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DECISION OF THE PLANNING ADMINISTRATOR

CLASS 2 SITE PLAN REVIEW

APPLICATION NO.: 25-111099-PLN

NOTICE OF DECISION DATE: August 19, 2025

REQUEST: A Class 2 Site Plan Review for a change of use and accessibility site improvements for an existing 3,558 square foot building and off-street parking area. The property is approximately 0.20 acres in size, zoned CO (Commercial Office), and located at 1097 Liberty Street SE (Marion County Assessor's Map and Tax Lot Number: 073W27CA / 6700).

APPLICANT: Blake Bural, AC+Co. on behalf of Eden Rose Brown

LOCATION: 1097 Liberty Street SE

FINDINGS: The findings are in the attached Decision dated August 19, 2025.

DECISION: The **Planning Administrator APPROVED** the application based upon the submitted materials and the findings as presented in the decision.

The rights granted by the attached decision, which are effective as of the date of this decision, must be exercised by August 19, 2029, or this approval shall be null and void.

Case Manager: Quincy Miller, AICP, Planner I, gmiller@cityofsalem.net, (503) 584-4676

This decision is final; there is no local appeal process. Any person with standing may appeal this decision by filing a "Notice of Intent to Appeal" with the Land Use Board of Appeals, 775 Summer St NE, Suite 330, Salem OR 97301, **not later than 21 days** after **August 19, 2025**. Anyone with questions regarding filing an appeal with the Oregon Land Use Board of Appeals should contact an attorney.

<http://www.cityofsalem.net/planning>

BEFORE THE PLANNING ADMINISTRATOR OF THE CITY OF SALEM

DECISION

IN THE MATTER OF APPROVAL OF) FINDINGS & ORDER
CLASS 2 SITE PLAN REVIEW)
25-111099-PLN)
1097 LIBERTY STREET SE) AUGUST 19, 2025

In the matter of the application for a Class 2 Site Plan Review, the Planning Administrator, having received and reviewed the evidence and application materials, makes the following findings and adopts the following order as set forth herein.

REQUEST

A Class 2 Site Plan Review for a change of use and accessibility site improvements for an existing 3,558 square foot building and off-street parking area. The property is approximately 0.20 acres in size, zoned CO (Commercial Office), and located at 1097 Liberty Street SE (Marion County Assessor's Map and Tax Lot Number: 073W27CA / 6700).

A vicinity map illustrating the location of the property is attached hereto and made a part of this staff report (**Attachment A**).

PROCEDURAL FINDINGS

1. Background

On May 30, 2025, an application for a Class 2 Site Plan Review was submitted for property located at 1097 Liberty Street SE. After additional requested information was provided by the applicant, the application was deemed complete for processing on August 5, 2025. The 120-day state mandated decision deadline for this application is December 3, 2025.

The applicant's proposed site plan is included as **Attachment B**, and the applicant's written statement addressing the approval criteria can be found in the record, accessible online as indicated below.

SUBSTANTIVE FINDINGS

2. Summary of Record

The following items are submitted to the record and are available: 1) all materials and testimony submitted by the applicant, including any applicable professional studies such as traffic impact analysis, geologic assessments, stormwater reports, and; 2) materials, testimony, and comments from public agencies, City Departments, neighborhood associations, and the public. All application materials are available on the City's online Permit Application Center at <https://permits.cityofsalem.net>. You may use the search function without registering and enter the permit number listed here: 25 111099.

3. City Department Comments

Development Services Division – Reviewed the proposal and indicated that while the existing driveway approach is considered existing nonconforming due to inadequate width, as the

proposed development will not be further narrowing the driveway approach, there are no additional concerns.

Building and Safety Division – Reviewed the proposal and indicated no concerns.

Salem Fire Department – Reviewed the proposal and indicated no concerns.

DECISION CRITERIA FINDINGS

4. Analysis of Class 2 Site Plan Review Approval Criteria

Pursuant to SRC 220.005(b)(2), Class 2 Site Plan Review is required for any development that requires a building permit, other than development subject to Class 1 Site Plan Review, and that does not involve a land use decision or limited land use decision, as those terms are defined in ORS 197.015. Salem Revised Code (SRC) 220.005(f)(2) provides that an application for a Class 2 Site Plan Review shall be granted if the following criteria are met.

SRC 220.005(f)(2)(A): Only clear and objective standards which do not require the exercise of discretion or legal judgment are applicable to the application.

Finding: Only clear and objective standards apply to the proposed development. Complete findings addressing the proposal's conformance with these standards are included within the findings addressing approval criterion SRC 220.005(f)(2)(B) below. This approval criterion is met.

SRC 220.005(f)(2)(B): The application meets all the applicable standards of the UDC.

Finding: The proposal is for a change of use from an *Outpatient Medical Services and Laboratories* use to an *Office* use, as well as accessibility improvements to the existing building, off-street parking area, and development site. The subject property is zoned CO (Commercial Office) and is therefore subject to the use and development standards of the CO zone, SRC Chapter 521. The proposed development meets the applicable standards of the CO zone and the UDC as follows:

❖ ZONING AND DEVELOPMENT STANDARDS

SRC Chapter 521 – CO (Commercial Office) Zone

SRC 521.005 – Uses.

The permitted (P), special (S), conditional (C), and prohibited (N) uses in the CO zone are set forth in Table 521-1.

Finding: The proposal includes a change of use from an *Outpatient Medical Services and Laboratories* use to an *Office* use, both of which are permitted uses in the CO zone. This standard is met.

SRC 521.010(a) – Lot Standards.

Lots within the CO zone shall conform to the standards set forth in Table 521-2.

Finding: For the proposed *Office* use, a minimum lot area of 6,000 square feet and minimum

16 feet of street frontage are required, though there are no minimum lot width or depth requirements for the proposed use. The existing property is approximately 8,550 square feet in area and has more than 16 feet of street frontage; furthermore, as no changes are proposed to the existing lot dimensions, this standard is met.

SRC 521.010(b) – Setbacks.

Setbacks within the CO zone shall be provided as set forth in Table 521-3 and Table 521-4.

▪ *Abutting Street*

South: The subject property is adjacent to Owens Street SE to the south, a Minor Arterial Street in the Salem Transportation System Plan (TSP). There is a minimum 12-foot setback for all buildings, accessory structures, and vehicle use areas for non-residential uses.

Finding: While the existing off-street parking area is nonconforming as it is setback less than 12 feet from the property line abutting Owens Street SE, the proposed development is restriping the existing space for an ADA space and removing some landscaping to create an accessible pathway to the existing sidewalk. Therefore, as no pavement is being removed, the configuration of the off-street parking area is not changing, and the setbacks for the existing building are not being altered, this standard is met.

East: The subject property is adjacent to Liberty Street SE to the east, a Major Arterial Street in the Salem Transportation System Plan (TSP). There is a minimum 12-foot setback for all buildings, accessory structures, and vehicle use areas for non-residential uses.

Finding: The proposed development does not alter the setbacks for the existing buildings, and the ADA restriping and pathway improvement do not alter the configuration or setbacks of the vehicle use area. Therefore, this standard is met.

▪ *Interior Property Lines*

North/West: The subject property is adjacent to the CO zone to the north and west. For non-residential uses, all buildings and accessory structures do not have a minimum setback per the Zone-to-Zone setback table, though vehicle use areas require a minimum five-foot setback with Type A landscaping.

Finding: As the proposed development does not alter the setbacks for the existing building or vehicle use area, this standard is met.

SRC 521.010(c) – Lot Coverage & Height.

Buildings and accessory structures within the CO zone shall conform to the lot coverage and height standards set forth in Table 521-5.

Finding: For all uses, a maximum lot coverage of 60 percent applies, as well as a maximum height of 70 feet for buildings and accessory structures for non-residential or short-term commercial lodging uses. As the existing building's dimensions are not being modified as part of the proposed development, this standard is met.

SRC 521.010(d) – Landscaping.

- (1) *Setbacks*. Required setbacks shall be landscaped. Landscaping shall conform to the standards set forth in SRC Chapter 807.
- (2) *Vehicle Use Areas*. Vehicle use areas shall be landscaped as provided under SRC Chapter 806 and SRC Chapter 807.
- (3) *Development Site*. A minimum of 15 percent of the development site shall be landscaped. Landscaping shall meet the Type A standard set forth in SRC Chapter 807. Other required landscaping under the UDC, such as landscaping required for setbacks or vehicular use areas, may count toward meeting this requirement.

Finding: For the 8,550 square-foot development site, a minimum 1,283 square feet of development site landscaping is required. Although some landscaping is proposed to be removed as part of the ADA parking space pathway improvements, approximately 1,880 square feet of development site landscaping will remain, meeting the standard.

SRC Chapter 800 – General Development Standards

▪ *Solid Waste Service Areas*

SRC 800.055(a) – Applicability.

Solid waste service area design standards shall apply to all new solid waste, recycling, and compostable services areas, where use of a solid waste, recycling, and compostable receptacle of 1 cubic yard or larger is proposed.

Finding: No modifications to or addition of a solid waste service area is proposed for the development; therefore, this section is not applicable.

▪ *Pedestrian Access*

SRC 800.065 – Pedestrian Access.

Except where pedestrian access standards are provided elsewhere under the UDC, all developments, other than single family, two family, three family, four family, and multiple family developments, shall include an on-site pedestrian circulation system developed in conformance with the standards in this section. For purposes of this section development means the construction of, or addition to, a building or accessory structure or the construction of, or alteration or addition to, an off-street parking or vehicle use area. Development does not include construction of, or additions to, buildings or accessory structures that are less than 200 square feet in floor area.

When a development site is comprised of lots under separate ownership, the pedestrian access standards set forth in this section shall apply only to the lot, or lots, proposed for development, together with any additional contiguous lots within the development site that are under the same ownership as those proposed for development.

Finding: As the modifications to the off-street parking area are limited to restriping for an accessible parking space and accessible path, the standards of this section are not applicable.

SRC Chapter 806 – Off-Street Parking, Loading, and Driveways

SRC 806.015 – Amount Off-Street Parking.

- (a) *Maximum Off-Street Parking*. Except as otherwise provided in this section, and unless otherwise provided under the UDC, off-street parking shall not exceed the amounts set

forth in Table 806-1. For the purposes of calculating the maximum amount of off-street parking allowed, driveways shall not be considered off-street parking spaces.

Finding: The proposal includes the removal of one off-street parking space to restripe and create a new ADA parking space. Per Table 806-1 in SRC 806.015(a), an *Office* use is limited to a maximum of one off-street parking space per 250 square feet of gross floor area; therefore, the proposed development is limited to 14 off-street parking spaces ($3,558 / 250 = 14.2$). As the proposed development is reducing the number of off-street parking spaces from seven to six, and the total is still below the maximum allowed, this standard is met.

(b) *Compact parking.* Up to 75 percent of the off-street parking spaces provided on a development site may be compact parking spaces.

Finding: No spaces are proposed to be compact; therefore, this standard is not applicable.

(c) *Carpool and vanpool parking.* New developments with 60 or more off-street parking spaces, and falling within the public services and industrial use classifications, and the business and professional services use category, shall designate a minimum of five percent of their total off-street parking spaces for carpool or vanpool parking.

Finding: The development has less than 60 off-street parking spaces; therefore, this standard is not applicable.

(d) *Required electric vehicle charging spaces.* For any newly constructed building with five or more dwelling units on the same lot, including buildings with a mix of residential and nonresidential uses, a minimum of 40 percent of the off-street parking spaces provided on the site for the building shall be designated as spaces to serve electrical vehicle charging. In order to comply with this subsection, such spaces shall include provisions for electrical service capacity, as defined in ORS 455.417.

Finding: The development does not include residential uses; therefore, this standard is not applicable.

▪ ***Off-Street Parking and Vehicle Use Area Development Standards***

SRC 806.035 – Off-Street Parking and Vehicle Use Area Development Standards.

(a) *General Applicability.* The off-street parking and vehicle use area development standards set forth in this section apply to:

- (1) The development of new off-street parking and vehicle use areas;
- (2) The expansion of existing off-street parking and vehicle use areas, where additional paved surface is added;
- (3) The alteration of existing off-street parking and vehicle use areas, where the existing paved surface is replaced with a new paved surface; and
- (4) The paving of an unpaved area.

Finding: The modifications to the off-street parking area are limited to the restriping of the existing parking spaces to accommodate an ADA space, with no pavement proposed to be removed. Therefore, this section is not applicable.

▪ ***Bicycle Parking***

SRC 806.045 – General Applicability.

- (a) Bicycle parking shall be provided as required under this chapter for each proposed new use or activity; any change of use or activity; or any intensification, expansion, or enlargement of a use or activity.
- (b) *Applicability to change of use of existing building in Central Business District (CB), West Salem Central Business District (WSCB), Mixed Use-I (MU-I), Mixed Use-II (MU-II), Mixed Use-III (MU-III), Mixed Use-Riverfront (MU-R), and Edgewater/Second Street Mixed-Use Corridor (ESMU) zone.* Notwithstanding any other provision of this chapter, the bicycle parking requirements for a change of use of an existing building within the CB, WSCB, MU-I, MU-II, MU-III, MU-R, and ESMU zones where at least 75 percent of the width of the lot at the front setback line is occupied by existing buildings shall be met if there are a minimum of eight bicycle parking spaces located within the public right-of-way of the block face adjacent to the primary entrance of the building. If the minimum number of required bicycle parking spaces are not present within the block face, the applicant shall be required to obtain a permit to have the required number of spaces installed. For purposes of this subsection, "block face" means the area within the public street right-of-way located along one side of a block, from intersecting street to intersecting street.
- (c) *Applicability to nonconforming bicycle parking area.* When bicycle parking is required to be added to an existing bicycle parking area that has a nonconforming number of spaces, the number of spaces required under this chapter for any new use or activity, any change of use or activity, or any intensification, expansion, or enlargement of a use or activity shall be provided, in addition to the number of spaces required to remedy the existing deficiency.

SRC 806.050 – Proximity of Bicycle Parking to Use or Activity Served.

Bicycle parking shall be located on the same development site as the use or activity it serves.

SRC 806.055 – Amount of Bicycle Parking.

Unless otherwise provided under the UDC, bicycle parking shall be provided in amounts not less than those set forth in Table 806-9.

Finding: Per Table 806-9 in SRC 806.055(a), an *Office* use requires the greater of four bicycle parking spaces or one space per 10,000 square feet of gross floor area. Therefore, for the 3,558 square-foot building, a minimum of four bicycle parking spaces are required. The applicant's plans indicate that two bicycle racks (four bicycle parking spaces) will be installed on the development site, meeting the standard.

SRC 806.060 - Bicycle Parking Development Standards.

Bicycle parking areas shall be developed and maintained as set forth in this section.

(a) *Location.*

- (1) *Short-term bicycle parking.* Short-term bicycle parking shall be located outside a building within a convenient distance of, and clearly visible from, the primary building entrance. In no event shall bicycle parking be located more than 50 feet from the primary building entrance, as measured along a direct pedestrian access route

Finding: The site plan indicates the proposed bicycle parking is within 50 feet of a primary entrance; therefore, this standard is met.

- (b) *Access.* Bicycle parking areas shall have direct and accessible access to the public right-of-way and the primary building entrance that is free of obstructions and any barriers, such as

curbs or stairs, which would require users to lift their bikes in order to access the bicycle parking area.

Finding: As shown on the site plan, each bicycle parking area has direct access to a primary building entrance through the pedestrian pathways, free of obstruction or barriers, in conformance with the requirements of SRC 806.060(b).

(c) *Dimensions.* All bicycle parking areas shall meet the following dimension requirements:

- (1) *Bicycle parking spaces.* Bicycle parking spaces shall conform to the minimum dimensions set forth in Table 806-10.
- (2) *Access aisles.* Bicycle parking spaces shall be served by a minimum four-foot-wide access aisle. Access aisles serving bicycle parking spaces may be located within the public right-of-way.

Finding: Bicycle parking spaces shall be a minimum of six feet in length and one and half feet in width when the proposed bicycle racks are side-by-side. The proposed bicycle parking spaces are located on a paved walkway, with adequate dimensions to provide pedestrian passage for all four proposed spaces; the length of each proposed stall is at least six feet, while the width is at least two feet wide. The applicant is also providing a minimum five-foot-wide access aisle; therefore, the standard is met.

(d) *Surfacing.* Where bicycle parking is located outside a building, the bicycle parking area shall consist of a hard surface material.

Finding: The proposed bicycle parking spaces are placed on a hard surface material; therefore, the standard is met.

(e) *Bicycle Racks.* Where bicycle parking is provided in racks, the racks may be floor, wall, or ceiling racks. Bicycle racks shall meet the following standards:

- (1) Racks must support the bicycle frame in a stable position, in two or more places without damage to wheels, frame, or components.
- (2) Racks must allow the bicycle frame and at least one wheel to be locked to the rack with a high security, U-shaped shackle lock;
- (3) Racks shall be of a material that resists cutting, rusting, and bending or deformation; and
- (4) Racks shall be securely anchored.
- (5) Examples of types of bicycle racks that do, and do not, meet these standards are shown in Figure 806-11.

Finding: The site plan indicates inverted style bike racks will be provided. The proposed bike racks conform to the material requirements of SRC 806.060(e).

▪ ***Off-Street Loading Areas***

SRC 806.065 – General Applicability.

(a) Off-street loading areas shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a greater number of required off-street loading spaces than the previous use or activity; or any intensification, expansion, or enlargement of a use or activity.

(b) *Applicability to nonconforming off-street loading area.* When off-street loading is required to be added to an existing off-street loading area that has a nonconforming number of spaces, the number of spaces required under this chapter for any new use or activity, any change of use or activity, or any intensification, expansion, or enlargement of a use or activity shall be provided, in addition to the number of spaces required to remedy the existing deficiency.

SRC 806.070 – Proximity of Off-Street Loading Areas to Use or Activity Served.

Off-street loading shall be located on the same development site as the use or activity it serves.

SRC 806.075 – Amount of Off-Street Loading.

Unless otherwise provided under the UDC, off-street loading shall be provided in amounts not less than those set forth in Table 806-11.

Finding: As the building is less than 5,000 square feet in area, no loading space is required. Therefore, this standard is met.

5. Conclusion

Based on the conformance with the preceding requirements the Planning Administrator certifies that the proposed Class 2 Site Plan Review is in conformance with the UDC and the approval criteria provided in SRC 220.005(f)(2), provided compliance occurs with any applicable items noted above.

Please Note: Findings included in this decision by the direction of the Salem Fire Department are based on non-discretionary standards. Fire Code related findings are intended to inform the applicant of the clear and objective Fire Prevention Code standards of SRC Chapter 58 that will apply to this development proposal on application for building permit(s). Additional or different Fire Prevention Code standards may apply based on the actual building permit application submitted.

Building permits are required for the proposed development.

Next Steps:

Please submit building permits or other development permits for this project with the Building and Safety Division. Please submit a copy of this decision or this application file number with your building permit application for the work proposed.

IT IS HEREBY ORDERED

The proposed Class 2 Site Plan Review is consistent with the provisions of SRC Chapter 220 and is hereby **APPROVED** subject to the applicable standards of the Salem Revised Code and the findings contained herein.

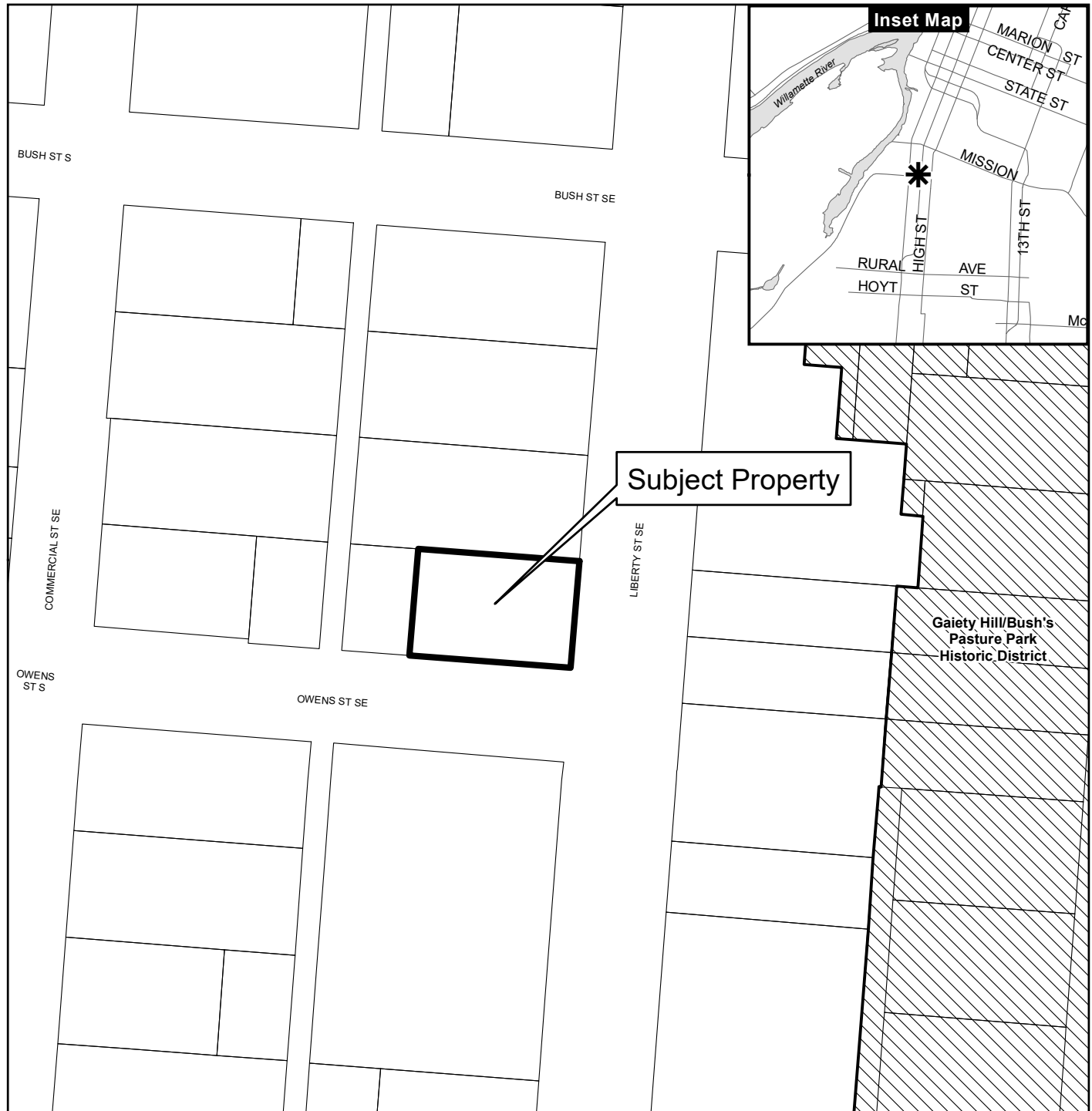


Quincy Miller, AICP, Planner I
On behalf of Lisa Anderson-Ogilvie, AICP
Planning Administrator







Attachments: A. Vicinity Map
 B. Proposed Development Plans

Vicinity Map

1097 Liberty Street SE



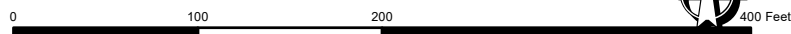
Legend

-  Taxlots
-  Urban Growth Boundary
-  City Limits
-  Outside Salem City Limits
-  Historic District
-  Schools

-  Parks

CITY OF Salem
AT YOUR SERVICE
Community Planning and Development

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GENERAL NOTES:

- GENERAL NOTES APPLY TO ALL DRAWINGS.
- DIMENSIONS ARE TO EXISTING FACE OF STUDS, FACE OF MASONRY AND FACE OF CONCRETE, UNLESS OTHERWISE NOTED. ('CLEAR' DIMENSION IS TO FACE OF FINISHED SURFACE).
- DRAWINGS ARE DIAGRAMMATIC ONLY AND SHOULD NOT BE SCALED. NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES OR QUESTIONABLE DIMENSIONS FOR CLARIFICATION PRIOR TO PROCEEDING WITH AREA OF REQUIRED WORK.
- IN CASE OF ANY CONFLICTS IN THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, THE CONTRACTOR IS REQUIRED TO INCLUDE THE BETTER QUALITY AND LARGER QUANTITY OF THE WORK.
- CONTRACTOR AND BIDDERS SHALL USE COMPLETE SETS OF CONTRACT DOCUMENTS; NEITHER THE OWNER NOR ARCHITECT ASSUMES RESPONSIBILITY FOR ERRORS OR MISINTERPRETATIONS RESULTING FROM THE USE OF INCOMPLETE SETS OF CONTRACT DOCUMENTS.

REFERENCE NOTES:

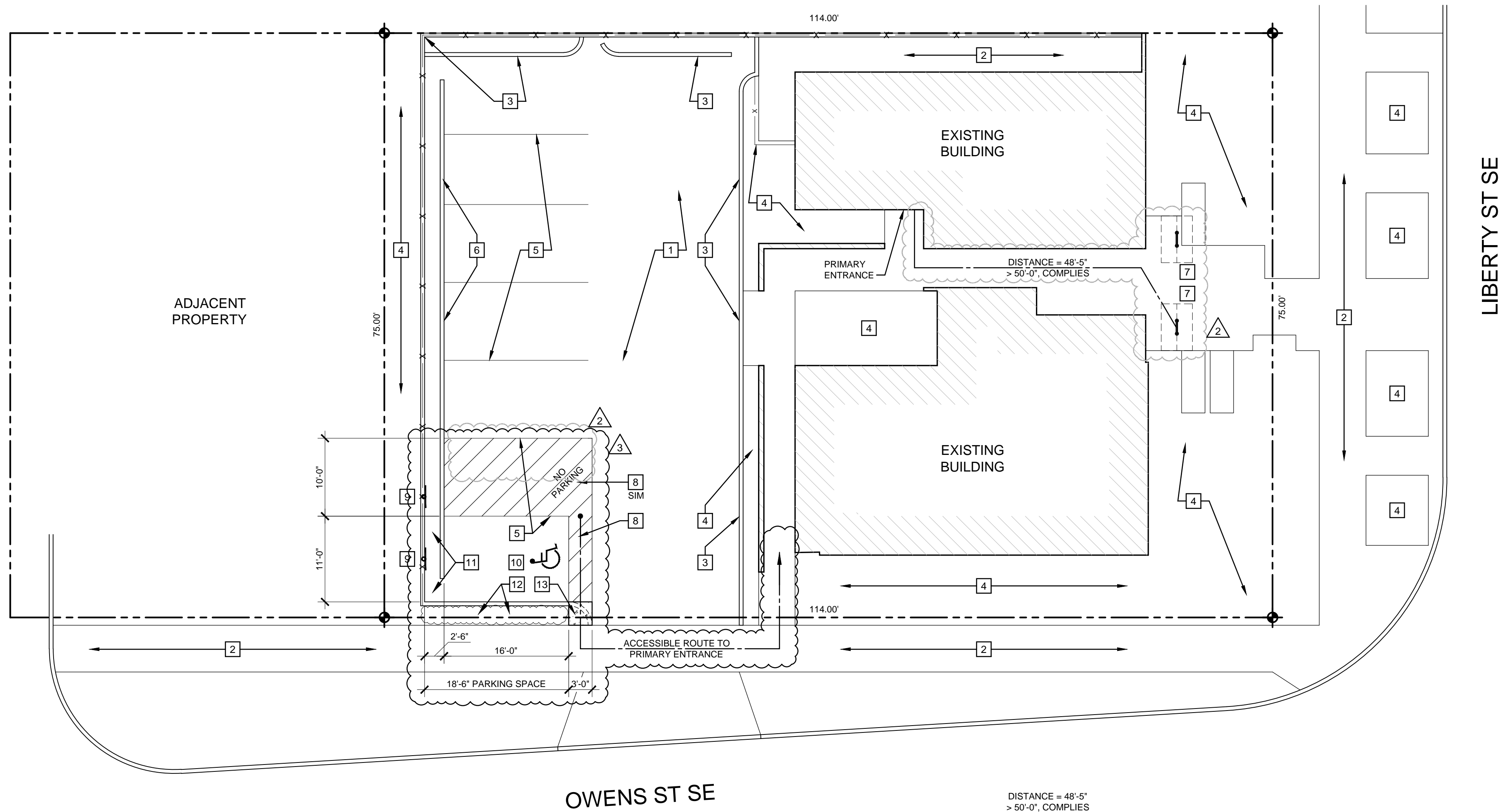
- EXISTING ASPHALT PAVING TO REMAIN
- EXISTING CONCRETE PAVING TO REMAIN
- EXISTING CURB TO REMAIN
- EXISTING LANDSCAPE AREA TO REMAIN
- EXISTING PAINTED PARKING STRIPE TO REMAIN
- EXISTING WHEEL STOP TO REMAIN
- NEW BIKE PARKING SPACE, REFER TO $\frac{2}{A1.0}$ $\frac{3}{A1.0}$
- NEW PEDESTRIAN ACCESS ROUTE, 4" PAINTED SAFETY STRIPES AT 24" O.C. DIAGONALLY, AT SIM CONDITION, INCLUDE 12" HIGH PAINTED LETTING 'NO PARKING'
- NEW ACCESSIBLE PARKING SIGN, INSTALL ON FENCE, REFER TO $\frac{4}{A1.0}$ $\frac{5}{A1.0}$
- NEW ACCESSIBLE PARKING SYMBOL
- EXISTING 30" CAR OVERHANG SPACE TO REMAIN
- EXISTING SHRUB TO REMAIN, SELECTIVELY DEMO AS REQUIRED FOR NEW ACCESSIBLE ROUTE
- NEW CONCRETE PAD FROM EXISTING PARKING LOT TO EXISTING SIDEWALK, SELECTIVELY DEMO EXISTING CURB AS REQUIRED FOR NEW WORK, PROVIDE FLUSH TRANSITION AT BOTH EDGES

SITE SUMMARY:		NOTES:
ZONING DESIGNATION	= CO (COMMERCIAL OFFICE)	
BUILDING USE	= OFFICE	PERMITTED USE
TOTAL SITE AREA	= 8,550 SF (0.19 ACRES)	
GROSS FLOOR AREA	= 2,260 SF	
GROSS PARKING AREA	= 2,995 SF	
TOTAL LANDSCAPE AREA	= 2,060 SF	
BUILDING HEIGHT	= ~20'-0"	
PARKING		
FULL	= 5 PS (EXISTING)	
COMPACT	= 0	
HANDICAP	= 1 PS (NEW)	1 PS REQUIRED, COMPLIES
BICYCLE	= 4 PS (NEW)	4 PS REQUIRED, COMPLIES
LOADING ZONE	= N/A	<5,000 SF

ARCHITECT'S STATEMENT:
FOR ARCHITECTURAL BARRIERS, WITH THIS PROJECT THE SITE WILL NOW HAVE AN ADA PARKING STALL, A PATHWAY, AND AN INTERIOR ADA RESTROOM OF WHICH THE PROPERTY HAS NEVER HAD. WE FEEL THE IMPROVEMENTS PROVIDED, ALTHOUGH MIGHT NOT STRICTLY MATCH THE CODE TODAY, THE OWNER AND TEAM ARE ATTEMPTING TO MEET THE INTENT WHICH IS TO PROVIDE LESS BARRIERS TO THIS PROPERTY.

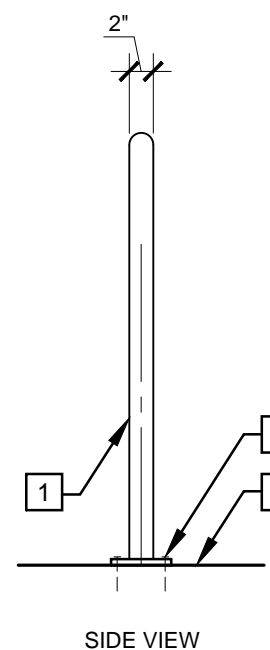
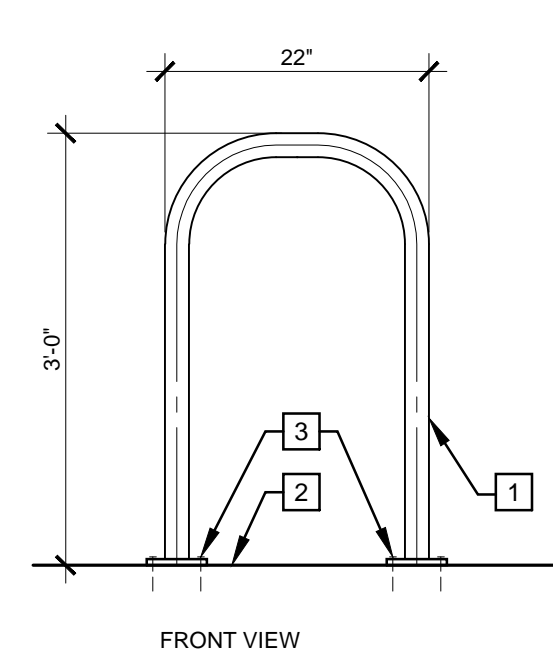
SITE CONSTRAINTS FOR ADA ACCESS TO VARY FROM STANDARD CODE REQUIREMENTS ARE AS FOLLOWS:

- DRIVEWAY WIDTH AND VEHICLE ACCESS WAY IS ONLY 18'-10" TODAY WHICH IS NARROWER THAN TODAY'S STANDARDS. PROPOSED ACCESS THROUGH THE DRIVEWAY WOULD FURTHER REDUCE THE WIDTH.
- RELOCATING THE ADA STALL TO A CENTRALIZED LOCATION AND RAMPING UP TO THE SIDEWALK WILL IMPEDE THE EXISTING PATHWAY. IF RAMP WAS SLOPED TO MAINTAIN EXISTING PATHS THE RAMP WOULD NEED TO BE 16% SLOPE.
- TO ADD A RAMP CENTRALIZED WITHIN THE PARKING LOT WOULD AGAIN FURTHER IMPACT THE MANEUVERABILITY OF VEHICLES.



1 SITE PLAN

SCALE: 1"=10'-0"



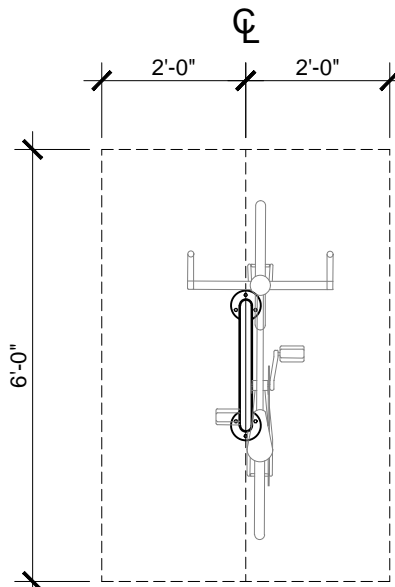
NOTES:

- 2" DIA. SCHEDULE 40 PIPE WITH EMBEDDED ANCHOR MOUNT, POWDER COAT FINISH
- NEW CONCRETE, REFER TO CIVIL DRAWINGS
- NEW LAG BOLTS AND PLATE, ANCHOR PER MANUFACTURER

2 BIKE RACK ANCHORAGE DETAIL

SCALE: 3/4" = 1'-0"

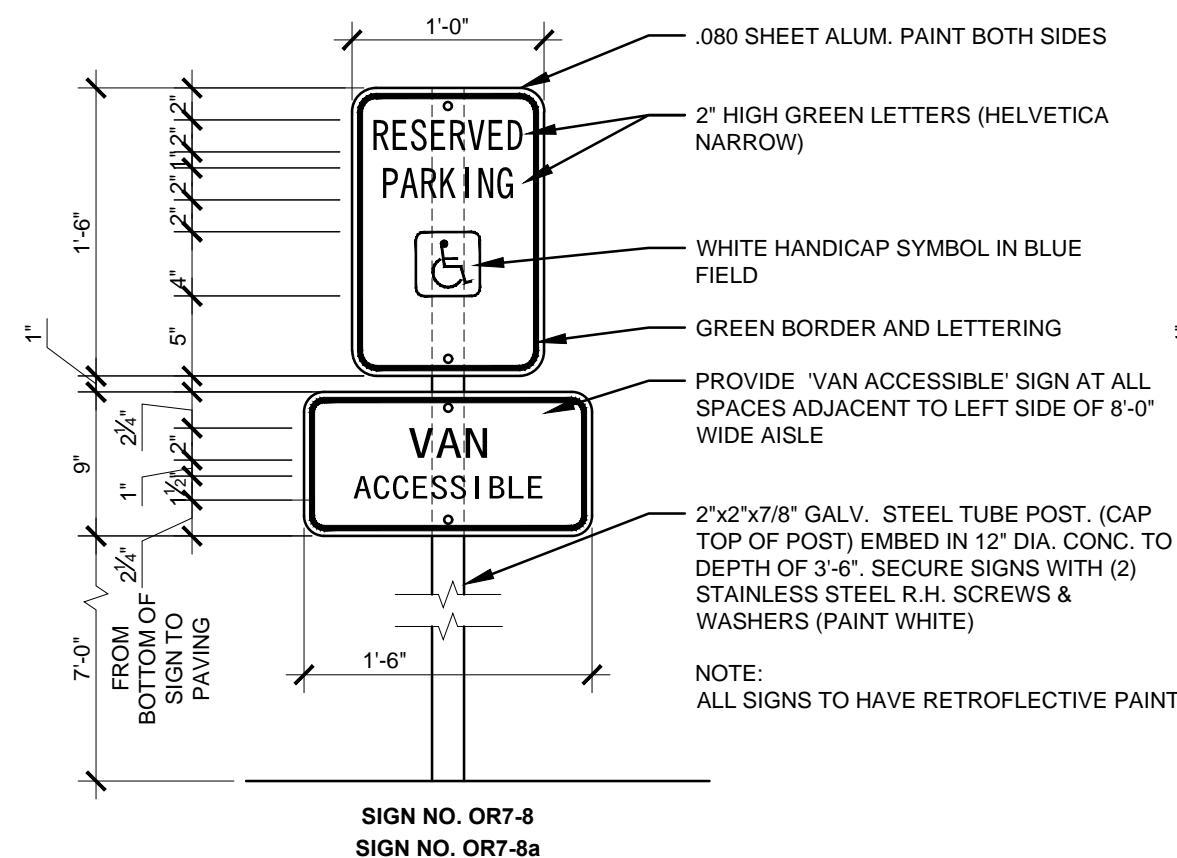
NOTE: ONLY ONE BIKE SHOWN FOR CLARITY



3 BIKE RACK PLAN DETAIL

SCALE: 3/4" = 1'-0"

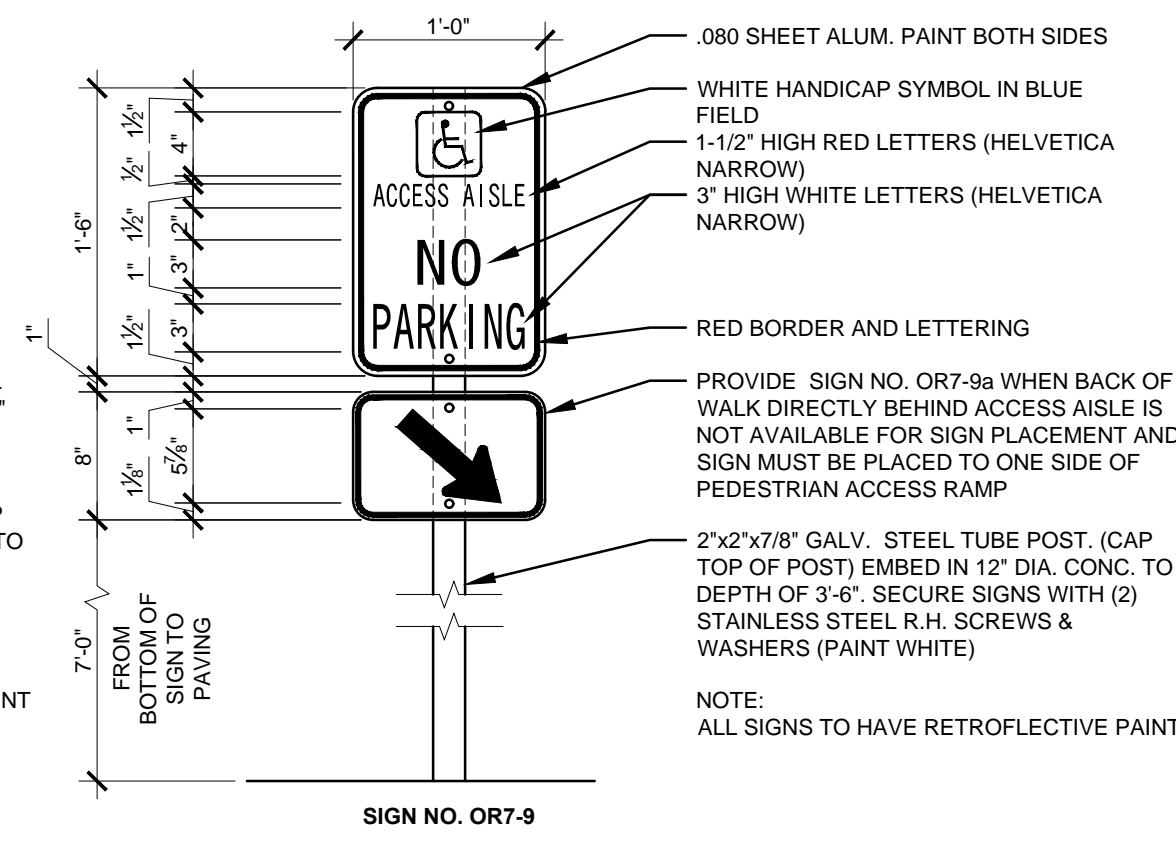
25005-A1.0-02



4 ACCESSIBLE PARKING SPACE SIGN

SCALE: 1" = 1'-0"

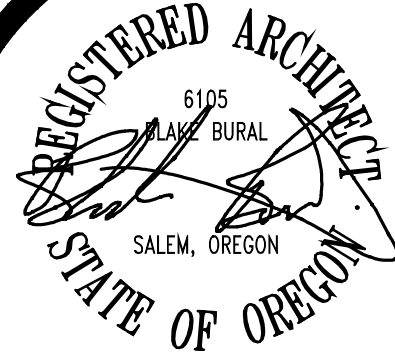
25005-A1.0-04



5 ACCESSIBLE LOADING SPACE SIGN

SCALE: 1" = 1'-0"

25005-A1.0-05



In the event conflicts are discovered between the original signed and sealed documents prepared by the Architects and/or their Consultants, and any copy of the documents transmitted by mail, fax, electronically or otherwise, the original signed and sealed documents shall govern.

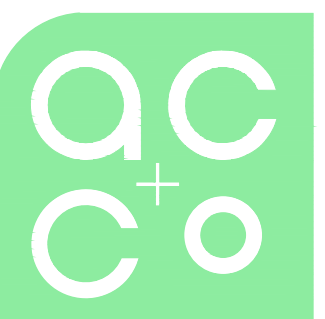
JOB NO.: 2025.0005

DATE: JUNE 5, 2025

DRAWN: CJA

REVISIONS:

- CITY REV 7.15.2025
- CITY REV 8.4.2025



ARCHITECTURE COMMUNITY

1100 SE LIBERTY ST

SUITE 200

Salem, OR 97302

P: 503.581.4114

www.accoas.com

HERITAGE FIDUCIARY

TENANT

IMPROVEMENT

1097 LIBERTY ST SE

SALEM, OREGON 97302

SHEET

A1.0