guidelines (Sheet SDR3, A1.8, and A2.8): These guidelines are intended to promote building and site design that contributes positively to a sense of neighborhood and to the overall streetscape by carefully relating building mass, entries and yards to public streets.

The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. Both buildings within the development will not exceed 150 feet in length. The height and length of the buildings and structures conform to the measuring requirements in code.

All buildings face the interior of the lot. The rear side of Building 2 faces Mildred Lane to the south of the site. The street side of this building (rear) will be designed to be visually appealing, by providing similar design as is being provided for the front building facade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions of the building facing the right-of-way.

Varied materials and textures are being used on the building facade. The applicant has provided building elevations to show how this is being complied with. The materials used on the front, rear, and sides of the apartments are the same; shake siding, trim board, lap siding, and stone around the pillars. See attached building elevations. Therefore, this guideline has been met.

702.035 Compatibility Guidelines (Sheet SDR3): The subject property does abut RA zoned property to the west. Setbacks are shown on the tentative plan.

Northwest:	20-foot (Building 1) setback; (RM2 zoned/existing residential uses)
Northeast:	24-foot (Building 2) setback; (RM2 zoned/existing residential uses)
East:	Adjacent Woodside Drive, 18-foot (building)
Southeast:	Adjacent Mildred Lane, 10-foot (Building 2) setback
Southwest:	Adjacent Mildred Lane, 10-foot (Building 1) setback
West:	15-foot (Building 1) setback; (RA zoned/vacant land)

Building 1 has an average building height of about 33.6' in height. Therefore, Building 1 is required to provide a 33.6' setback along the west property line, where only a 15-foot setback is being provided. The intent of setbacks is to provide a privacy buffer for residents and adjacent residents. The 15-foot setback between Building 1 and the adjacent RA zone property will provide landscaping and a 6-foot high sight obscuring fence. All of which will help to provide privacy.

The primary entrances for each individual unit are provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use

of roofline offsets and covered entry ways, promote a positive sense of neighborhood. All building entrances face the internal street/parking system.

Mildred Lane

The subject property has 241 feet of buildable width (this excludes required side setbacks and driveway) along Mildred Lane. Buildings 1 and 2 are located on the setback line along Mildred Lane. Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 138.5 feet of the buildable width along the street frontage. Therefore, occupying 55% of the buildable width of street frontage along Mildred Lane.

All roof-mounted equipment will be screened and integrated into the building design. Further review of this requirement will take place at the time of building permits.

Therefore, this guideline has been met.

702.035 Building Articulation Guidelines (Sheet SDR3, A1.8 and A2.8): All buildings have entrances physically and visually connected to the internal public sidewalk system and the parking lots. All external stairways are recessed into the buildings. Therefore, physically and visually incorporating them into the building's architecture design.

The primary entrances for each individual unit are provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood.

The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. All buildings will have a minimum of 4-foot offsets, balconies, patios, eaves, and windows incorporated into the design of each of the buildings. Therefore, this standard has been met. See building elevations.

702.040 Recycling (Sheet SDR3): There is one trash/recycle area provided within the development. The trash receptacle is accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle area will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided. Therefore, meeting this standard.

Conclusion: The applicant is requesting to meet all Design Review Guidelines as outlined above.

Charlene's House-Apartments

Conditional Use

January 22, 2020

SITE HISTORY:

On March 4, 2019, a Design Review Pre-Application Conference (PRE-AP19-13) was held with the applicant and City staff to discuss the development of the subject property.

PROPOSAL:

Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.

The subject property is 0.67 acres in size, zoned IC, and located at 5611 Woodside Drive (083W14CB/Tax Lot 2400). The subject property is Parcel 2 of Partition Case No. 19-11, dated August 29, 2019.

The applicant is proposing a development consisting of 18-apartment units as shown on the site plans. The applicant is applying for a Conditional Use for the construction of an apartment complex.

CONDITIONAL USE CRITERIA:

SRC 240.005(d) - An application for a Conditional Use permit shall be granted if all of the following criteria are met:

- (1) The proposed use is allowed as a conditional use in the zone;*
- (2) The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions; and*
- (3) The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.*

APPLICANT'S REASONS ADDRESSING CHAPTER 240.005(d):

- (1) Under SRC 551.005(a)-Table 551-1, multiple family dwellings are allowed within the IC zone with a Conditional Use permit.
- (2) The proposed apartments will have little to no impact on the neighborhood. The property is bordered on the north by existing multi-family development and to the west by RA zoned property. Therefore, the proposed development will be consistent with the surrounding development and zones. There are only 18 units proposed. The small development (18 units) and adjacent uses/zoning help eliminate any impacts on the surrounding neighborhood.

The proposed apartments will have less an impact on the area then an industrial use that is allowed within the IC zone.

(3) The surrounding land uses within the vicinity are zoned and used as follows.

Northwest:	20-foot (Building 1) setback; (RM2 zoned/existing residential uses)
Northeast:	24-foot (Building 2) setback; (RM2 zoned/existing residential uses)
East:	Adjacent Woodside Drive, 18-foot (building)
Southeast:	Adjacent Mildred Lane, 10-foot (Building 2) setback
Southwest:	Adjacent Mildred Lane, 10-foot (Building 1) setback
West:	15-foot (Building 1) setback; (RA zoned/vacant land)

The apartments will be required to go through Site Plan/Design Review, which requires open space and landscaping at a higher percent than what a commercial use would be required to provide. Amenities like landscaped open space will help with the visual appeal of this area and reduce impacts on the neighborhood. The design standards are in place to help ensure compatibility with adjacent uses.

The proposed apartment development will provide pedestrian paths throughout the site and to Woodside Drive and Mildred Lane. The pedestrian paths will provide access and circulation to the surrounding neighborhoods. Therefore, increasing their livability as well.

Therefore, this criteria will be complied with through the Site Plan/Design Review process.

Charlene's House-Apartments

Class 3-Site Plan Review

Revised-April 9, 2020

SRC 220.005(f)(3) Class 3 Site Plan Review Criteria:

(A) The application meets all applicable standards of the UDC;

Applicant Findings: The applicant is requesting to meet all Design Review Guidelines.

All guidelines have been addressed and met as outlined within the Design Review narrative and on the site plans.

The subject property is 0.67 acres in size, zoned IC, and located at 5611 Woodside Drive (083W14CB/Tax Lot 2400). The subject property is Parcel 2 of Partition Case No. 19-11, dated August 29, 2019.

The applicant is proposing a development consisting of 18-apartment units as shown on the site plans.

All applicable guidelines have been outlined below and on the attached site plans.

Industrial Commercial (IC)-SRC Chapter 551

Setbacks (Sheet SDR3): Setbacks are shown on the tentative plan.

Northwest:	20-foot (Building 1) setback; (RM2 zoned/existing residential uses)
Northeast:	24-foot (Building 2) setback; (RM2 zoned/existing residential uses)
East:	Adjacent Woodside Drive, 18-foot (building)
Southeast:	Adjacent Mildred Lane, 10-foot (Building 2) setback
Southwest:	Adjacent Mildred Lane, 10-foot (Building 1) setback
West:	15-foot (Building 1) setback; (RA zoned/vacant land)

Maximum Height (Sheets A1.8 and A2.8): Maximum building height allowed in the IC zone is 70'. Both proposed buildings are in compliance with the requirements of the Code.

*Building 1 is 37.9 feet in height (measured to the highest point)

*Building 2 is 37.6 feet in height (measured to the highest point)

Therefore, the buildings are in compliance with the building height requirement.

Stormwater: As stated on the Grading and Drainage Plan, the proposal is treating at least 80% hard surface with Green Water Infrastructure. Therefore, meeting the requirements of the Public Works Department.

(B) *The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;*

Applicant Findings: A TGE form has been submitted as part of this packet to determine if a TIA is needed. The subject property has street frontage on Woodside Drive to the east and Mildred Lane to the south.

As shown on the site plan, safe and efficient access and circulation has been provided into and throughout the development. The proposed development 26-foot wide driveways throughout the site. The driveways provide circulation throughout the site and onto the surrounding street system.

The design of on-site circulation is clearly identifiable, safe, pedestrian friendly and interconnected. The subject property is located in a developing area where improved streets and sidewalks continue as required by the City. Improved access is required by code. Approval does not adversely affect the safe and healthful development of any adjoining land or access thereto.

(C) *Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and*

Applicant Findings: Parking: The development is for an 18-unit apartment complex. Code requires 1.5 vehicle parking spaces per every 1 dwelling units. The applicant is required to provide a minimum of 27 on-site vehicle parking spaces. As shown on the site plan, 26 on-site parking spaces are being provided. A parking adjustment has been requested.

Total:

7	Standard Parking Stalls
17	Compact Parking Stalls
2	Handicap Parking Stalls
26	Total Parking Stalls

Adequate parking has been provided throughout the development with 1.5 parking spaces per dwelling unit.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development. Parking areas and driveways have been designed to City standards and provide safe circulation throughout the development.

Bike racks have been provided on the site and located in a convenient location for the residents.

The design of pedestrian circulation systems shall provide clear and identifiable connections within the multiple family development and to adjacent uses and public streets/sidewalks. The proposed development provides safe and convenient bicycle and pedestrian access from within the development to adjacent residential areas.

Therefore, this criteria has been met.

(D) *The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.*

Applicant Findings: Utility plans have been provided that show how the site will be served with City water, sewer, storm water facilities, and other utilities appropriate to the development.

CLASS-2 ADJUSTMENT

The applicant is requesting a Class-2 Adjustment to Section 806 (Off-Street Parking).

The development is for an 18-unit apartment complex. Code requires 1.5 vehicle parking spaces per every 1 dwelling units. The applicant is required to provide a minimum of 27 on-site vehicle parking spaces. As shown on the site plan, 26 on-site parking spaces are being provided. Required setbacks and landscaping requirements on a lot this size make it difficult to provided additional parking. Therefore, the applicant has requested a Class-1 Adjustment to the parking requirements.

Total:

7	Standard Parking Stalls
17	Compact Parking Stalls
2	Handicap Parking Stalls
26	Total Parking Stalls

Adjustment Criteria-SRC 250.005(d)(2) Criteria

(A) *The purpose underlying the specific development standard proposed for adjustment is:*

- (i) Clearly inapplicable to the proposed development; or***
- (ii) Equally or better met by the proposed development.***

(B) *If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.*

(C) *If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.*

Applicant Findings:

- (A) The purpose of having adequate parking on-site is to provide enough parking for residents and not have overflow parking into adjacent neighborhoods. The development is for an 18-unit apartment complex. Code requires 1.5 vehicle parking spaces per every 1 dwelling units. The applicant is required to provide a minimum of 27 on-site vehicle parking spaces. As shown on the site plan, 26 on-site parking spaces are being provided. The applicant is only requesting an adjustment for a reduction of 1 parking space. There will be 26 on-site parking available to residents, which means 1.44 parking spaces are available to each unit.

Under the new parking standards, the proposed development would only be required to have 23 on-site parking spaces. By providing 26 on-site parking spacing the proposal meets the purpose of the code and is clearly satisfied by the proposed development. Therefore, development standard proposed for adjustment is better met by the reduction in parking.

- (B) The proposed development is located in a residential area. However, with more than adequate parking spaces on-site, the adjustment will not impact the surrounding existing or

potential developments in the area. Adequate parking is provided and is only a reduction of 1 parking space. The reduction of 1 parking space will still provide 1.44 spaces per unit. The parking provided on-site exceeds the newly adopted parking requirements.

Therefore, the adjustment to parking will not create parking overflow issues and will have no effect on the surrounding uses.

- (C) There are more than one adjustment being requested for this proposed development. However, the adjustments will allow the site to be fully developed will being consistent with the intent of the zone.

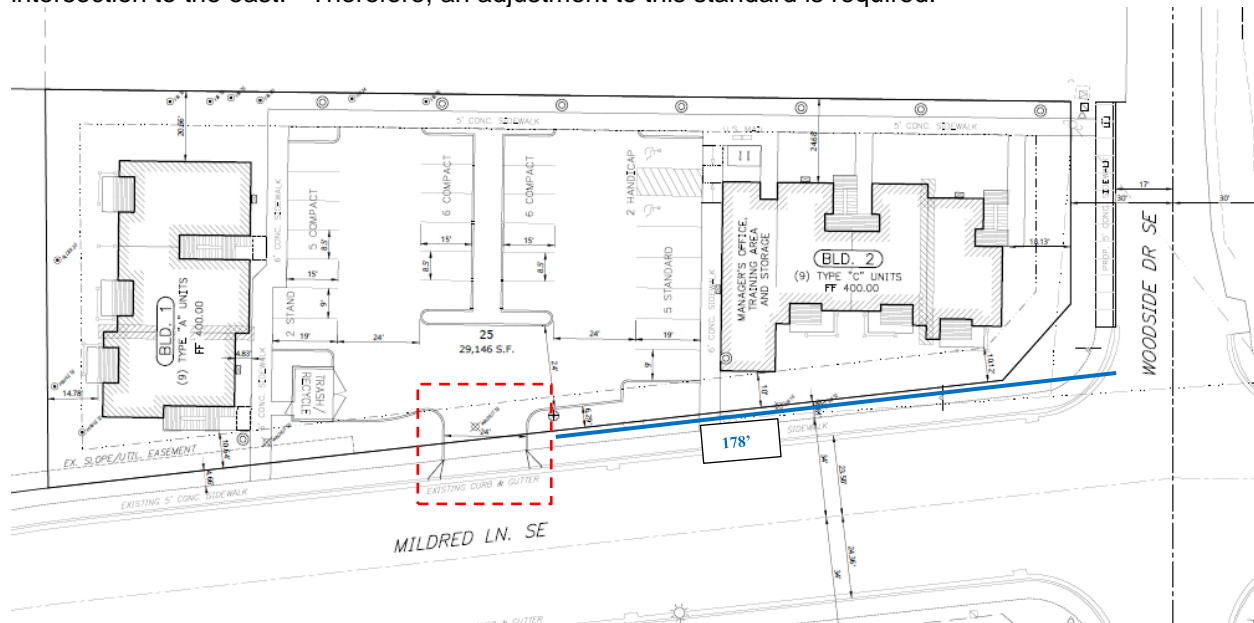
Charlene's House Apartments

Adjustment Class-2 Application

Proposal:

The subject property is 0.67 acres in size, zoned IC, and located at 5611 Woodside Drive (083W14CB/Tax Lot 2400). The subject property is Parcel 2 of Partition Case No. 19-11, dated August 29, 2019. The applicant is proposing a development consisting of 18-apartment units as shown on the site plans.

The applicant is proposing a driveway that is located closing than 370 feet from Woodside Drive intersection to the east. Therefore, an adjustment to this standard is required.



The applicant is requesting an adjustment greater than 20% adjustment to SRC 804.035(d):

(d) Spacing. Driveway approaches providing direct access to a major or minor arterial shall be no less than 370 feet from the nearest driveway or street intersection, measured from centerline to centerline.

Adjustment Criteria-SRC 250.005(d)(2) Criteria

(A) The purpose underlying the specific development standard proposed for adjustment is:

- (i) Clearly inapplicable to the proposed development; or*
- (ii) Equally or better met by the proposed development.*

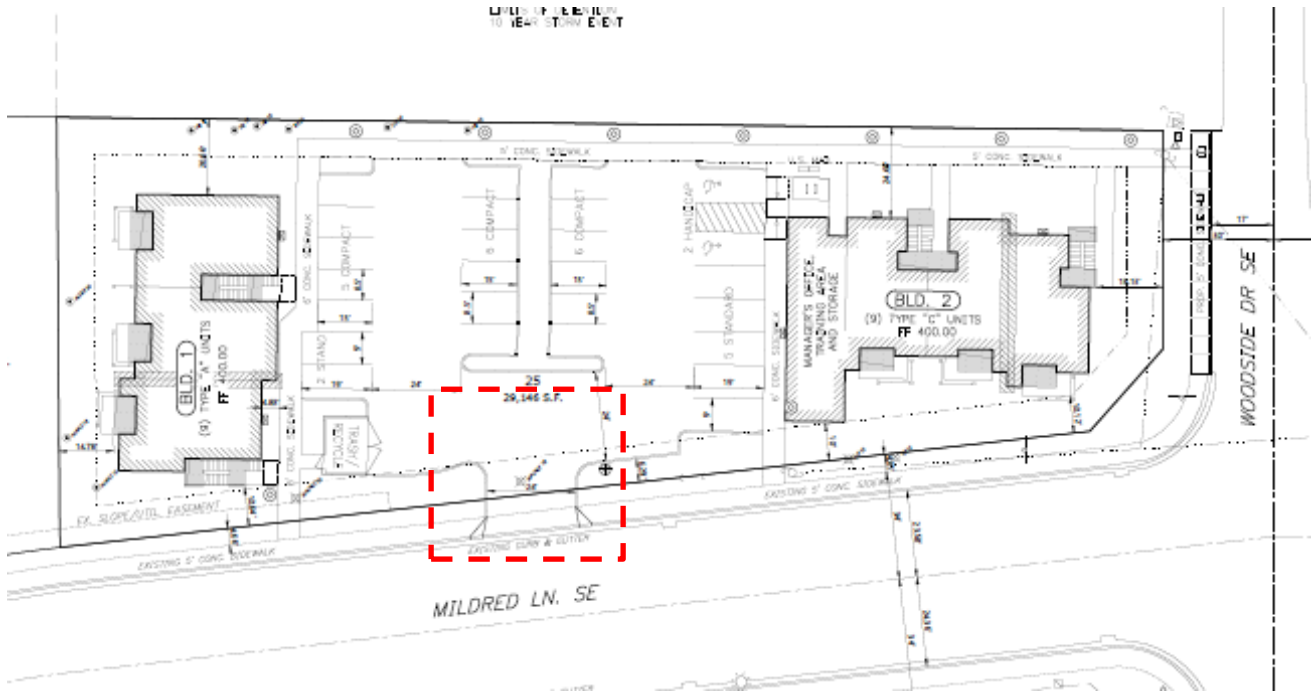
(B) If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.

(C) If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.

Applicant Findings:

- (A) One two-way driveway is proposed onto Mildred Lane from the development site. The proposed driveway appears to be located approximately 178 feet from the Mildred Lane and Woodside Intersection. Due to the size and location of the subject property, locating the driveway further away from the intersection is not feasible or safe. Relocating the driveway would require the elimination of parking spaces and would provide an unsafe visual situation near the curve of Mildred Lane. Therefore, this standard is clearly inapplicable to the proposed development. After review of the site and layout, it was determined that the most feasible driveway location is as shown on the site plan, and is clearly better met by the proposed. Therefore, the applicant is requesting an Adjustment to this requirement.
- (B) The subject property is located within a residential zone. The subject property is zoned IC and surrounded by residential uses. The location of the driveway as shown on the site plan will not have an impact on residential uses or any of the other uses in the area. The location will provide a safe and convenient one-way entrance and exit out of the development. The location does not create any vision or traffic hazards onto Mildred Lane as shown on the site plans. Therefore, the driveway location will have no effect on the proposed use or surrounding uses.
- (C) The applicant is requesting more than one adjustment. The requested adjustment will not have any effect on the overall purpose of the zone. The site will be developed to Code and designed to City standards. Therefore, the purpose of the zone will be met.

Class 2-Driveway Approach Permit



SRC 804.025 (d) Criteria. A Class 2 Driveway Approach Permit shall be granted if:

(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;

Applicant Response: The subject property is 0.67 acres in size, zoned IC, and located at 5611 Woodside Drive (083W14CB/Tax Lot 2400). The subject property is Parcel 2 of Partition Case No. 19-11, dated August 29, 2019.

The applicant is proposing a development consisting of 18-apartment units as shown on the site plans.

The proposed development will have access onto Mildred Lane to the south of the property. Mildred Lane is designated as a 'minor arterial' street on the Salem Transportation System Plan. The development will not have access onto Woodside Drive abutting the east property line of the site. As shown on the site plan the driveway is required for access to the site and is in compliance with design standards.

(2) No site conditions prevent placing the driveway approach in the required location;

Applicant Response: The location of the driveway was taken into consideration prior to laying the site out. Access onto Woodside Drive to the east is not allowed, so all access for the development is taken to and from Mildred Lane. The location of the proposed driveway takes into consideration the layout of the site. Therefore, all factors were taken into consideration and there are no conditions on the site that prevent the driveway approach.

(3) The number of driveway approaches onto an arterial are minimized;

Applicant Response: The driveway approach is onto Mildred Lane, which is an arterial street. Access onto Woodside Drive is not permitted, therefore, the driveway approach is onto Mildred Lane.

(4) The proposed driveway approach, where possible:

(A) Is shared with an adjacent property; or

(B) Takes access from the lowest classification of street abutting the property;

Applicant Response to (4)(B): The subject property is located on Mildred Lane to the south and Woodside Drive to the east. Woodside Drive is designated as a 'local' street, however, access onto Woodside Drive is not permitted due to safety issues. A driveway approach onto Woodside Drive would not meet separation standards with the intersection. Therefore, access onto the local street would not be safe or feasible.

There are no adjacent driveways to share access with. Therefore, there is no lower classified streets abutting the property that can provide safe and efficient access.

Therefore, this criterion has been met.

(5) The proposed driveway approach meets vision clearance standards;

Applicant Response: Through the pre-app process, the applicant has been working with Public Works to ensure that the driveway approach is in the required location and meets vision clearance standards. As shown on the site plan, this criterion has been met.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Applicant Response: The driveway approach does not create traffic hazards. As shown on the site plan, this criterion has been met.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Applicant Response: Public Works has had the opportunity to review the site plan for any adverse impacts. No adverse impacts to the vicinity have been identified. As shown on the site plan, the location of the driveway will not have any impacts on the subject property or adjacent properties. This criterion has been met.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required locations to minimize impacts to adjacent streets and intersections. As shown on the site plan, this criterion has been met.


(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required location to help balance the adverse impacts to residentially zoned property. The location of the proposed driveway takes into consideration the location of the streets adjacent to the site and access onto Mildred Lane. As shown on the site plan, this criterion has been met.



MEMO

TO: Brandon Pike, Planner I
Community Development Department

FROM: Glenn J. Davis, PE, CFM, Chief Development Engineer 
Public Works Department

DATE: June 29, 2020

**SUBJECT: PUBLIC WORKS RECOMMENDATIONS
DR-CU-SPR-ADJ-DAP20-02 (20-102373-RP)
5611 WOODSIDE DRIVE SE
18-UNIT MULTI-FAMILY DEVELOPMENT**

PROPOSAL

A conditional use permit, Class 3 design review, Class 3 site plan review, and Class 2 driveway approach permit application for a new multiple-family residential development consisting of two 9-unit buildings, with associated site improvements including an off-street parking area and common open space. The application also includes a Class 2 adjustment request to reduce the minimum spacing between the proposed driveway approach and Woodside Drive SE from 370 feet to approximately 200 feet. For property approximately 0.67 acres in size, zoned IC (Industrial Commercial), and located at 5611 Woodside Drive SE (Marion County Assessor map and tax lot number(s): 083W14CB / 02400).

RECOMMENDED CONDITIONS OF APPROVAL

1. Record the partition plat for partition case number PAR19-11.
2. Construct a half-street improvement along the frontage of Woodside Drive SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803. The sidewalk along Woodside Drive SE shall be located parallel to and one foot from the adjacent right-of-way pursuant to SRC 803.035(l)(2)(A).
3. Install street trees to the maximum extent feasible pursuant to SRC Chapter 86.
4. Extend an 8-inch sewer main from the terminus of the existing sewer main in Woodside Drive SE to the northern boundary of the subject property.
5. Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Code authority references are abbreviated in this document as follows: *Salem Revised Code* (SRC); *Public Works Design Standards* (PWDS); *Salem Transportation System Plan* (Salem TSP); and *Stormwater Management Plan* (SMP).

FACTS

Streets

1. Woodside Drive SE

- a. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.
- b. Existing Conditions—This street has an approximate 25-foot turnpike improvement within a 59-foot-wide right-of-way abutting the subject property.
- c. The conditions of approval for land use case PAR19-11 require dedication of right-of-way along the frontage of Woodside Drive SE.

2. Mildred Lane SE

- a. Standard—This street is designated as a Minor Arterial street in the Salem TSP. The standard for this street classification is a 46-foot-wide improvement within a 72-foot-wide right-of-way.
- b. Existing Conditions—This street has an approximate 46-foot improvement within a 68-foot-wide right-of-way abutting the subject property.
- c. The conditions of approval for land use case PAR19-11 require dedication of right-of-way along the frontage of Mildred Lane SE.

Storm Drainage

1. Existing Conditions

- a. A 10-inch storm main is located in Mildred Lane SE.

Water

1. Existing Conditions

- a. The subject property is located in the S-2 water service level.
- b. A 10-inch water main is located in Woodside Drive SE.
- c. A 16-inch water main is located in Mildred Lane SE.

Sanitary Sewer

1. Existing Conditions

- a. The nearest available sewer main is an 8-inch sewer main in Woodside Drive SE approximately 275 feet south of the subject property. Condition 1 of land use case number PAR19-11 requires that an 8-inch sewer main be extended in Woodside Drive SE to the northern boundary of the subject property.

CRITERIA AND FINDINGS

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

Criteria: SRC 220.005(f)(3)(A) The application meets all applicable standards of the UDC (*Unified Development Code*)

Finding—With completion of the conditions above and approval of the adjustment for driveway spacing, the subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain; 802 – Public Improvements; 803 – Streets and Right-of-Way Improvements; 804 – Driveway Approaches; 805 – Vision Clearance; 809 – Wetlands and 810 - Landslides.

Waln Creek is located adjacent to the proposed development. This portion of the creek was not analyzed in conjunction with the Flood Insurance Study for Battle Creek basin. The applicant is advised that the City's recent *Stormwater Master Plan* update has generated preliminary base flood elevations for the subject property. The applicant shall coordinate with City staff to establish base flood elevations and ensure that the buildings are elevated sufficiently to limit flood damage pursuant to SRC 601.110(a)(2).

According to the Salem-Keizer Local Wetland Inventory (LWI), the subject property does not contain any wetland areas or hydric soils in the area of proposed development.

According to the City's adopted landslide hazard susceptibility maps and SRC Chapter 810 (Landslide Hazards), there are no mapped landslide hazard areas in the area of proposed development.

Criteria: SRC 220.005(f)(3)(B) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately

Finding—The existing conditions of Woodside Drive SE and Mildred Lane SE do not meet current standards for their classification of street per the Salem TSP. However, street improvements, including right-of-way dedication, along both frontages are required as conditions of approval under land use case number PAR19-11. Prior to the issuance of building permits, the applicant shall complete all conditions and record the partition plat for partition case number PAR19-11.

Street standards require that sidewalks shall be located parallel to and one foot from the adjacent right-of-way (SRC 803.035(I)(2)(A)); therefore, the new sidewalk constructed along Woodside Drive SE shall be constructed pursuant to this code and PWDS. The existing sidewalk along the frontage of Mildred Lane SE may remain in its current configuration pursuant to SRC 803.035(I)(2)(B).

Criteria: SRC 220.005(f)(3)(C) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians

Finding—The driveway access onto Mildred Lane SE provides for safe turning movements into and out of the property. See Class 2 Driveway Approach and Class 2 Zoning Adjustment findings below.

Criteria: SRC 220.005(f)(3)(D) The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development

Finding—The Public Works Department has reviewed the applicant's preliminary plan for this site. The water and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development. The nearest available sewer main is an 8-inch sewer main in Woodside Drive SE approximately 275 feet south of the subject property. Condition 1 of land use case number PAR19-11 requires that an 8-inch sewer main be extended in Woodside Drive SE to the northern boundary of the subject property. Once constructed, the proposed development will be adequately served by sewer.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible. At the time of development, the applicant shall design and construct a storm drainage system in compliance with SRC Chapter 71 and PWDS.

The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that the trash area shall be designed in compliance with PWDS.

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

- (1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

Finding—The proposed driveway is located less than 370 feet from adjacent driveways; therefore, a Class 2 adjustment is required for driveway spacing as described below. Otherwise, the proposed driveway meets the standards for SRC 804 and PWDS.

- (2) No site conditions prevent placing the driveway approach in the required location;**

Finding—There are no site conditions prohibiting the location of the proposed driveway.

- (3) The number of driveway approaches onto an arterial are minimized;**

Finding—One access is proposed to the arterial street.

- (4) The proposed driveway approach, where possible:**

- i. Is shared with an adjacent property; or**
- ii. Takes access from the lowest classification of street abutting the property;**

Finding—The proposed development abuts a Local street and a Minor Arterial street. The applicant proposes only one driveway to the Minor Arterial street with an adjustment for driveway spacing. A shared driveway approach is not possible because of existing topography. The subject property has limited frontage along the Local street. A driveway approach to the lower classification of street is not possible due to proximity to the intersection and queuing concerns. The Assistant City Traffic Engineer recommends approval of the single driveway approach to the Minor Arterial street as proposed on the applicant's site plan.

- (5) Proposed driveway approach meets vision clearance standards;**

Finding—The proposed driveway meets the PWDS vision clearance standards set forth in SRC Chapter 805.

- (6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;**

Finding—No evidence has been submitted to indicate that the proposed driveway will create traffic hazards or unsafe turning movements. Additionally, staff analysis of the proposed driveway indicates that it will not create a traffic hazard and will provide for safe turning movements for access to the subject property.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—Staff analysis of the proposed driveway and the evidence that has been submitted indicate that the location of the proposed driveway will not have any adverse impacts to the adjacent properties or streets.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The proposed driveway approach is located on a Minor Arterial street and minimizes the impact to adjacent streets and intersections by limiting access to a single driveway.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding—The proposed development is surrounded by residentially zoned property. The driveway balances the adverse impacts to residentially zoned property by limiting the development to a single driveway and will not have an adverse effect on the functionality of the adjacent streets.

CRITERIA AND FINDINGS—Class 2 Adjustments

Analysis of the proposed Class 2 adjustment based on relevant criteria in SRC 250.005(d)(2) is as follows:

Criteria—The purpose underlying the specific development standard proposed for adjustment is:

1. Clearly inapplicable to the proposed development; or
2. Equally or better met by the proposed development.

Finding—The applicant is requesting a Class 2 adjustment to allow for reduced spacing between driveways less than the standard of 370 feet. The development is proposing a single two-way driveway located approximately 200 feet from the intersection. The

subject property has limited frontage along the Local street, which does not have a spacing requirement. A driveway approach to the lower classification of street is not possible due to proximity to the intersection and queuing concerns. The Assistant City Traffic Engineer recommends approval of the single driveway approach to the Minor Arterial street as proposed on the applicant's site plan. The proposed driveway configuration meets the adjustment criteria by allowing for turning movements and traffic safety equal to what would be accomplished by meeting the development standard.

Prepared by: Jennifer Scott, Program Manager
cc: File



ATTACHMENT E

DAVID FRIDENMAKER, Manager
Facility Rental, Planning, Property Services
3630 State Street, Bldg. C ● Salem, Oregon 97301-5316
503-399-3335 ● FAX: 503-375-7847

Christy Perry, Superintendent

June 29, 2020

Brandon Pike, Planner
Planning Division, City of Salem
555 Liberty Street SE, Room 305
Salem OR 97301

RE: Land Use Activity Case No. DR-CU-SPR-ADJ-DAP20-02, 5611 Woodside Dr SE

The City of Salem issued a Request for Comments for a Land Use Case as referenced above. Please find below comments on the impact of the proposed land use change on the Salem-Keizer School District.

IDENTIFICATION OF SCHOOLS SERVING THE SUBJECT PROPERTY

The School District has established geographical school attendance areas for each school known as school boundaries. Students residing in any residence within that boundary are assigned to the school identified to serve that area. There are three school levels, elementary school serving kindergarten thru fifth grade, middle school serving sixth thru eighth grade, and high school serving ninth thru twelfth grade. . The schools identified to serve the subject property are:

School Name	School Type	Grades Served
Battle Creek	Elementary	K thru 5
Judson	Middle	6 thru 8
Sprague	High	9 thru 12

Table 1

SCHOOL CAPACITY & CURRENT ENROLLMENT

The School District has established school capacities which are the number of students that a particular school is designed to serve. Capacities can change based on class size. School capacities are established by taking into account core infrastructure (gymnasium, cafeteria, library, etc.) counting the number of classrooms and multiplying by the number of students that each classroom will serve. A more detailed explanation of school capacity can be found in the School District's adopted Facility Plan.

School Name	School Type	School Enrollment	School Design Capacity	Enroll./Capacity Ratio
Battle Creek	Elementary	594	601	99%
Judson	Middle	995	995	100%
Sprague	High	1,701	1,940	88%

Table 2

POTENTIAL ADDITIONAL STUDENTS IN BOUNDARY AREA RESULTING FROM APPROVAL OF LAND USE CASE

The School District anticipates the number of students that may reside at the proposed development based on the housing type, single family (SF), duplex/triplex/four-plex (DU), multi-family (MF) and mobile home park (MHP). The School District commissioned a study by the Mid-Willamette Valley Council of Governments in 2014 to determine an estimate of students per residence, for the Salem-Keizer area, in each of the four housing types. Since the results are averages, the actual number of students in any given housing type will vary. The table below represents the resulting estimates for the subject property:

School Type	Qty. of New Residences	Housing Type	Average Qty. of Students per Residence	Total New Students
Elementary	18	MF	0.201	4
Middle	18	MF	0.077	1
High	18	MF	0.084	2

Table 3

POTENTIAL EFFECT OF THIS DEVELOPMENT ON SCHOOL ENROLLMENT

To determine the impact of the new residential development on school enrollment, the School District compares the school capacity to the current enrollment plus estimates of potential additional students resulting from land use cases over the previous two calendar years. A ratio of the existing and new students is then compared with the school design capacity and expressed as a percentage to show how much of the school capacity may be used.

School Name	School Type	School Enrollment	New Students During Past 2 yrs	New Student from this Case	Total New Students	School Design Cap.	Enroll./Cap. Ratio
Battle Creek	Elem.	594	6	4	10	601	101%
Judson	Mid.	995	38	1	39	995	104%
Sprague	High	1,701	47	2	49	1,940	90%

Table 4

ESTIMATE OF THE EFFECT ON INFRASTRUCTURE – IDENTIFICATION OF WALK ZONES AND SCHOOL TRANSPORTATION SERVICE

Civic infrastructure needed to provide connectivity between the new residential development and the schools serving the new development will generally require roads, sidewalks and bicycle lanes. When developing within one mile of school(s), adequate pathways to the school should be provided that would have raised sidewalks. If there are a large number of students walking, the sidewalks should be wider to accommodate the number of students that would be traveling the

path at the same time. Bike lanes should be included, crosswalks with flashing lights and signs where appropriate, traffic signals to allow for safe crossings at busy intersections, and any easements that would allow students to travel through neighborhoods. If the development is farther than one mile away from any school, provide bus pullouts and a covered shelter (like those provided by the transit district). Locate in collaboration with the District at a reasonable distance away from an intersection for buses if the distance is greater than ½ mile from the main road. If the distance is less than a ½ mile then raised sidewalks should be provided with stop signs where students would cross intersections within the development as access to the bus stop on the main road. Following is an identification, for the new development location, that the development is either located in a school walk zone or is eligible for school transportation services.

School Name	School Type	Walk Zone or Eligible for School Transportation
Battle Creek	Elementary	Eligible for School Transportation
Judson	Middle	Eligible for School Transportation
Sprague	High	Eligible for School Transportation

Table 5

ESTIMATE OF NEW SCHOOL CONSTRUCTION NEEDED TO SERVE DEVELOPMENT

The School District estimates the cost of constructing new school facilities to serve our community. The costs of new school construction is estimated using the Rider Levett Bucknall (RLB) North America Quarterly Construction Cost Report and building area per student from Cornerstone Management Group, Inc. estimates. The costs to construct school facilities to serve the proposed development are in the following table.

School Type	Number of Students	Estimate of Facility Cost Per Student*	Total Cost of Facilities for Proposed Development*
Elementary	4	\$54,925	\$219,700
Middle	1	\$64,045	\$64,045
High	2	\$73,164	\$146,328
TOTAL			\$430,073

Table 6

*Cornerstone Management Group, Inc. estimates based on RLB cost index average, 2019 Fourth Quarter.

Sincerely,

David Fridenmaker

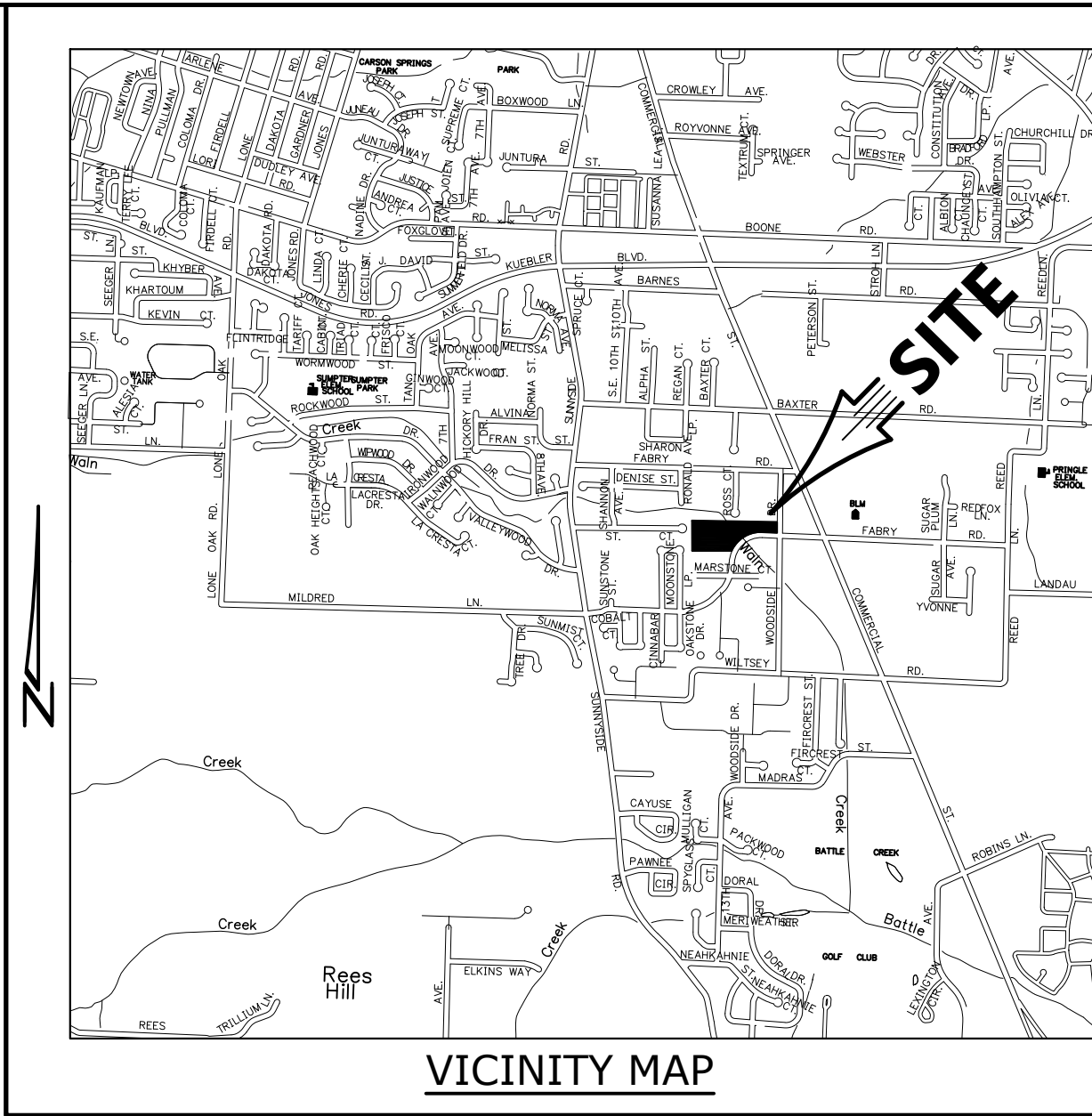
David Fridenmaker, Manager
Planning and Property Services

c: Mike Wolfe, Chief Operations Officer, David Hughes, Director – Custodial, Property and Auxiliary Services, Michael Shields, Director of Transportation

Owner / Developer:
EMPIRE BUILDERS OF OREGON, L.L.C.
8527 SAGHALIE DR. S.
SALEM, OREGON 97306

Owner / Developer:
JCT CONSTRUCTION GROUP, L.L.C.
201 FERRY ST. S.E., SUITE 400
SALEM, OREGON 97301

TONEY ESTATES
SEC. 14, T. 8 S., R. 3 W., W.M.
CITY OF SALEM
MARION COUNTY, OREGON
TAX MAP: 08W14CB
LOTS: 2301, 2400, 2500
5.88 ACRES



MULTI//TECH
ENGINEERING SERVICES, INC.
11555 13th ST. S.E. SALEM, OR. 97302
PH: (503) 363 - 9227 FAX (503) 364-1260
www.mtengineering.net office@mtengineering.net

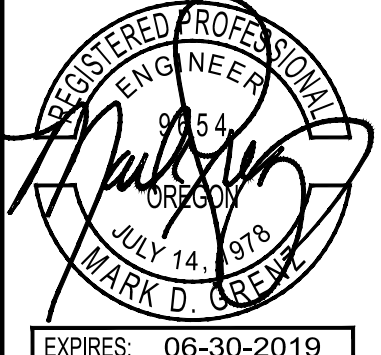
**PROPOSED
PARTITION PLAN**

TONEY ESTATES

MULTITECH ENGINEERING EXEMPT FROM
LIABILITY IF NOT STAMPED APPROVED
**NOT FOR
CONSTRUCTION
UNLESS STAMPED
APPROVED HERE**

NO CHANGES, MODIFICATIONS
OR REPRODUCTIONS TO BE
MADE TO THESE DRAWINGS
WITHOUT WRITTEN
AUTHORIZATION FROM THE
DESIGN ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER ANY
GRAPHICAL REPRESENTATION.

6606P-Part 1
Design: M.D.G.
Drawn: P.H.S.
Checked: M.D.G.
Date: FEB. 2019
Scale: AS SHOWN
As-Built: ----



EXPIRES: 06-30-2019
JOB # 6606

