

July 26, 2024

Bryce Bishop, Senior Planner
City of Salem Community Development Planning Division
555 Liberty Street SE, Room 305
Salem, OR 97301



RE: 1105 Front Street NE – The Cannery (Reference No. 24-106451-PLN) Completeness Response

Mr. Bishop,

Please accept this letter and the accompanying materials as Applicant’s response to the City’s July 19, 2024, second review comments letter regarding application 24-106451-PLN (Attachment A). We believe the materials provided here fully respond to the items outlined in the City’s letter and establish the necessary basis to deem the application complete. We look forward to continuing to work with City staff on any issues, as necessary, during the review and approval process.

Our responses to the incompleteness items are as follows:

Completeness Items

1. ***Approved Subdivision Name.*** *A copy of the required Marion County Subdivision/Condominium Name Request Form was submitted but the form does not include the approval of the Marion County Surveyor. A copy of the completed form with Marion County Surveyor’s approval is needed.*

Response: A copy of the approved Marion County Subdivision/Condominium Name Request Form is included as Attachment B.

2. ***Additional Comments on Plans.***

Preliminary Site Plan

Removed portion of Building 3

Response: The Preliminary Site Plan has been revised to reflect the removed portion of Building 3 as shown in Attachment C. This item is complete.

Driveway entrances to parking garages

Response: The driveway entrances have been revised on the Preliminary Site Plan as shown in Attachment C. This item is complete.

Summary tables

Response: The summary tables on the Preliminary Site Plan have been revised as shown in Attachment C. This item is complete.

Please note Staff’s comment regarding the additional two bicycle parking spaces required to accommodate the dwelling unit increase is not applicable and no change has been made. As shown on the Preliminary Site Plan summary tables, 421 bicycle parking spaces are required, and 439 bicycle parking spaces are provided. Per SRC Table 806-9, up to 100 percent of the minimum required bicycle parking spaces are *allowed* to be long-term, they

are not required to be long-term. The surplus in bicycle parking spaces in front of Buildings 1, 2, and 3 provides the additional 2 spaces required for the dwelling unit increase.

Landscape Land Use Diagram and Planting Schedule Level 1

Tree canopy calculations

Response: A revised Landscape Land Use Diagram is provided in Attachment D. Tree planning islands have been revised to include at least three trees and to meet soil requirements. The parking lot tree canopy coverage is now 42 percent of the total parking area, exceeding the required 40 percent canopy coverage. This item is met.

Tree species

Response: A revised Planting Schedule is provided in Attachment D. The trees identified as not being within the approved trees for planting in parking lots to achieve required tree canopy coverage have been replaced with trees specified in SRC Table 806-7. This item is met.

Please note that previous application materials indicated that all landscaping will be comprised of native vegetation. Unfortunately, few of the trees specified in SRC Table 806-7 are native species. Applicant's Landscape Architect identified one tree which could be replaced with a native tree; however, there are no suitable options for the other two trees. Although they are not native, the tree species selected have close relatives which are native.

Preliminary Building Elevations and Floor Plans

Compact vehicle parking stall width abutting wall or post

Response: Vehicle parking spaces within the parking garages in Buildings 1, 2, and 3 have been revised to meet parking space dimensions as shown on the revised Preliminary Building Elevations and Floor Plans in Attachment E. This item is met.

Canopy and balcony projections

Response: Planned property lines have been modified such that no canopies, balconies, or living space project across a property line as detailed on the revised Preliminary Building Elevations and Floor Plans in Attachment E. The 6th floor living space that projected into the Front Street NE right-of-way has been modified. This item is met.

Bicycle parking spaces

Response: The bicycle parking spaces have been revised as requested. Refer to the revised Preliminary Building Elevations and Floor Plans in Attachment E. This item is met.

Development Services Comments

1. ***Willamette Greenway Permit Submittal Requirements.*** As identified in the Supplemental Completeness Letter dated April 22, 2024, a Class 2 Willamette Greenway Permit is required and

the applicant has not submitted the required items for the Willamette Greenway Permit. Please provide the following additional information:

SRC 600.015(d)(5): A report by a certified engineering geologist or geotechnical engineer demonstrating that the standards specified in SRC 600.025(a)(2) have been met.

- The applicants geotechnical report submitted does not specifically address SRC 600.025(a)(2). Please modify the report or provide a memorandum addressing the development standard.*

Response: A memorandum from Applicant's geotechnical engineer addressing SRC 600.025(a)(2) is included as Attachment F. This item is complete.

SRC 600.015(d)(6): A report by a registered professional engineer detailing the hydraulic and flood carrying capacity of the river.

- The applicant's Engineer shall submit a memorandum describing the minimum impact to the flood capacity of the river in order to meet this submittal requirement in lieu of a hydraulic analysis.*

Response: A flood capacity memorandum from Applicant's engineer is included as Attachment G. This item is complete.

Items of Concern

- 1. Stormwater Management – Design Exceptions.** *The applicant has met their minimum submittal requirement relating to stormwater management; however, the proposal includes an intention to apply for Design Exceptions for water quality facility depth. The applicant is advised that if the Design Exceptions(s) are not approved, modifications to the site plan may be required.*
- 2. Street Tree Plan.** *Phase 1 of the proposed development has approximately 900-feet of frontage along Front Street NE and includes three (3) street trees on the landscaping plan, one of which conflicts with a fire hydrant. The applicant is advised that additional street trees along Front Street NE are required. The applicant should explore ways to accommodate additional trees along the frontage. The applicant should explore the use of tree wells along the property frontage. Please review the standards listed in PWDS 6.34(e)(4).*
- 3. Alternative Street Standard.** *Front Street NE requires an Alternative Street Standard to allow a modified cross section. The final Front Street improvements will be subject to Railroad requirements after the Railroad Diagnostic Review scheduled for July 16, 2024. Final Front Street improvement requirements will be established in the written railroad diagnostic provided after the on-site meeting.*
- 4. Public Utility Easements.** *SRC 803.035(n) requires a 10-foot public utility easement along all street frontages to accommodate Franchise Utility Companies. It appears the applicant is not proposing any PUEs along Front Street NE. The applicant is advised to submit an alternative PUE plan that can be included in the request for comments to the Utilities providers. If a PUE is requested by a franchise utility company, it may be a condition of approval, which would require modifications to the building layout.*
- 5. Driveway Approach Standard Detail.** *The applicant's plans show the driveway approaches to Front Street NE utilizing PWDS Standard Plan No. 315 where PWDS Standard Plan No. 302 is*

required. Conditions of approval for the Driveway Approach Permits will require the driveway approaches be constructed to the appropriate PWDS standard plan.

Response: Applicant acknowledges the items of concern and will continue to coordinate with City staff and other necessary stakeholders.

Additional Information from Applicant

Applicant's traffic engineer prepared an addendum to the Traffic Impact Analysis (TIA) providing an updated trip generation with the adjusted unit count (Attachment H). The memorandum concludes that the change in the number of units from 371 to 382 will not affect the conclusions in the TIA.

Thank you for reviewing our application. We believe these additional clarifications and plan revisions completely respond to the items included in your July 19, 2024, second review comments letter. Please contact me if you require any additional information.

Sincerely,

AKS ENGINEERING & FORESTRY, LLC



Grace Wolff

3700 River Road N, Suite 1

Keizer, OR 97303

(503) 400-6028 | wolffg@aks-eng.com

Attachments:

- A. Land Use Application Completeness Review (July 19, 2024)
- B. Marion County Subdivision Name Approval
- C. Revised Preliminary Land Use Plans
- D. Revised Preliminary Landscape Plans
- E. Revised Preliminary Building Elevations and Floor Plans
- F. Geotechnical Engineering Services Memorandum
- G. Flood Carrying Capacity Memorandum
- H. Traffic Impact Analysis Memorandum

Attachment A: Land Use Application Completeness Review (July 19, 2024)



Community Planning and Development

555 Liberty St. SE / Room 305 • Salem, OR 97301-3503 • 503-588-6173 • www.cityofsalem.net

July 19, 2024

LAND USE APPLICATION COMPLETENESS REVIEW

Second Review Comments

Subject Property: 1105 Front Street NE

Reference Nos.: 24-106451-PLN (Class 3 Site Plan Review, Tentative Subdivision Plan, Class 1 & 2 Adjustment, Class 2 Driveway Approach Permit)

Applicant: Trent Michels
The Future of Neighborhood Development, LLC
Phone:
E-Mail: trent.michels@gmail.com

Agent: Grace Wolff
AKS Engineering & Forestry, LLC
3700 River Road N
Keizer, OR 97303
Phone: 503-400-6028
E-Mail: wolffg@aks-eng.com

The Planning Division has conducted its review of the revised application materials submitted for the proposed Class 3 Site Plan Review, Tentative Subdivision Plan, Class 1 and 2 Adjustment, and Class 2 Driveway Approach Permit for property located at 1105 Front Street NE. In order to deem the application complete and to continue processing the application, modifications/and or additional information is needed to address the following item(s):

Item:	Description:
Approved Subdivision Name	A copy of the required <i>Marion County Subdivision/Condominium Name Request Form</i> was submitted but the form does not include the approval of the Marion County Surveyor. A copy of the completed form with Marion County Surveyor's approval is needed.
Additional Comments on Plans	Please see the additional staff comments included on the attached plans.
<u>Development Services Comments</u>	
<i>Submittal Requirements - The following items have been identified as required material to be provided by the applicant prior to deeming the application "complete".</i>	
Willamette Greenway Permit Submittal Requirements	As identified in the Supplemental Completeness Letter dated April 22, 2024, a Class 2 Willamette Greenway Permit is required and the applicant has not submitted the required items for the Willamette Greenway Permit. Please provide the following additional information: SRC 600.015(d)(5) : A report by a certified engineering geologist or geotechnical engineer demonstrating that the standards specified in SRC 600.025(a)(2) have been met.

Item:	Description:
	<ul style="list-style-type: none"> ▪ The applicants geotechnical report submitted does not specifically address SRC 600.025(a)(2). Please modify the report or provide a memorandum addressing the development standard. <p>SRC 600.015(d)(6): A report by a registered professional engineer detailing the hydraulic and flood carrying capacity of the river.</p> <ul style="list-style-type: none"> ▪ The applicant's Engineer shall submit a memorandum describing the minimum impact to the flood capacity of the river in order to meet this submittal requirement in lieu of a hydraulic analysis.
<p><i>Items of Concern</i> - The following items are not listed in the SRC as specific requirements for a complete application; however, are advisories that address areas of concern on the application. Failure to address advisory comments could result in condition of approval or denial of the application(s).</p>	
<p>Stormwater Management – Design Exceptions</p>	<p>The applicant has met their minimum submittal requirement relating to stormwater management; however, the proposal includes an intention to apply for Design Exceptions for water quality facility depth. The applicant is advised that if the Design Exceptions(s) are not approved, modifications to the site plan may be required.</p>
<p>Street Tree Plan</p>	<p>Phase 1 of the proposed development has approximately 900-feet of frontage along Front Street NE and includes three (3) street trees on the landscaping plan, one of which conflicts with a fire hydrant. The applicant is advised that additional street trees along Front Street NE are required. The applicant should explore ways to accommodate additional trees along the frontage. The applicant should explore the use of tree wells along the property frontage. Please review the standards listed in PWDS 6.34(e)(4).</p>
<p>Alternative Street Standard</p>	<p>Front Street NE requires an Alternative Street Standard to allow a modified cross section. The final Front Street improvements will be subject to Railroad requirements after the Railroad Diagnostic Review scheduled for July 16, 2024. Final Front Street improvement requirements will be established in the written railroad diagnostic provided after the on-site meeting.</p>
<p>Public Utility Easements</p>	<p>SRC 803.035(n) requires a 10-foot public utility easement along all street frontages to accommodate Franchise Utility Companies. It appears the applicant is not proposing any PUEs along Front Street NE. The applicant is advised to submit an alternative PUE plan that can be included in the request for comments to the Utilities providers. If a PUE is requested by a franchise utility company, it may be a condition of approval, which would require modifications to the building layout.</p>
<p>Driveway Approach Standard Detail</p>	<p>The applicant's plans show the driveway approaches to Front Street NE utilizing PWDS Standard Plan No. 315 where PWDS Standard Plan No. 302 is required. Conditions of approval for the Driveway Approach Permits will require the driveway approaches be constructed to the appropriate PWDS standard plan.</p>

Your application, which is incomplete, will be deemed complete upon receipt of one of the following:

- 1) All of the missing information;
- 2) Some of the missing information and written notice from you (the applicant) that no other information will be provided; or

- 3) Written notice from you (the applicant) that none of the missing information will be provided.

You have 180 days from the date the application was first submitted to respond in one of the three ways listed above, or the application will be deemed void.

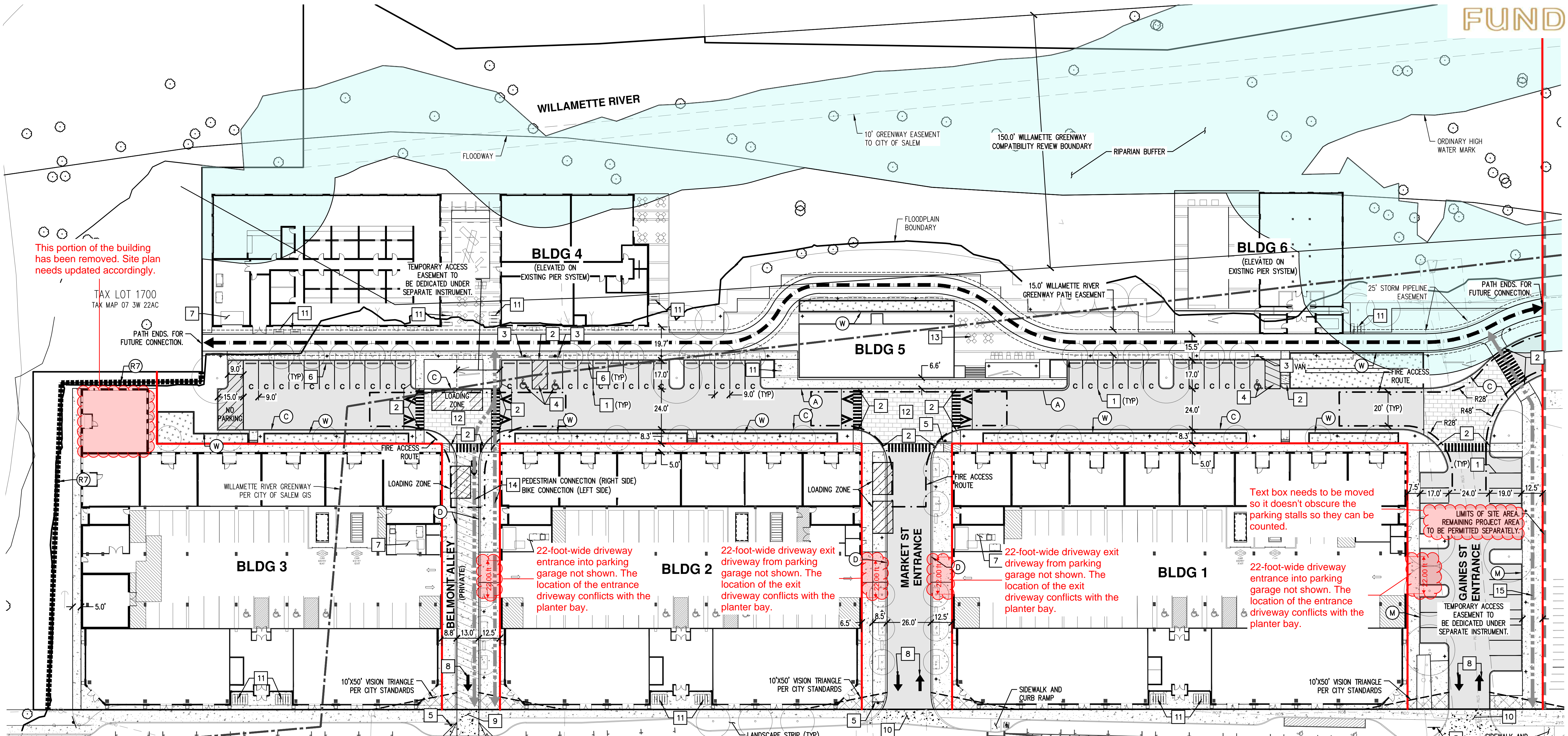
For questions regarding any of the above requirements, please feel free to contact me directly by calling (503) 540-2399 or via e-mail at bbishop@cityofsalem.net.

The Salem Revised Code may be accessed online at the following location:

<https://www.cityofsalem.net/government/laws-rules/salem-revised-code>

Sincerely,

Bryce Bishop
Planner III



This portion of the building has been removed. Site plan needs updated accordingly.

TAX LOT 1700
TAX MAP 07 3W 22AC

Text box needs to be moved so it doesn't obscure the parking stalls so they can be counted.

22-foot-wide driveway entrance into parking garage not shown. The location of the entrance driveway conflicts with the planter bay.

22-foot-wide driveway exit driveway from parking garage not shown. The location of the exit driveway conflicts with the planter bay.

22-foot-wide driveway exit driveway from parking garage not shown. The location of the exit driveway conflicts with the planter bay.

22-foot-wide driveway entrance into parking garage not shown. The location of the entrance driveway conflicts with the planter bay.

LIMITS OF SITE AREA. REMAINING PROJECT AREA TO BE PERMITTED SEPARATELY.

CURB KEYED NOTES: [TR]

- (A) TYPE 'A' CURB AND GUTTER
- (C) TYPE 'C' CURB
- (D) TYPE 'D' MOUNTABLE CURB
- (M) MONOLITHIC CURB AND SIDEWALK
- (W) PLANTER WALL
- (R7) EXISTING RETAINING WALL. STRUCTURAL IMPROVEMENTS REQUIRED TO BE DETERMINED AT TIME OF BUILDING PERMIT

SITE KEYED NOTES: [#]

1. PAINT 4-INCH WIDE WHITE STRIPE PER CITY STANDARDS.
2. ACCESSIBLE CURB RAMP AND DETECTABLE WARNING SURFACE.
3. ACCESSIBLE PARKING SIGN. "VAN" INDICATES VAN ACCESSIBLE SIGN.
4. ACCESSIBLE PARKING STALLS AND AISLE STRIPING.
5. INSTALL 30"x30" STOP SIGN AND STOP BAR. (36"x36" WHEN ENTERING PUBLIC ROW)
6. CONCRETE WHEEL STOP.
7. TRASH ENCLOSURE. SEE ARCHITECTURAL PLANS FOR DETAILS.
8. DIRECTIONAL ARROW STRIPE.
9. COMMERCIAL DRIVEWAY APPROACH PER CITY DETAIL NO.302.
10. COMMERCIAL DRIVEWAY APPROACH PER CITY DETAIL NO.315.
11. BIKE RACK, SEE ARCHITECTURAL PLANS FOR DETAILS.
12. RAISED SPEED TABLE PEDESTRIAN CROSSING.
13. OUTDOOR SEATING. SEE LANDSCAPE PLANS FOR DETAILS.
14. SHARED ACCESS FROM WILLAMETTE GREENWAY PATH TO FRONT STREET. EASEMENT TO BE QUITCLAIMED UPON CONSTRUCTION OF PATH TO THE SOUTH.
15. SHARED ACCESS FROM WILLAMETTE GREENWAY PATH TO FRONT STREET. EASEMENT TO BE QUITCLAIMED UPON CONSTRUCTION OF PHASE 2 IMPROVEMENTS.

SITE PLAN DATA:

ZONING =	MU-R
SUBJECT PROPERTY AREA =	±593,899 SF (±13.6 ACRES)
SITE AREA =	±333,110 SF (±7.6 ACRES)
DENSITY:	
MULTI-FAMILY =	371 UNITS
*RETAIL =	12,160 SF
*OFFICE =	5,885 SF
*EATING/DRINKING ESTABLISHMENT =	30,875 SF
*DISTRIBUTION OF RETAIL, OFFICE, AND EATING/DRINKING ESTABLISHMENTS SQUARE FOOTAGE ARE SUBJECT TO CHANGE.	
PARKING SUMMARY:	
MAXIMUM VEHICLE PARKING:	
MULTI-FAMILY =	649 SPACES (1.75 COUNT)
RETAIL =	61 SPACES (1/200 SF)
OFFICE =	24 SPACES (1/250 SF)
EATING/DRINKING ESTABLISHMENT =	176 SPACES (1/175 SF)
MAX TOTAL =	910 SPACES
VEHICLE PARKING PROVIDED:	
GARAGE PARKING	
• AUTOMATED =	291 SPACES
• STANDARD =	30 SPACES
• COMPACT =	54 SPACES
• ACCESSIBLE =	12 SPACES
OFF-STREET PARKING	
• STANDARD =	20 SPACES
• COMPACT =	35 SPACES
• ACCESSIBLE =	3 SPACES
TOTAL PARKING =	445 SPACES

A total of 382 units are now provided based on the further revised plans

Max. 669 spaces based on new unit count

Max. Total 930 spaces based on new unit count

BICYCLE PARKING REQUIRED:	
MULTI-FAMILY =	571 SPACES (1/UNIT)
RETAIL =	4 SPACES (GREATER 0)
OFFICE =	4 SPACES (GREATER 0)
EATING/DRINKING ESTABLISHMENT =	31 SPACES (GREATER OF 4 OR 1/1,000 SF)
TOTAL REQUIRED =	610 SPACES
BICYCLE PARKING PROVIDED:	
SHORT-TERM =	59 SPACES
LONG-TERM =	380 SPACES
TOTAL =	439 SPACES
LOADING ZONE REQUIRED/PROVIDED:	
MULTI-FAMILY REQUIRED =	3 SPACES (12'WX19')
RETAIL SALES AND SERVICES REQUIRED =	1 SPACE (12'WX30')
OFFICE REQUIRED =	1 SPACE (OFF-STREET FOR LOADING PER DWELLING UNIT)
TOTAL REQUIRED =	4 SPACES
TOTAL PROVIDED =	3 SPACES (12'WX19') 1 SPACE (12'WX30')

382 spaces are now required based on the further revised plans

421 total spaces are now required based on the further revised plans

Additional two bike spaces required to accommodate dwelling unit increase to 382

NOTE: SPACES TO BE SCHEDULED AND CONED OFF WITH SITE OPERATOR FOR LOADING AND UNLOADING.

SETBACKS:

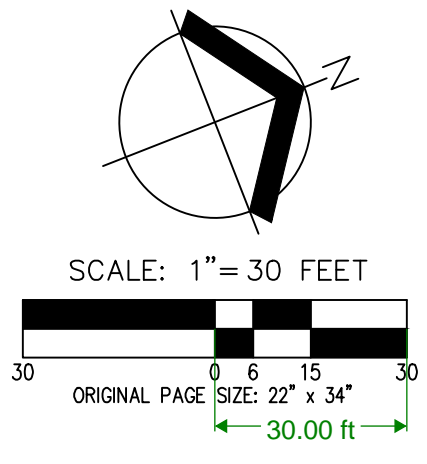
ALONG FRONT ST BUILDINGS =	0 FT OR MAX 10 FT (IF SETBACK AREA IS USED FOR PEDESTRIAN AMENITIES)
VEHICLE USE AREA =	10 FT
SIDE/REAR YARD BUILDINGS =	NONE
VEHICLE USE AREA =	5 FT (NOT REQUIRED ABUTTING AN ALLEY)

GENERAL NOTES:

1. BUILDINGS 1, 2, AND 3 ARE ON SEPARATE PROPERTIES REFER TO SHEET P4 FOR THE PROPOSED PROPERTY LINES.
2. THE FRONT STREET NE IMPROVEMENTS SHOWN ARE PRELIMINARY AND BASED ON CONCEPTUAL DESIGN WORK PROVIDED BY THE CITY'S RETAINED RAIL ENGINEER. REFINED FRONT STREET NE IMPROVEMENTS ARE ANTICIPATED AND WILL BE CONSTRUCTED IN ACCORDANCE WITH FEEDBACK RECEIVED FROM THE FINAL RAIL DIAGNOSTIC AND COORDINATION WITH THE CITY.

EV READY NOTE:

40% OF PARKING STALLS ARE REQUIRED TO BE EV READY PER STATE REQUIREMENTS. FINAL EV READY STALL LOCATION AND CONDUIT PLACEMENT WILL BE COORDINATED WITH PROJECT ELECTRICIAN AT THE TIME OF BUILDING PERMIT SUBMITTAL.



LEGEND

- 10' WILLAMETTE GREENWAY CONCRETE PATH (WITHIN 15' EASEMENT TO CITY OF SALEM)
- TEMPORARY GREENWAY ACCESS TO FRONT STREET UNTIL FUTURE CONNECTIONS ARE CONSTRUCTED.
- INTERIOR PROPERTY LINE
- ASPHALT PAVEMENT SECTION
- CONCRETE SIDEWALK (4" MIN THICKNESS)
- CONCRETE PAVEMENT SECTION (8" MIN THICKNESS)
- CONCRETE PAVERS (REFER TO PLANS BY OTHERS)
- STORMWATER FACILITY
- PLANNED TREE (REFER TO LANDSCAPE PLANS BY OTHERS)

PRELIMINARY SITE PLAN
THE CANNERY
FUND
SALEM, OREGON



REVISIONS: DECEMBER 31, 2024
JOB NUMBER: 5968-01
DATE: 05/31/2024
DESIGNED BY: TDR
DRAWN BY: MJM
CHECKED BY: TDR

WILLAMETTE RIVER

SITE LANDSCAPE DATA

SRC 600.025.B WILLAMETTE RIVER GREENWAY
LANDSCAPING
TOTAL LINEAR FEET OF RIVER FRONTAGE:
894 LF
TREES REQUIRED (1 PER 20LF): 45 TREES
TREES EXISTING: 57 TREES
TREES PROPOSED: 6 TREES
SHRUBS REQUIRED (1 PER 2LF): 447 SHRUBS
SHRUBS PROPOSED: 699 SHRUBS

SRC 806.035.D.2 INTERIOR PARKING LOT
LANDSCAPING
TOTAL PARKING AREA: 28,050 SF
INTERIOR LANDSCAPING REQUIRED:
5.0% (1,403 SF)
INTERIOR LANDSCAPING PROPOSED:
7.3% (2,049 SF)

SRC 806.035.D.3 INTERIOR PARKING LOT TREES
TOTAL PARKING STALLS: 58 STALLS
INTERIOR TREES REQUIRED: 5 TREES
(1 TREE PER 12 STALLS)
INTERIOR TREES PROPOSED: 7 TREES

SRC 806.035.N PARKING LOT TREE CANOPY
TOTAL PARKING AREA: 28,050 SF
REQUIRED TREE CANOPY PERCENTAGE: 40%
REQUIRED TREE CANOPY AREA: 11,220 SF
PROPOSED TREE CANOPY AREA: 15,339 SF
PROPOSED TREE CANOPY PERCENTAGE: 55%

LANDSCAPE ARCHITECTS PC
lango. hansen
1100 nw glisan #3A portland OR 97209 T 503.295.2437

Tree canopy cannot be counted toward max. 40 percent parking lot coverage because the tree is not planted in a planting island containing a minimum of three trees per SRC 806.035(n)(3)(B)(ii) and meeting the minimum soil volume requirements of Table 806-7.

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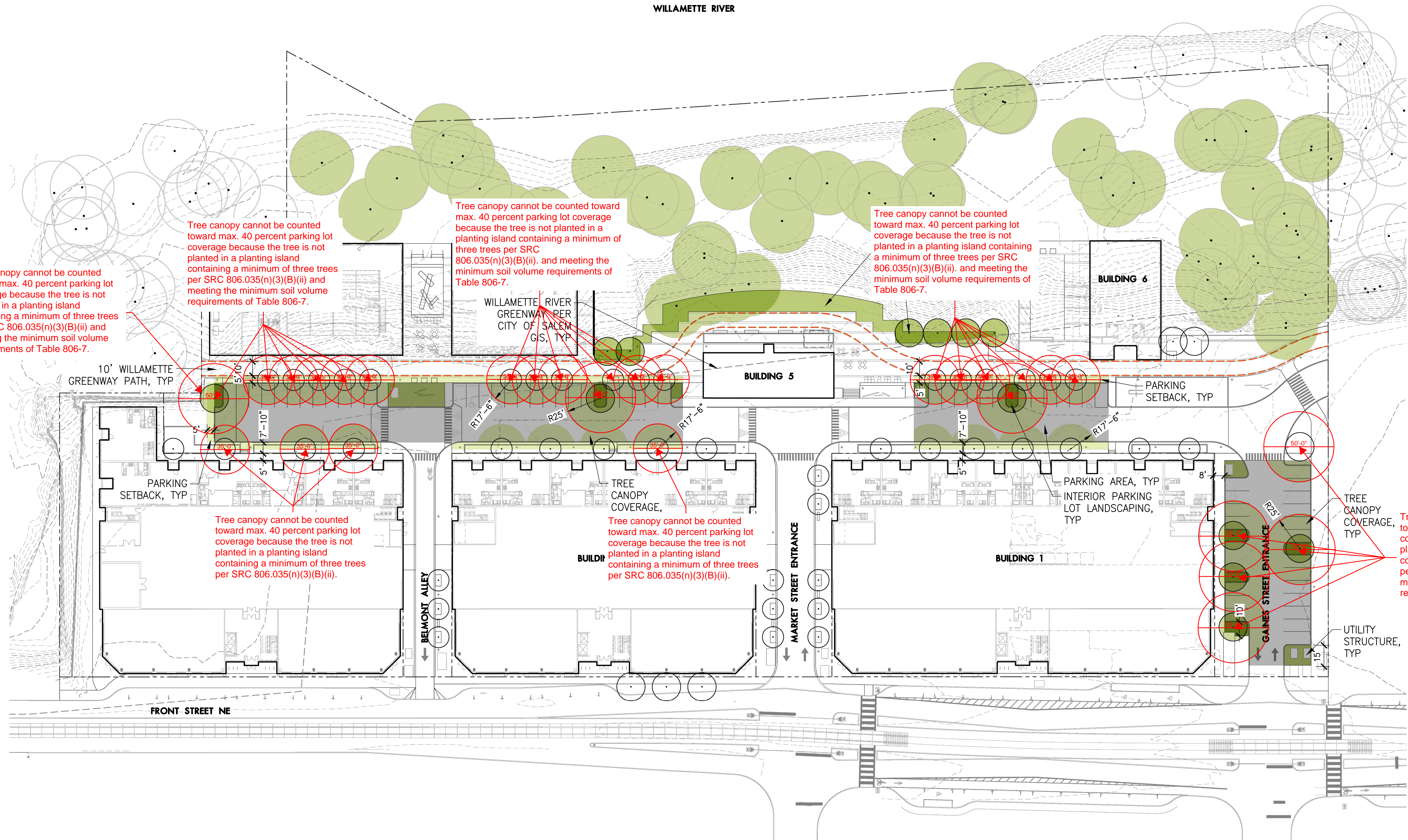
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Tree canopy cannot be counted toward max. 40 percent parking lot coverage because the tree is not planted in a planting island containing a minimum of three trees per SRC 806.035(n)(3)(B)(ii).

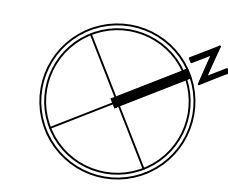
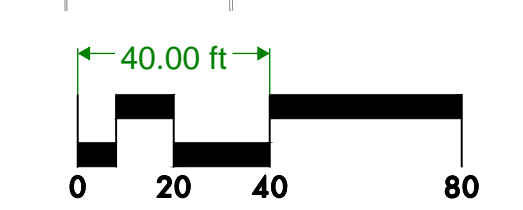
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Tree canopy cannot be counted toward max. 40 percent parking lot coverage because the tree is not planted in a planting island containing a minimum of three trees per SRC 806.035(n)(3)(B)(ii) and meeting the minimum soil volume requirements of Table 806-7.

1 LANDSCAPE LAND USE DIAGRAM



Plan
SCALE: 1" = 40'



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL

LANDSCAPE LAND USE DIAGRAM

REVISIONS

SCALE

DRAWN BY

DATE 2024.03.15

PROJECT NO. 2346-SAC

SHEET

L104

Red Alder is not a species of tree identified in the table of approved trees for planting in parking lots to achieve required tree canopy coverage per SRC 806.035(n)(3)(A)(ii). An alternative species can be approved by providing evidence that the proposed species is native, drought resistant, and will provide a tree canopy area equal to that of either a large tree or small tree as specified in Table 806-7.

Quaking Aspen is not a species of tree identified in the table of approved trees for planting in parking lots to achieve required tree canopy coverage per SRC 806.035(n)(3)(A)(ii). An alternative species can be approved by providing evidence that the proposed species is native, drought resistant, and will provide a tree canopy area equal to that of either a large tree or small tree as specified in Table 806-7.

California Black Oak is not a species of tree identified in the table of approved trees for planting in parking lots to achieve required tree canopy coverage per SRC 806.035(n)(3)(A)(ii). An alternative species can be approved by providing evidence that the proposed species is native, drought resistant, and will provide a tree canopy area equal to that of either a large tree or small tree as specified in Table 806-7.

PLANT SCHEDULE – ONSITE TREES							
SYMBOL	ABBR	BOTANICAL NAME	COMMON NAME	NATIVE	SIZE/CONDITION	SPACING	QUANTITY
TREES							
	AM	Acer macrophyllum	Big Leaf Maple	YES	3" CAL B&B	AS SHOWN	11
	AR	Alnus rubra	Red Alder	YES	3" CAL B&B (SMALL)	AS SHOWN	16
	PP	Parrotia persica 'Vanessa'	Vanessa Ironwood	NO (STREET TREE)	3" CAL B&B	AS SHOWN	3
	PT	Populus tremuloides	Quaking Aspen	YES	3" CAL B&B (SMALL)	AS SHOWN	25
	QK	Quercus kelloggii	California Black Oak	YES	3" CAL B&B (LARGE)	AS SHOWN	8

PLANT SCHEDULE – ONSITE PLANTINGS							
SYMBOL	ABBR	BOTANICAL NAME	COMMON NAME	NATIVE	SIZE/CONDITION	SPACING	QUANTITY
SHRUBS							
OK	COSE	Cornus sericea 'Arctic Fire'	Arctic Fire Redosier Dogwood	YES	#5/CONT. 6'+HT (MATURE)	AS SHOWN	86
OK	MARE	Mahonia repens	Creeping Oregon Grape	YES	#1/CONT.	AS SHOWN	462
OK	GASH	Gaultheria shallon	Salal	YES	#1/CONT.	AS SHOWN	100
OK	VAOV	Vaccinium ovatum	Evergreen Huckleberry	YES	#5/CONT. 6'+HT (MATURE)	AS SHOWN	505
GROUND COVER AND GRASSES							
	ARUV	Arctostaphylos uva-ursi	Kinnikinnick	YES	#1/CONT.	18" O.C.	662
	DECE	Deschampsia cespitosa	Tufted Hairgrass	YES	#3/CONT.	AS SHOWN	130
	FRCH	Fragaria chiloensis	Coast Strawberry	YES	#1/CONT.	18" O.C.	2369
	HEMI	Heuchera micrantha	Smallflowered Alumroot	YES	#1/CONT.	AS SHOWN	159
STORMWATER PLANTINGS – 5,233 SF							
	CADE	Carex densa	Dense Sedge	YES	#1/CONT.	12" O.C.	
	DECE	Deschampsia cespitosa	Tufted Hair Grass	YES	#1/CONT.	12" O.C.	
	JUPA	Juncus patens	Spreading Rush	YES	#1/CONT.	12" O.C.	

THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL
PLANTING SCHEDULE
LEVEL 1

REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

SHEET
L204

268' - 8"

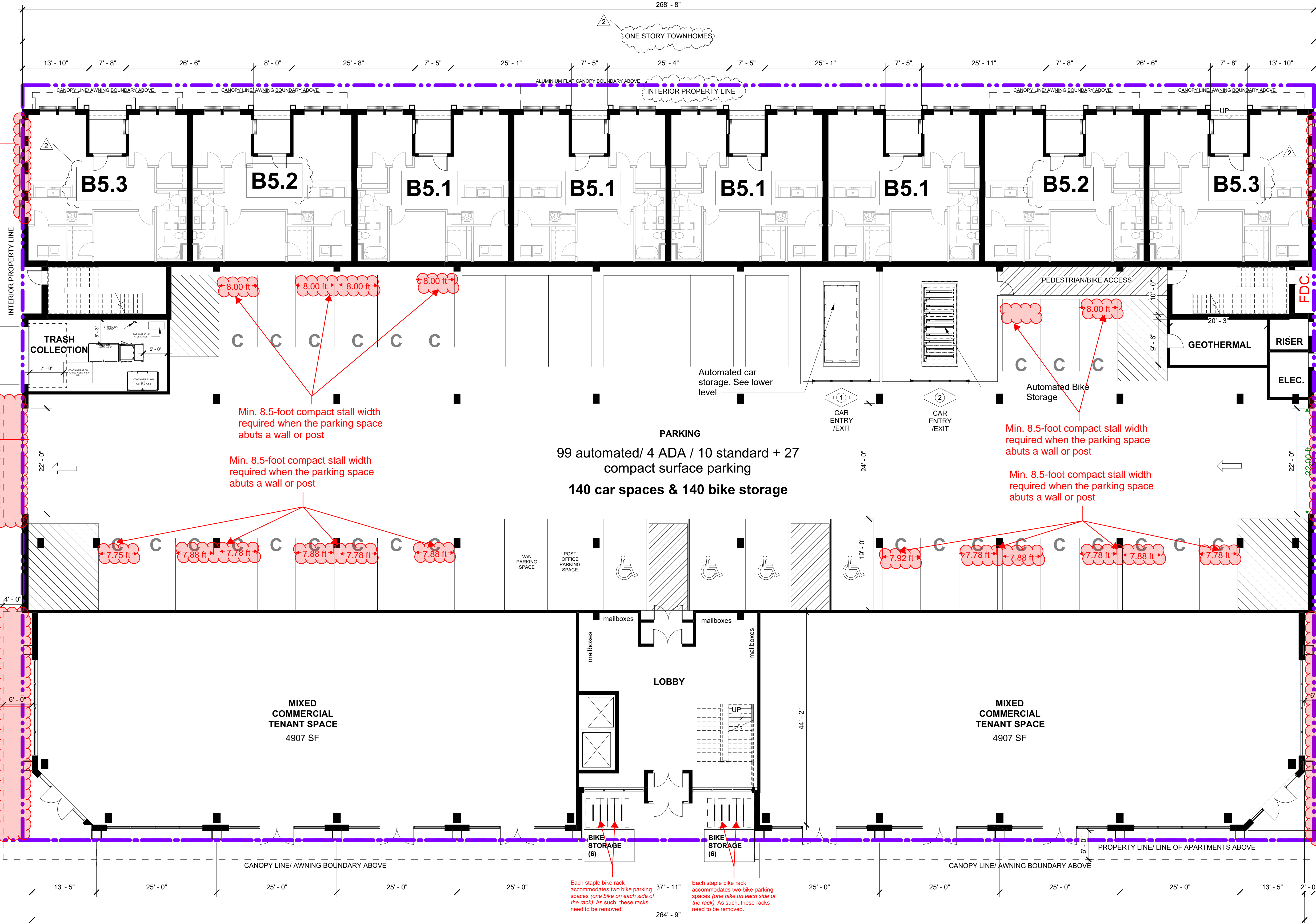
ONE STORY TOWNHOMES

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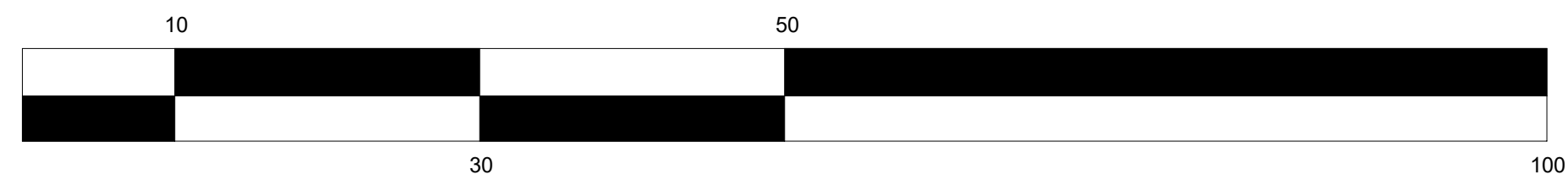
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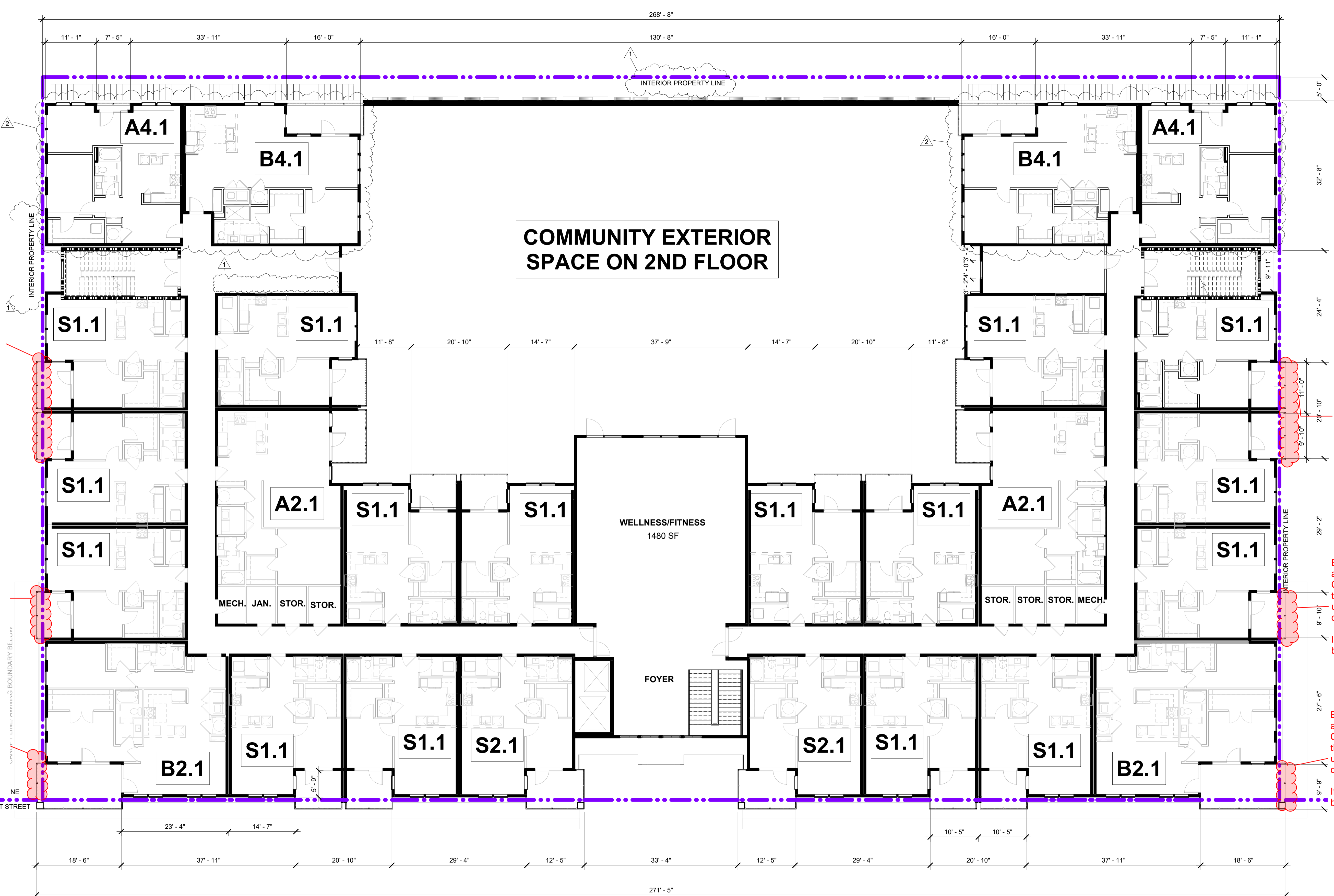
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2	Unit Changes	06/19/2024



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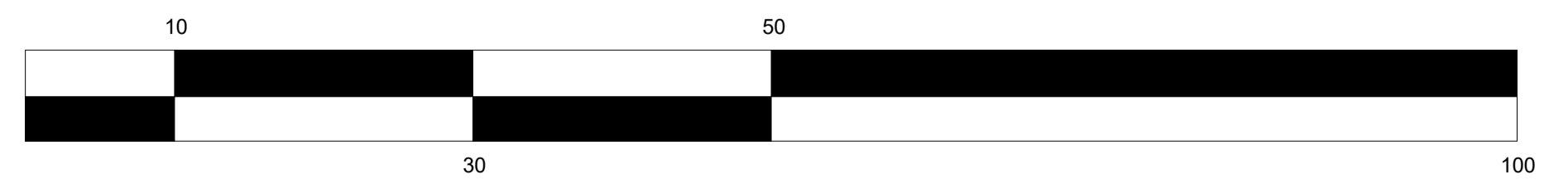
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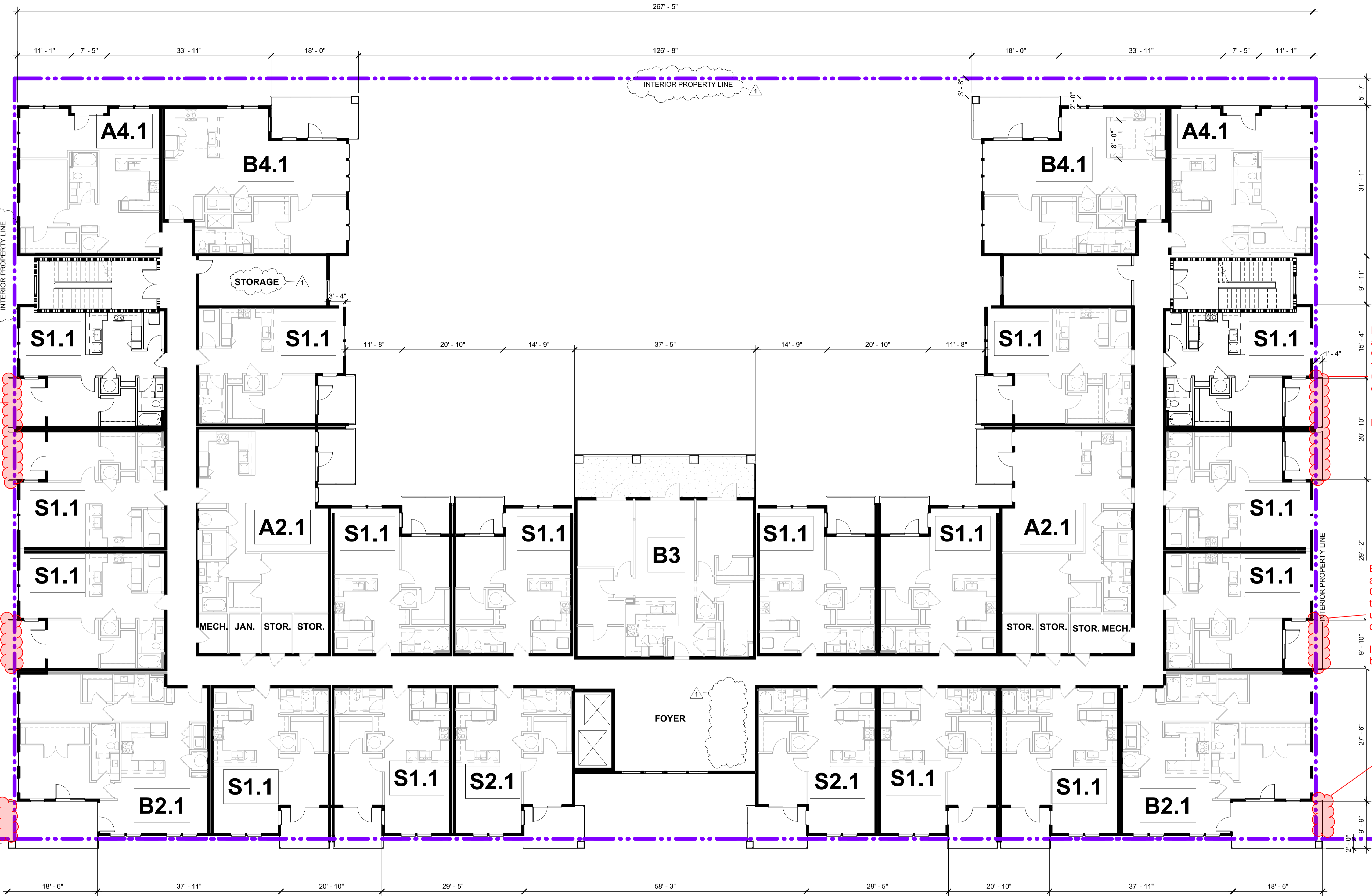
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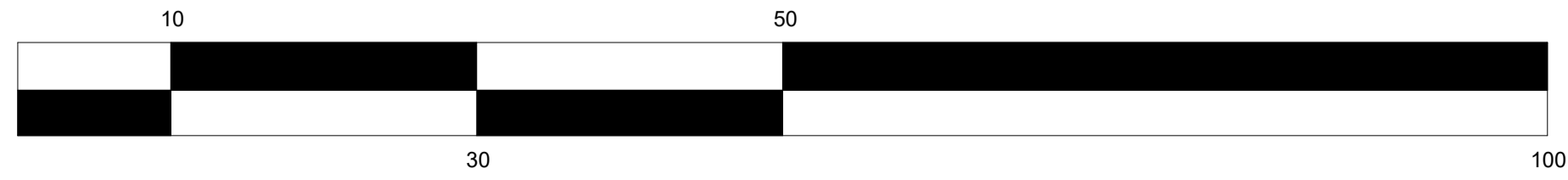
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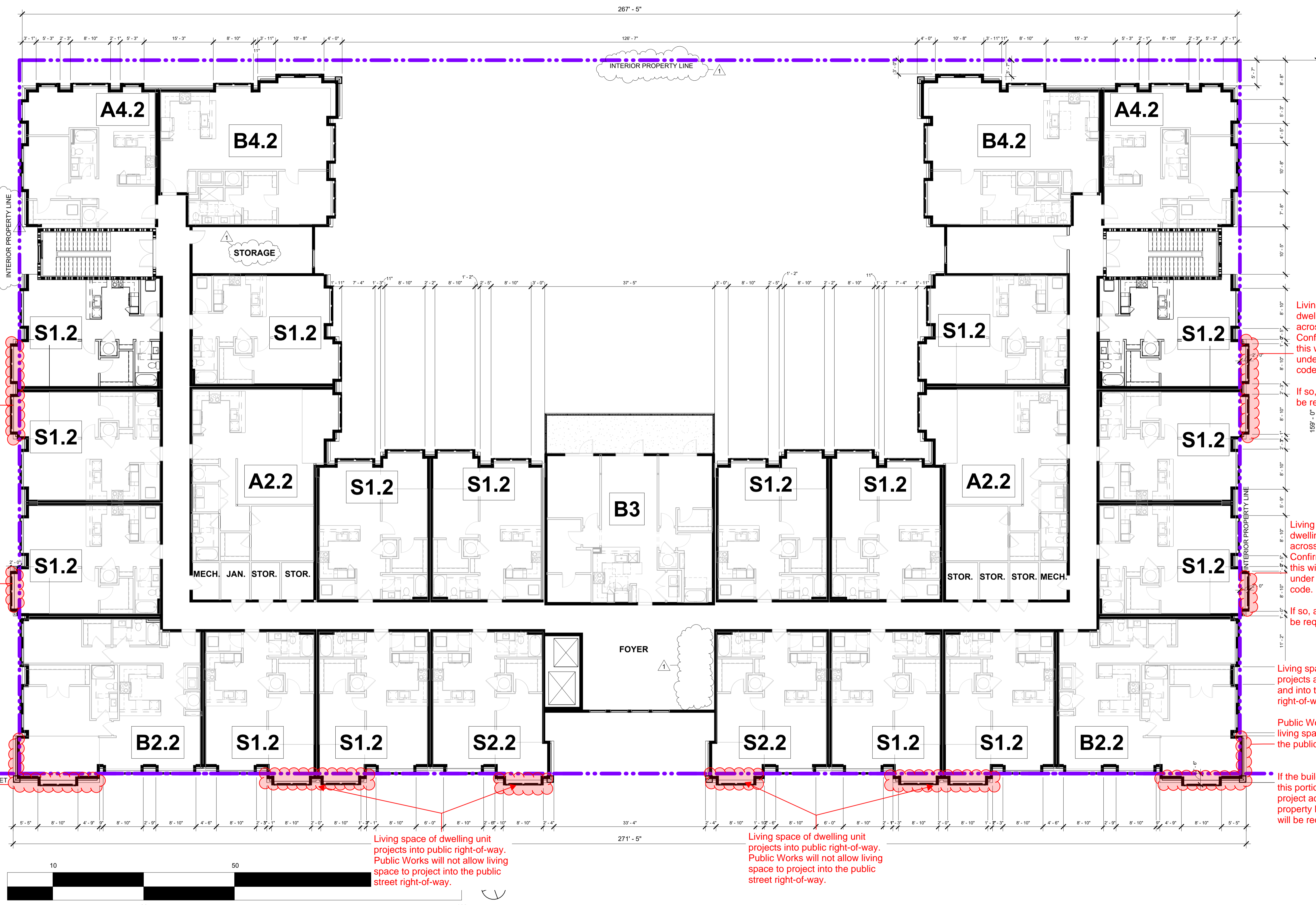
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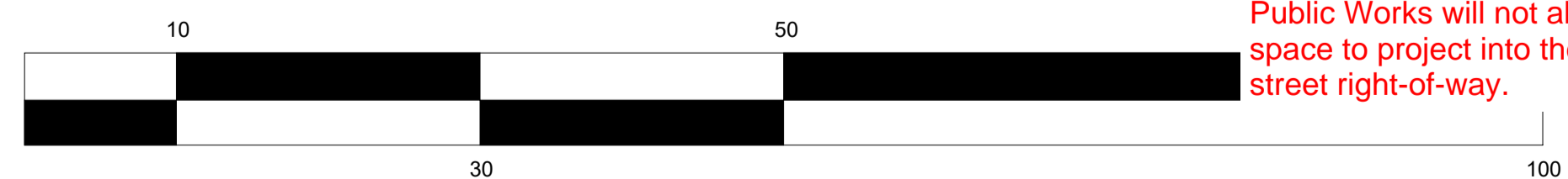
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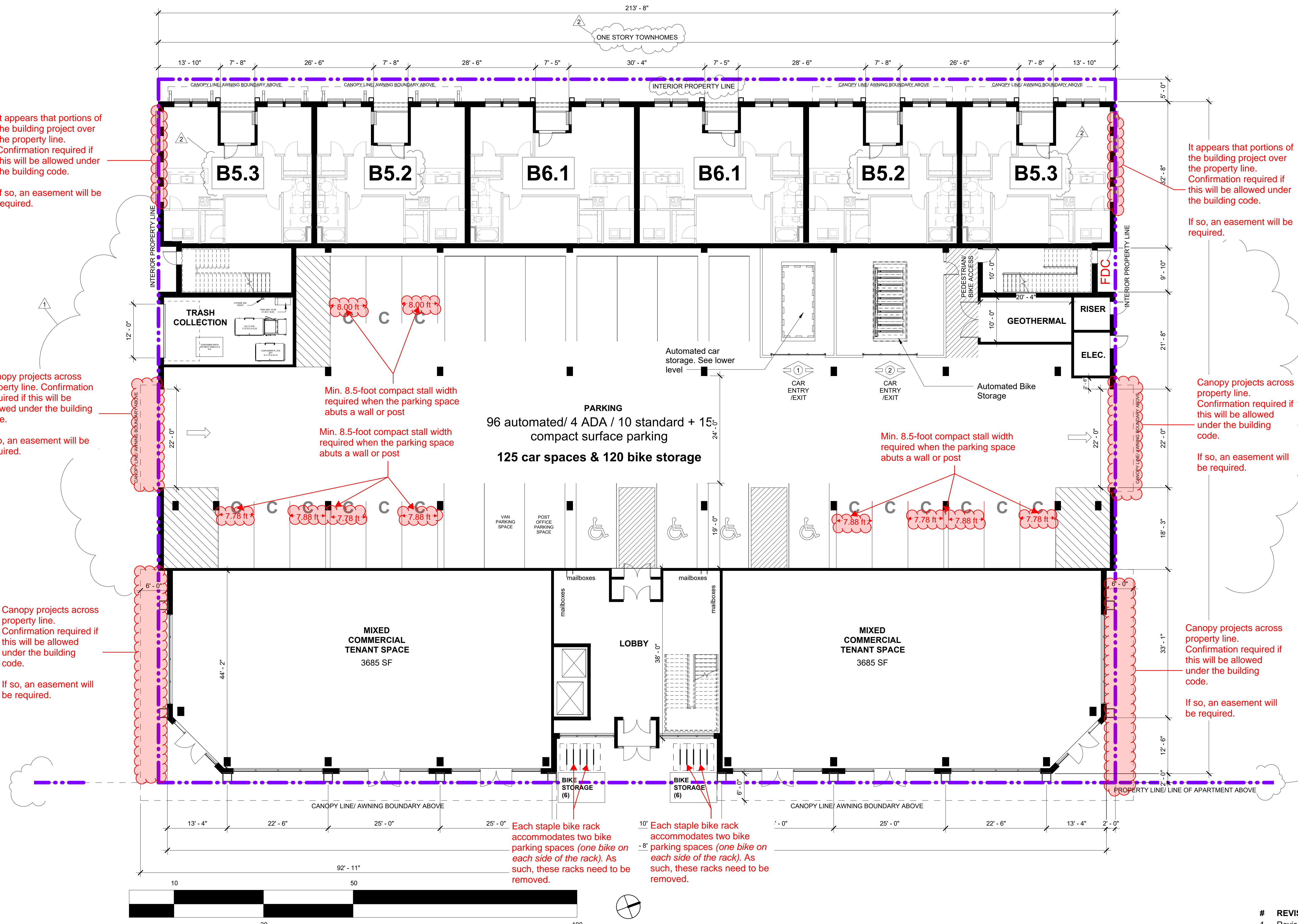
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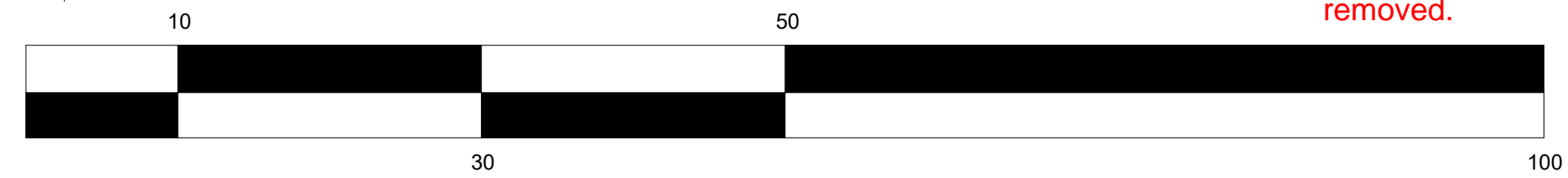
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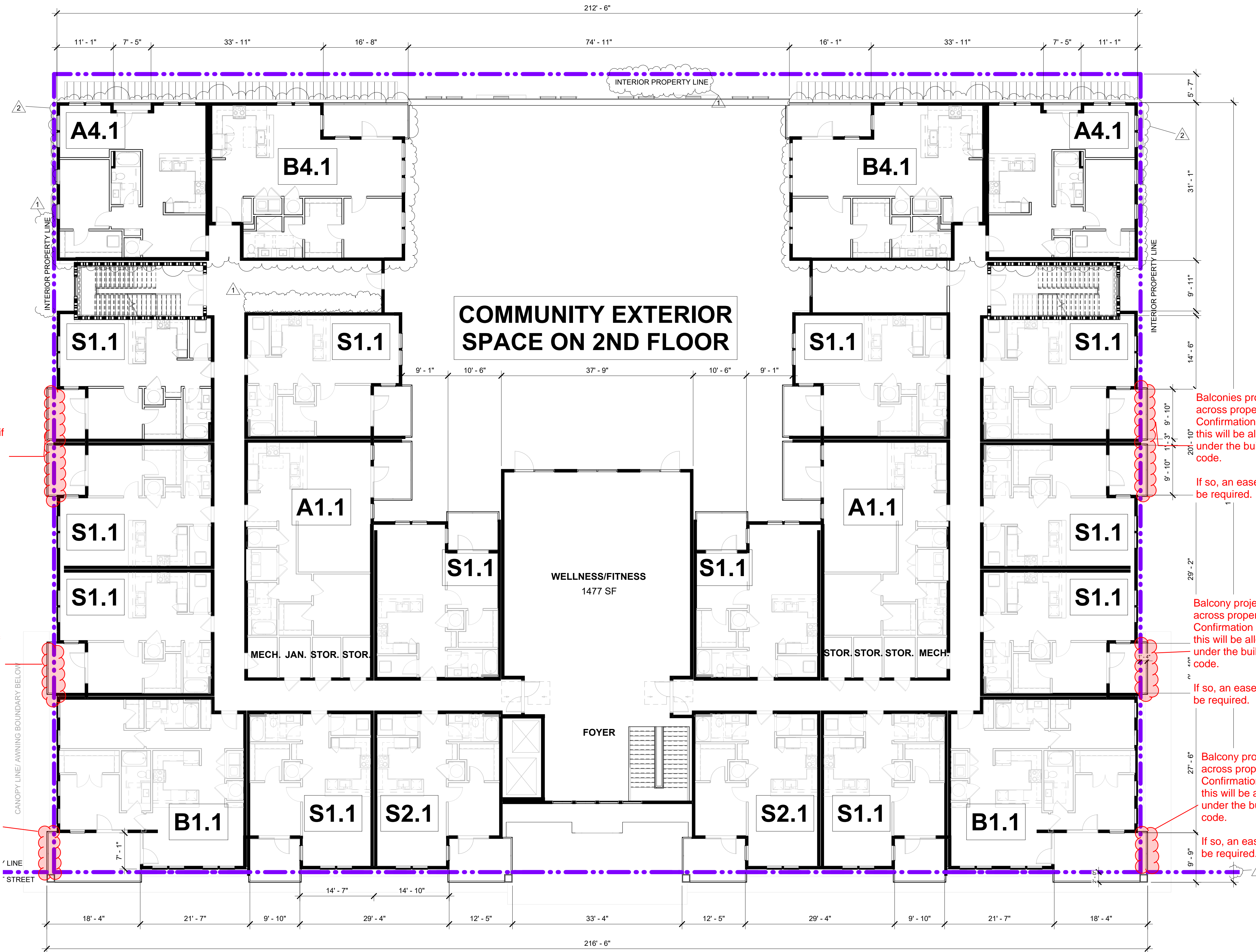
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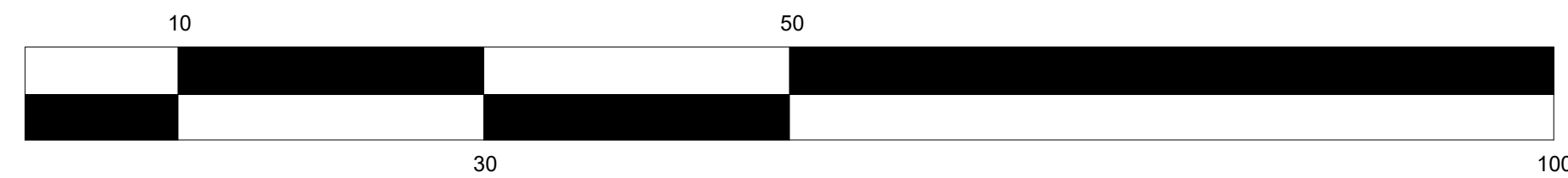
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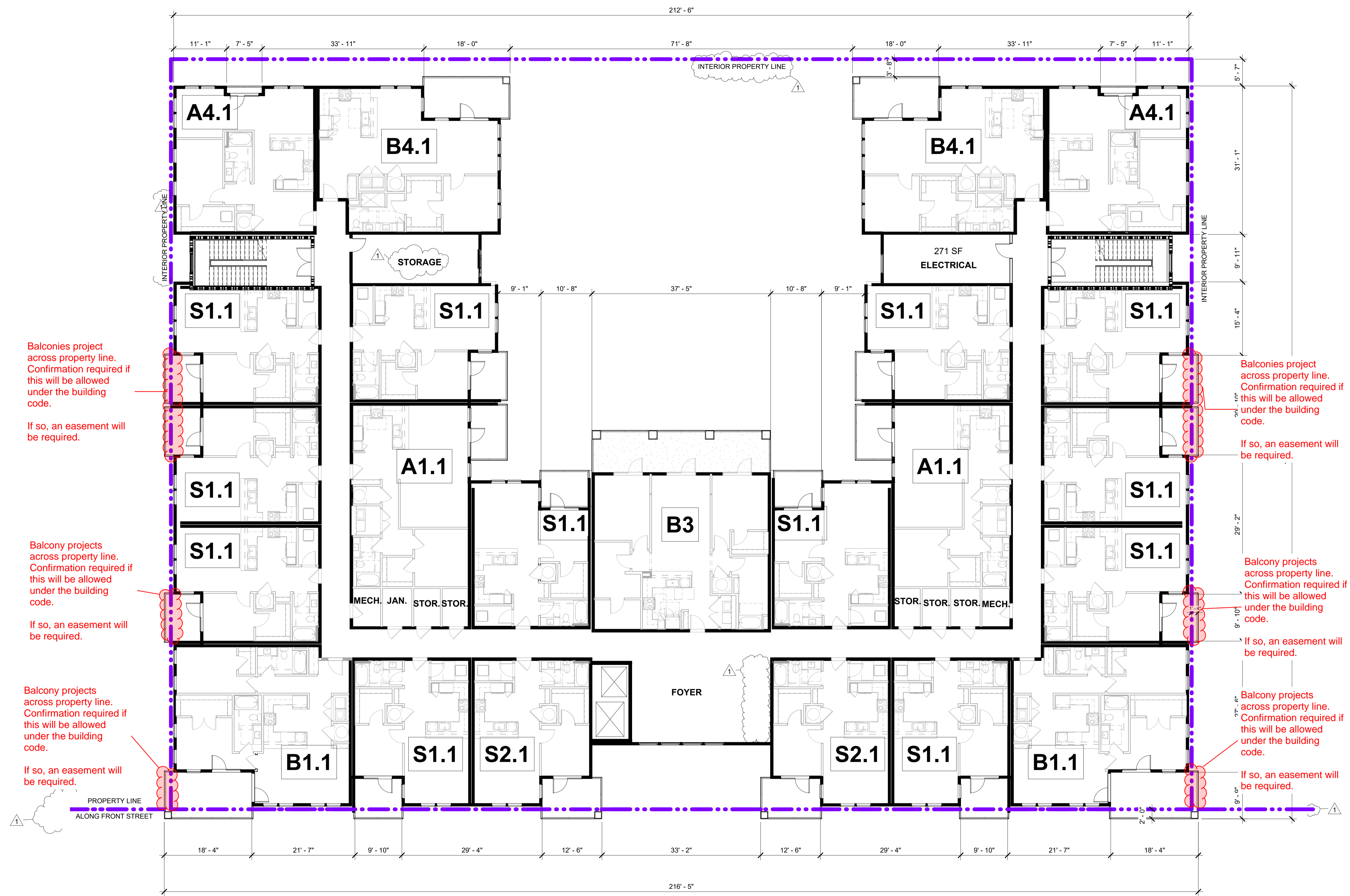
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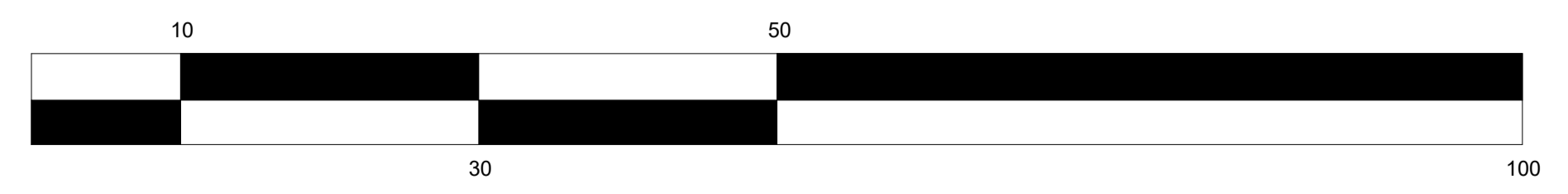
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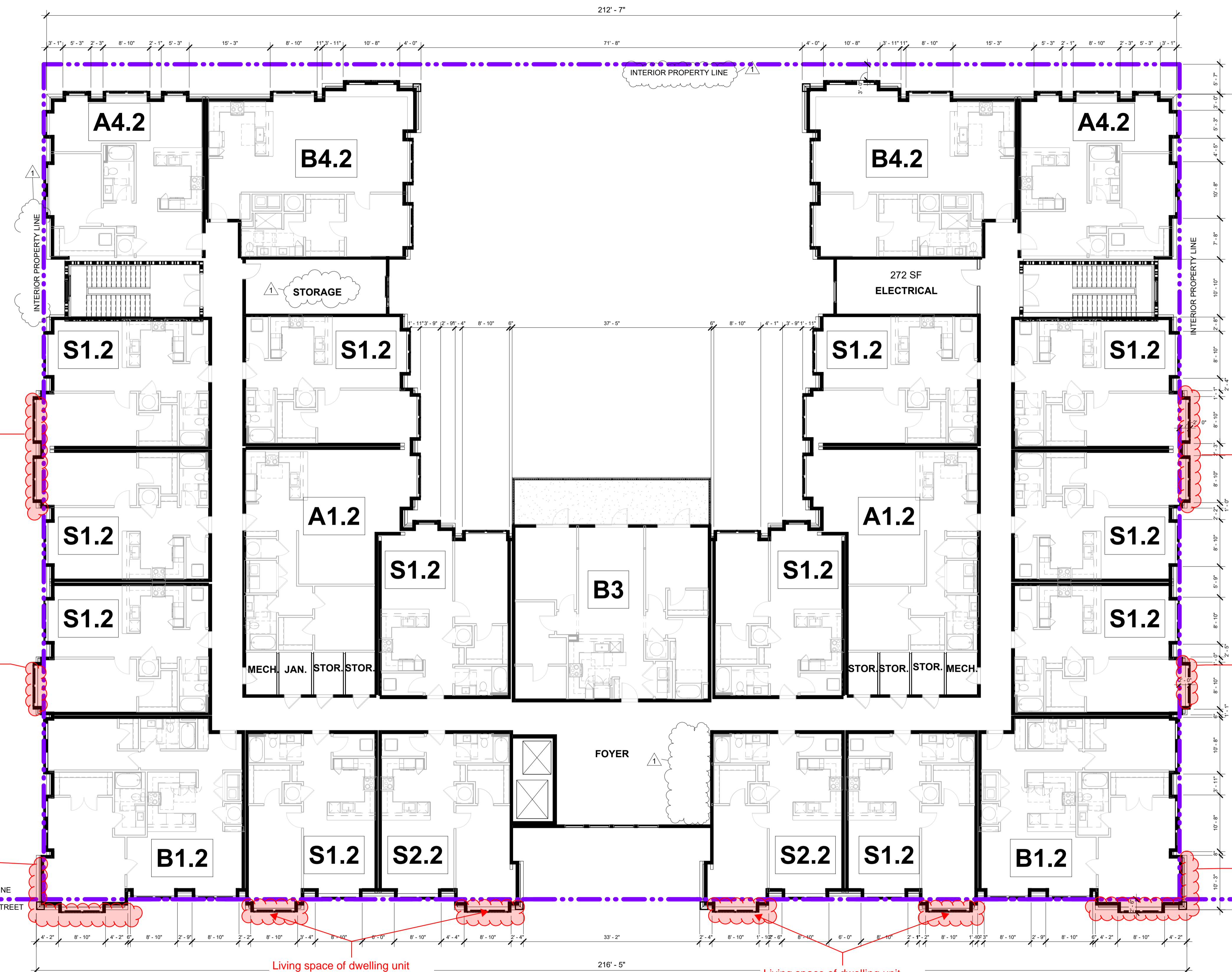
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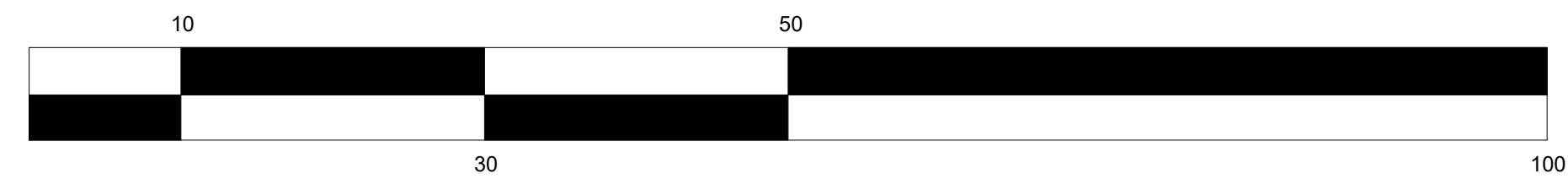
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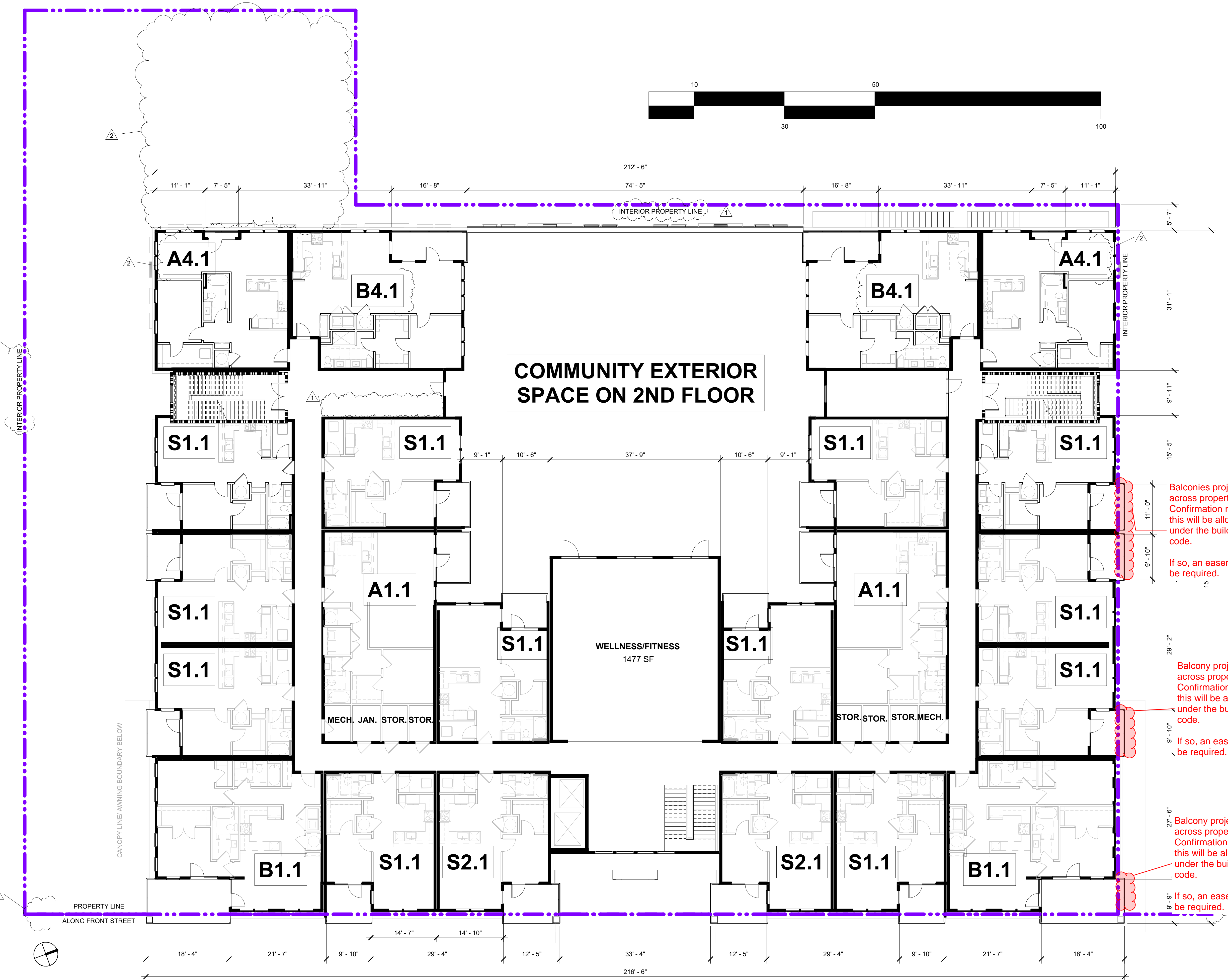
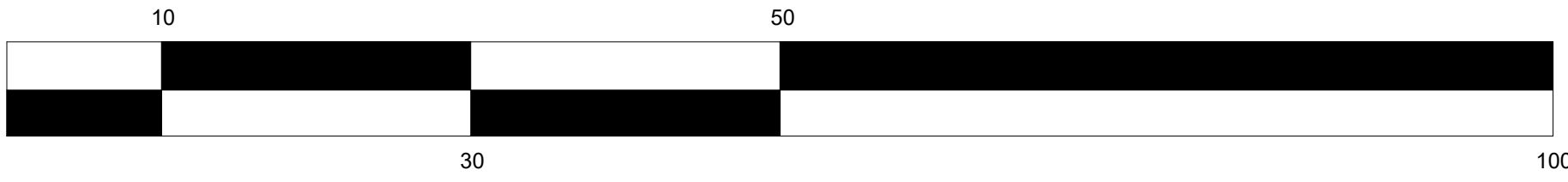
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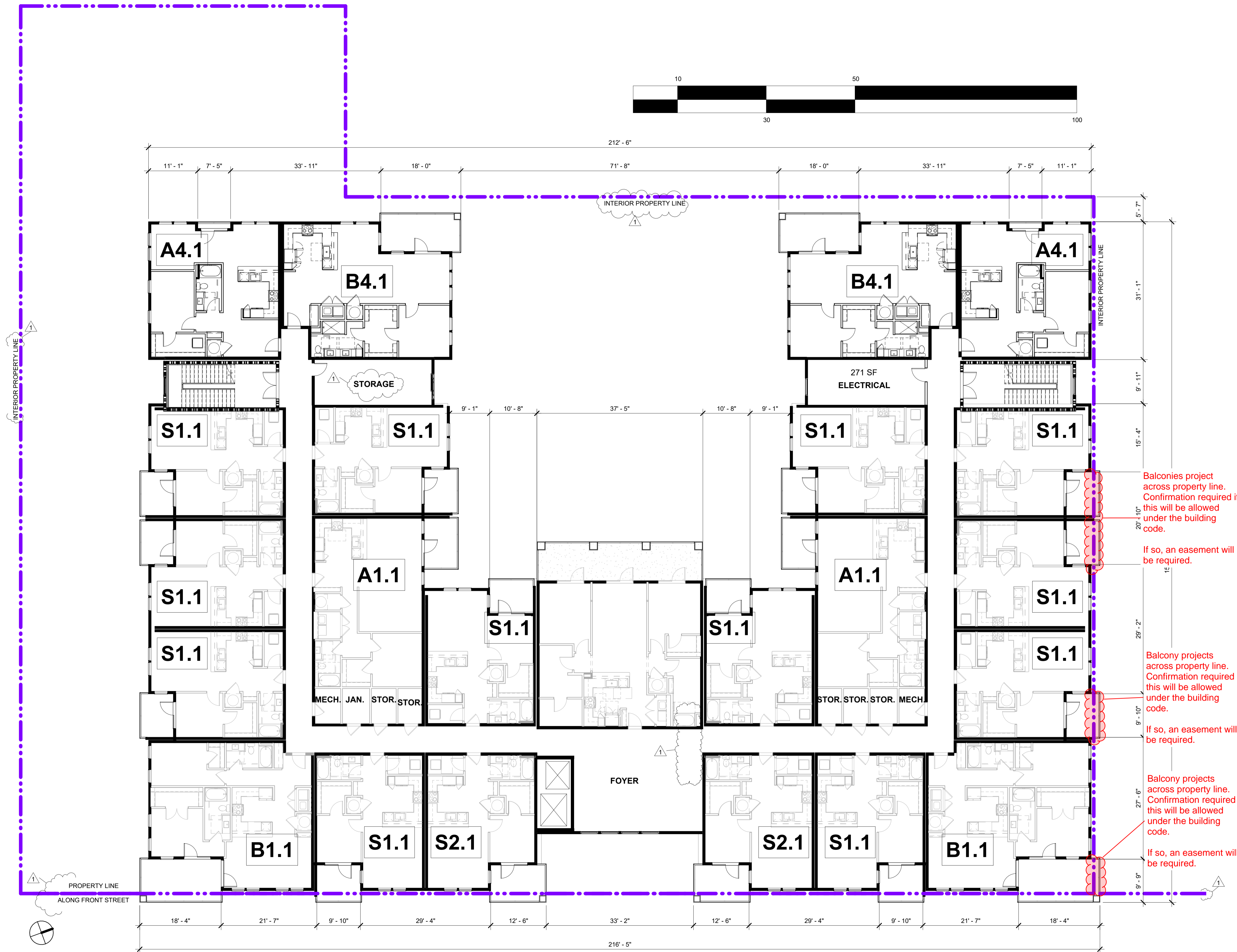
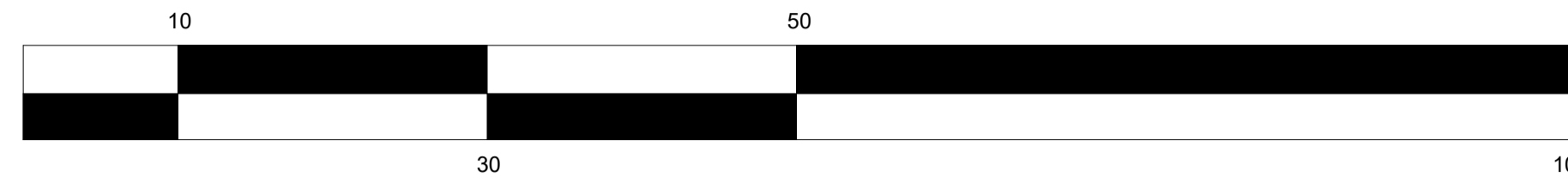
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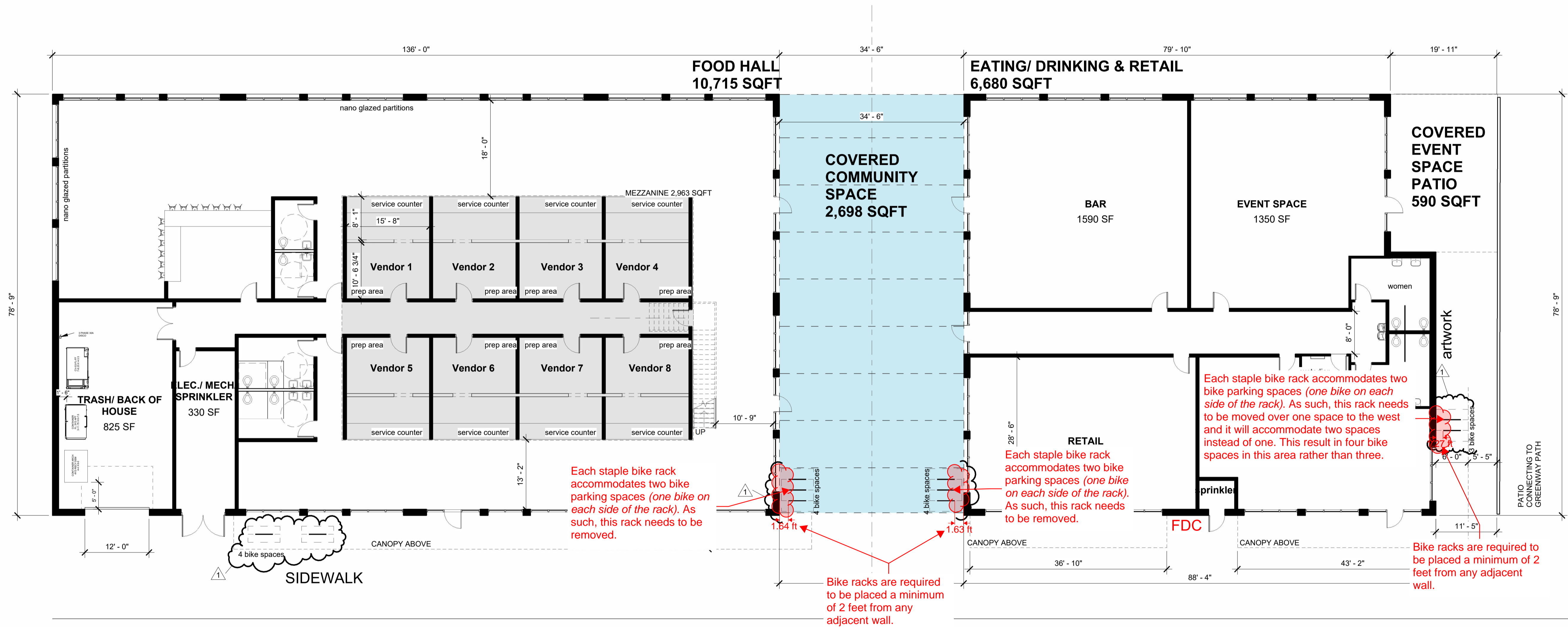
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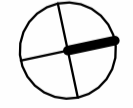
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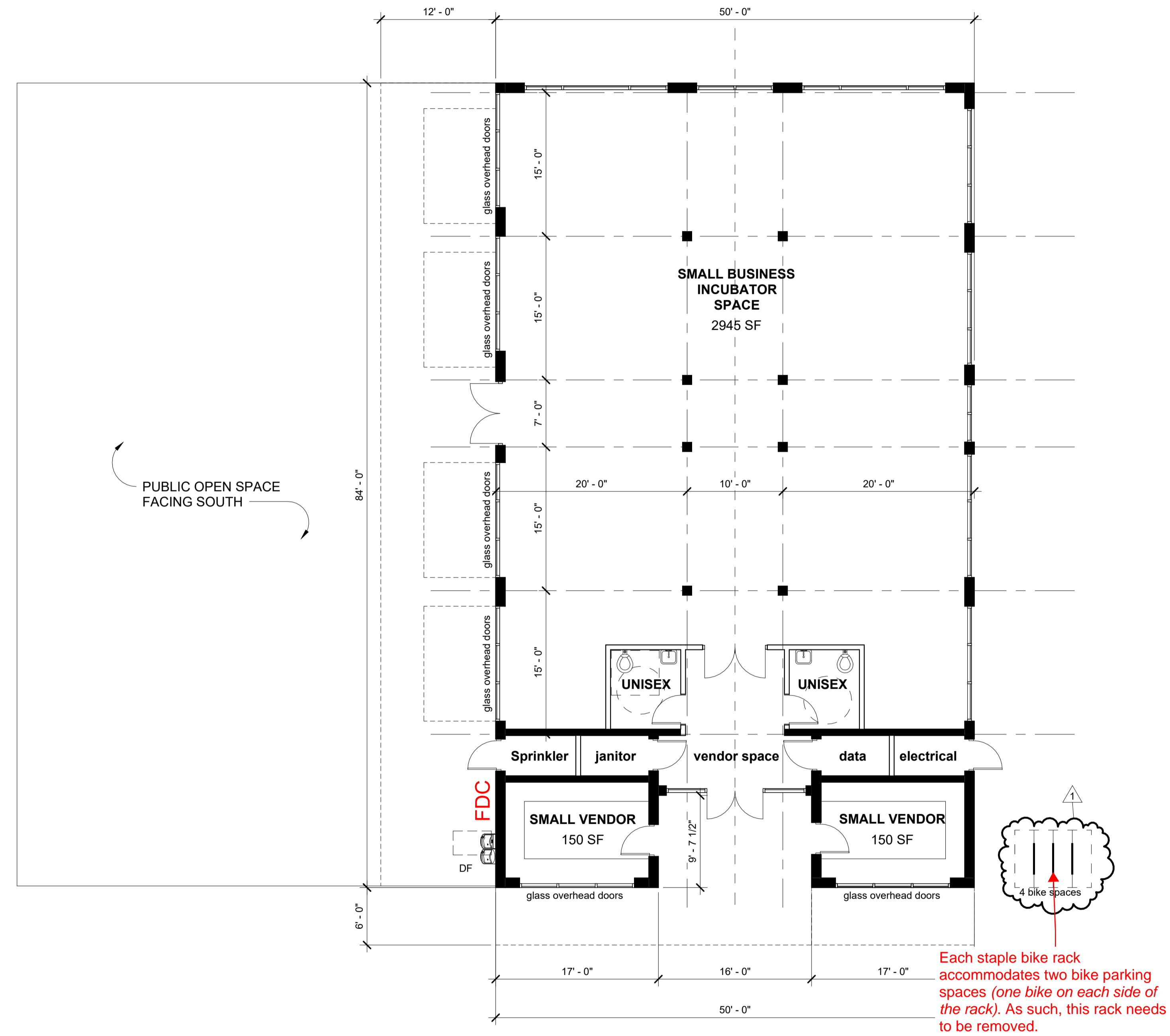
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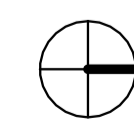
1 FOOD HALL
3/32" = 1'-0"



#	REVISION	DATE
1	Revision 1	05/07/2024



1 1st FLOOR PLAN
 1/8" = 1'-0"



TOTAL BUILDING SQFT 4,200 sf
 COVERED AREA SOUTH 1,008 sf
 TOTAL 5,208 sf

REVISION
 1 Revision 1

DATE
 05/07/2024

Attachment B: Marion County Subdivision Name Approval

**MARION COUNTY
SUBDIVISION/CONDOMINIUM NAME REQUEST**

Marion County Surveyor – 5155 Silverton Road NE, Salem, OR 97305
Fax: 503-588-7970 Phone: 503-588-5155 Email: MarionCountySurveyor@co.marion.or.us

Proposed Subdivision Name*: Cannery

(Please do not use the word "Subdivision" in the name.)

Proposed Condominium Name*:

(Must include either the word Condominium, Condominiums, or A Condominium)

***Subject to consent by prior party if name was previously used in a recorded plat, per ORS 92.090(1).**

NOTE: Reserved names expire 2 years from original approval date.

Renewal? Yes No

Owner Name: FRONT STREET PROPERTIES LLC

Address: PO BOX 2067, SALEM, OR, 97308

Email: jordan@firstcommercialoregon.com

Phone: 503-364-7400 Date: 05/28/2024

Location: Is the subdivision in a city? Yes No

City Name: Salem

Tax Map and Taxlot Number: 07 3W 22AB, Tax Lots 300 & 900

Office Use Only

Date Received: 5/28/24

[Signature] Approved as Submitted **(approval expires in 2 years)**

Not Approved for the following reason(s):

[Signature] Date 7/25/2024
Marion County Surveyor

**MARION COUNTY
SUBDIVISION/CONDOMINIUM NAME REQUEST**

Marion County Surveyor – 5155 Silverton Road NE, Salem, OR 97305
Fax: 503-588-7970 Phone: 503-588-5155 Email: MarionCountySurveyor@co.marion.or.us

Proposed Subdivision Name*: **Cannery**

(Please do not use the word "Subdivision" in the name.)

Proposed Condominium Name*:

(Must include either the word Condominium, Condominiums, or A Condominium)

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Email: jordan@firstcommercialoregon.com

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Tax Map and Taxlot Number: 07 3W 22AB, Tax Lot 600

Office Use Only

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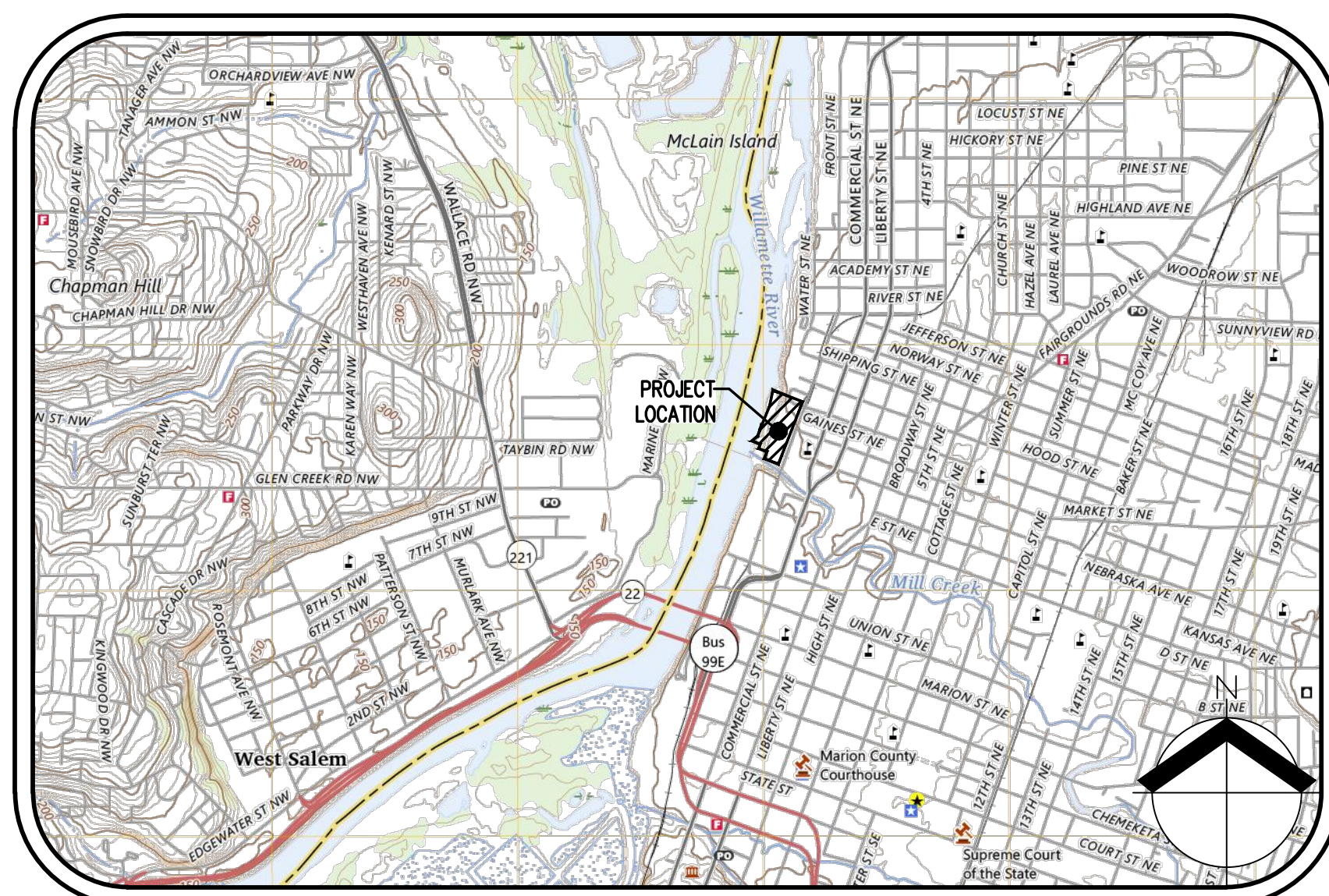
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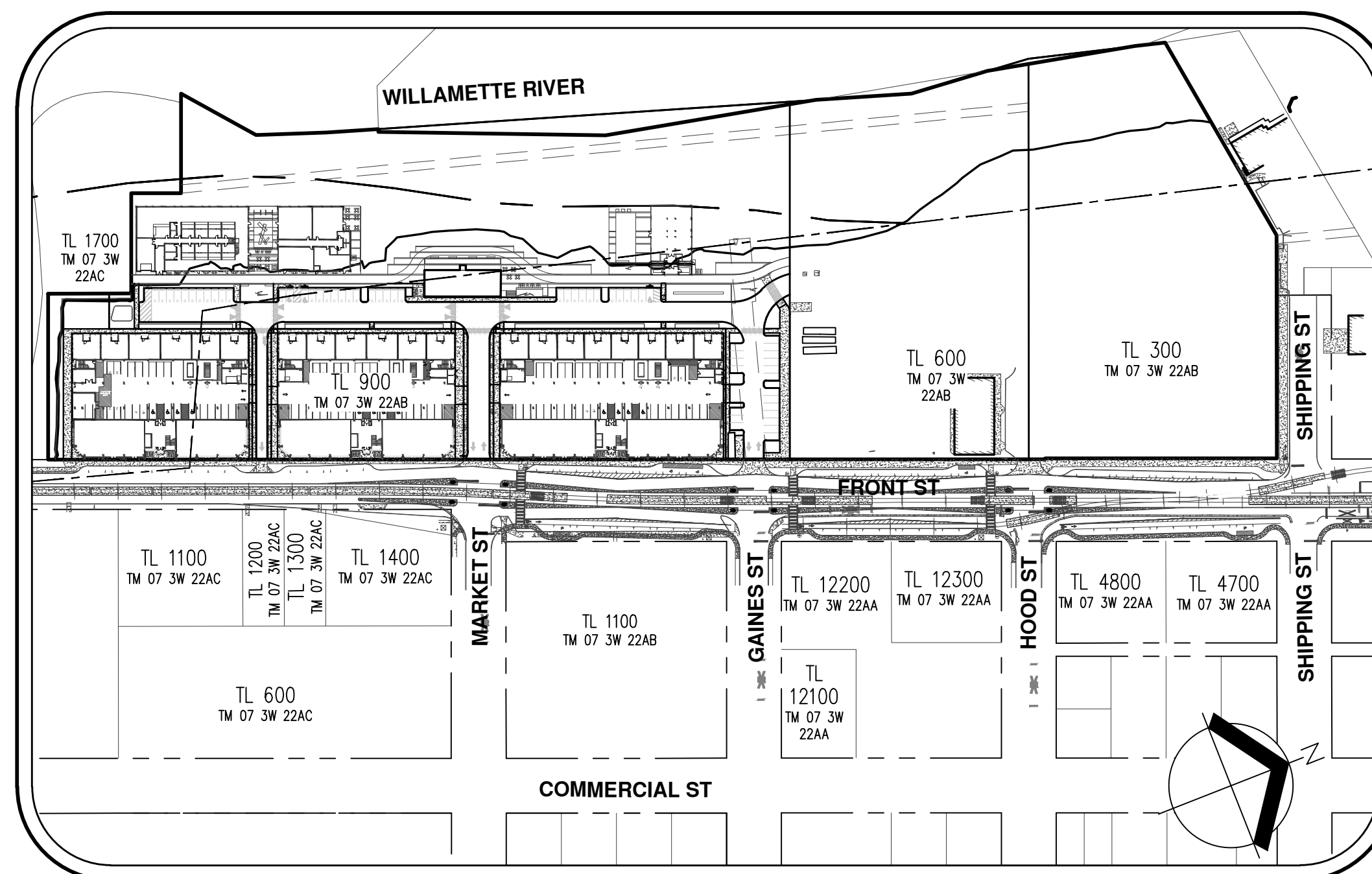
Attachment C: Revised Preliminary Land Use Plans

THE CANNERY

PRELIMINARY LAND USE PLANS



VICINITY MAP
NOT TO SCALE



SITE MAP
1" = 150'

**CIVIL ENGINEERING/
SURVEYING/LAND USE
PLANNING FIRM**

AKS ENGINEERING & FORESTRY, LLC
ENGINEERING CONTACT: TYLER ROTH, PE
PLANNING CONTACT: GRACE WOLFF
3700 RIVER RD N, STE 1
KEIZER, OR 97303
PH: 503.400.6028
WWW.AKS-ENG.COM

ARCHITECT

INSIGHT ARCHITECTS
CONTACT: KRISTINA HELD, AIA, LEED AP BD+C, CPHC
1307 WEST MOREHEAD ST, STE 108
CHARLOTTE, NC 28208
PH: 704.344.0445

LANDSCAPE ARCHITECT

LANGO HANSEN LANDSCAPE ARCHITECT
CONTACT: KYLE TRULEN, PLA, LEED AP
1100 NW GLISAN #3A,
PORTLAND, OR 97209
PH: 971.380.3580

**CONTRACT
PURCHASER/APPLICANT**

FUND
CONTACT: TRENT MICHELS
15017 THOMAS RD,
CHARLOTTE, NC 28278

GEOTECHNICAL FIRM

CENTRAL GEOTECHNICAL SERVICES, LLC
CONTACT: JULIO C. VELA, PHD, PE, GE
10240 SW NIMBUS AVE, STE L6
PORTLAND, OR 97223
PH: 503.994.0755

LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED
DECIDUOUS TREE	(Symbol)	STORM DRAIN CLEAN OUT	(Symbol)
CONIFEROUS TREE	(Symbol)	STORM DRAIN CATCH BASIN	(Symbol)
FIRE HYDRANT	(Symbol)	STORM DRAIN AREA DRAIN	(Symbol)
WATER BLOWOFF	(Symbol)	STORM DRAIN MANHOLE	(Symbol)
WATER METER	(Symbol)	GAS METER	(Symbol)
WATER VALVE	(Symbol)	GAS VALVE	(Symbol)
DOUBLE CHECK VALVE	(Symbol)	GUY WIRE ANCHOR	(Symbol)
AIR RELEASE VALVE	(Symbol)	UTILITY POLE	(Symbol)
SANITARY SEWER CLEAN OUT	(Symbol)	POWER VAULT	(Symbol)
SANITARY SEWER MANHOLE	(Symbol)	POWER JUNCTION BOX	(Symbol)
SIGN	(Symbol)	POWER PEDESTAL	(Symbol)
STREET LIGHT	(Symbol)	COMMUNICATIONS VAULT	(Symbol)
MAILBOX	(Symbol)	COMMUNICATIONS JUNCTION BOX	(Symbol)
		COMMUNICATIONS RISER	(Symbol)

	EXISTING	PROPOSED
RIGHT-OF-WAY LINE	(Symbol)	(Symbol)
BOUNDARY LINE	(Symbol)	(Symbol)
PROPERTY LINE	(Symbol)	(Symbol)
CENTERLINE	(Symbol)	(Symbol)
DITCH	(Symbol)	(Symbol)
CURB	(Symbol)	(Symbol)
EDGE OF PAVEMENT	(Symbol)	(Symbol)
EASEMENT	(Symbol)	(Symbol)
FENCE LINE	(Symbol)	(Symbol)
GRAVEL EDGE	(Symbol)	(Symbol)
POWER LINE	(Symbol)	(Symbol)
OVERHEAD WIRE	(Symbol)	(Symbol)
COMMUNICATIONS LINE	(Symbol)	(Symbol)
FIBER OPTIC LINE	(Symbol)	(Symbol)
GAS LINE	(Symbol)	(Symbol)
STORM DRAIN LINE	(Symbol)	(Symbol)
SANITARY SEWER LINE	(Symbol)	(Symbol)
WATER LINE	(Symbol)	(Symbol)
RECLAIMED WATER LINE	(Symbol)	(Symbol)

PROPERTY DESCRIPTION:

MARION COUNTY TAX MAP 07 3W 22AB,
TAX LOTS 300, 600, & 900
CITY OF SALEM, OREGON

VERTICAL DATUM

ELEVATIONS ARE BASED ON CITY OF SALEM
BENCHMARK NO. 1151, LOCATED AT THE SE
CORNER OF SUMMER AND MARION ST.
ELEVATION = 161.617 FEET (NGVD 29).

PROPERTY LOCATION:

1105 FRONT ST NE,
SALEM, OREGON 97301

SHEET INDEX

- P1 COVER SHEET
- C002 EXISTING CONDITIONS PLAN
- C003 EXISTING CONDITIONS PLAN
- P4 TENTATIVE PLAT
- P5 PRELIMINARY ONSITE DEMOLITION PLAN
- P6 PRELIMINARY TREE PRESERVATION AND REMOVAL PLAN
- P7 PRELIMINARY TREE TABLE
- P8 PRELIMINARY SITE PLAN
- P9 PRELIMINARY ONSITE GRADING AND DRAINAGE PLAN
- P10 PRELIMINARY ONSITE GRADING AND DRAINAGE PLAN
- P11 PRELIMINARY GRADING AND DRAINAGE SECTIONS
- P12 PRELIMINARY COMPOSITE UTILITY PLAN
- P13 PRELIMINARY FRONT ST IMPROVEMENTS
- P14 CONCEPTUAL LOT 5 AND 6 UTILITY PLAN

COVER SHEET
THE CANNERY
FUND
SALEM, OREGON



RENEWED: DECEMBER 31, 2024
JOB NUMBER: 5968-01
DATE: 07/26/2024
DESIGNED BY: TDR
DRAWN BY: MJM
CHECKED BY: TDR

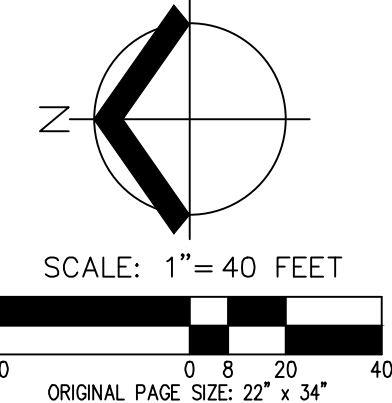
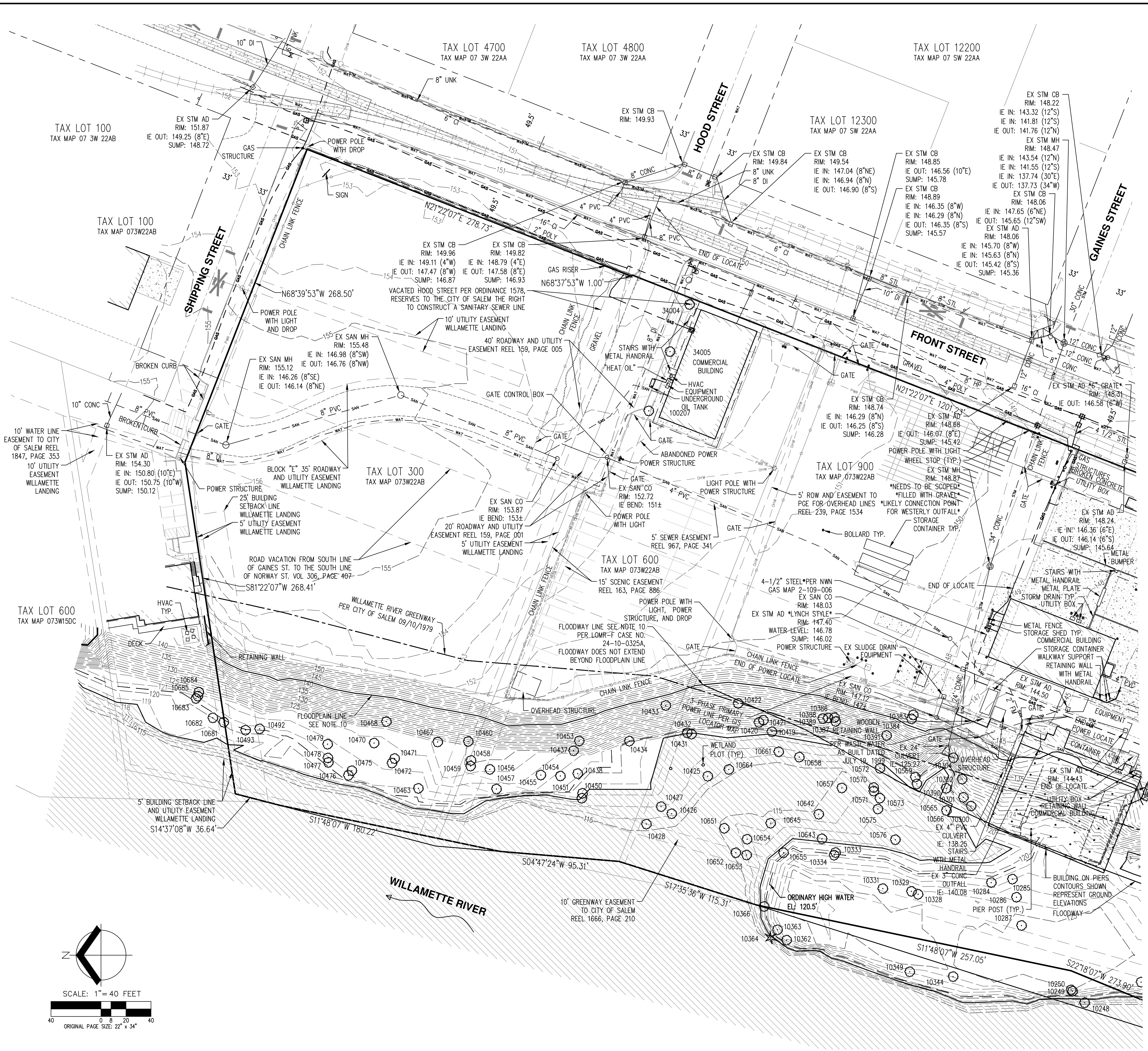
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CHARLOTTE, NC 28278

- NOTES:**
- UTILITIES SHOWN ARE BASED ON FIELD OBSERVATIONS AND LOCATE TICKET NUMBERS 23007190, 23007220, 23007221, 23007271, 23008701, 23008714, 23008722, 23008733, 23009893, 23009901, AND 23009993. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND LOCATES REPRESENT THE ONLY UTILITIES IN THE AREA. CONTRACTORS ARE RESPONSIBLE FOR VERIFYING ALL EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION.
 - FIELD WORK WAS CONDUCTED NOVEMBER 16 TO DECEMBER 12, 2017 AND JANUARY 18 TO JUNE 22, 2023.
 - VERTICAL DATUM: ELEVATIONS ARE BASED ON CITY OF SALEM BENCHMARK NO. 1151, LOCATED AT THE SE CORNER OF SUMMER AND MARION ST. ELEVATION = 161.617 FEET (NGVD 29).
 - HORIZONTAL DATUM: A LOCAL DATUM PLANE DERIVED FROM STATE PLANE OREGON NORTH 3601 NAD83(2011) EPOCH 2010.00 BY MULTIPLYING BY A PROJECT MEAN GRID COMBINED SCALE FACTOR OF 1.000107696 AT A CENTRAL PROJECT POINT WITH INTERNATIONAL FEET STATE PLANE GRID COORDINATES ANGLE OF -147.501515 E: 7545303.43662 AND A MERIDIAN CONVERGENCE ANGLE OF -147.59". STATE PLANE COORDINATES WERE DERIVED FROM GPS OBSERVATIONS USING THE TRIMBLE VRS NOW NETWORK. DISTANCES SHOWN ARE INTERNATIONAL FEET GROUND VALUES.
 - THIS MAP DOES NOT CONSTITUTE A PROPERTY BOUNDARY SURVEY.
 - SURVEY IS ONLY VALID WITH SURVEYOR'S STAMP AND SIGNATURE.
 - BUILDING FOOTPRINTS ARE MEASURED TO SIDING UNLESS NOTED OTHERWISE. CONTACT SURVEYOR WITH QUESTIONS REGARDING BUILDING TIES.
 - CONTOUR INTERVAL IS 1 FOOT.
 - TREES WITH DIAMETER OF 10" AND GREATER ARE SHOWN. TREE DIAMETERS WERE DETERMINED BY VISUAL INSPECTION. TREE INFORMATION IS SUBJECT TO CHANGE UPON ARBORIST INSPECTION.
 - FLOODWAY IS SHOWN BY GRAPHICAL OVERLAY OF FEMA FIRMEETTE MAP 41047C0333H WITH AN EFFECTIVE DATE OF JANUARY 2, 2003. ZONE AE IS SHOWN BY MAPPING THE BASE FLOOD ELEVATION (BFE) OF 141.1 (NGVD 29). BFE IS PER FLOOD INSURANCE STUDY FOR THE WILLAMETTE RIVER, MARION COUNTY, WITH EFFECTIVE DATE OF OCTOBER 10, 2019. CASE NO. 24-10-0325A

CONTINUES ON SHEET C003

LEGEND

EXISTING		EXISTING	
DECIDUOUS TREE		STORM SEWER CLEAN OUT	
CONIFEROUS TREE		STORM SEWER CATCH BASIN	
FIRE HYDRANT		STORM SEWER AREA DRAIN	
WATER BLOWOFF		STORM SEWER MANHOLE	
WATER METER		GAS METER	
WATER VALVE		GAS VALVE	
DOUBLE CHECK VALVE		GUY WIRE ANCHOR	
AIR RELEASE VALVE		UTILITY POLE	
WATER MANHOLE		POWER VAULT	
SANITARY SEWER CLEAN OUT		POWER JUNCTION BOX	
SANITARY SEWER MANHOLE		POWER PEDESTAL	
SIGN		COMMUNICATIONS VAULT	
STREET LIGHT		COMMUNICATIONS JUNCTION BOX	
MAILBOX		COMMUNICATIONS RISER	
EXISTING			
RIGHT-OF-WAY LINE			
BOUNDARY LINE			
PROPERTY LINE			
CENTERLINE			
DITCH			
CURB			
EDGE OF PAVEMENT			
EASEMENT			
FENCE LINE			
GRAVEL EDGE			
POWER LINE			
OVERHEAD WIRE			
COMMUNICATIONS LINE			
FIBER OPTIC LINE			
GAS LINE			
STORM SEWER LINE			
SANITARY SEWER LINE			
WATER LINE			



AKS DRAWING FILE: 5968-01EXCOND.DWG | LAYOUT: C002

EXISTING CONDITIONS
PLAN

DESIGNED BY: ---
DRAWN BY: TJA/AC/NJS
MANAGED BY: DGH/AK/RB
CHECKED BY: BRH
DATE: 7/25/2024

REGISTERED
PROFESSIONAL
LAND SURVEYOR

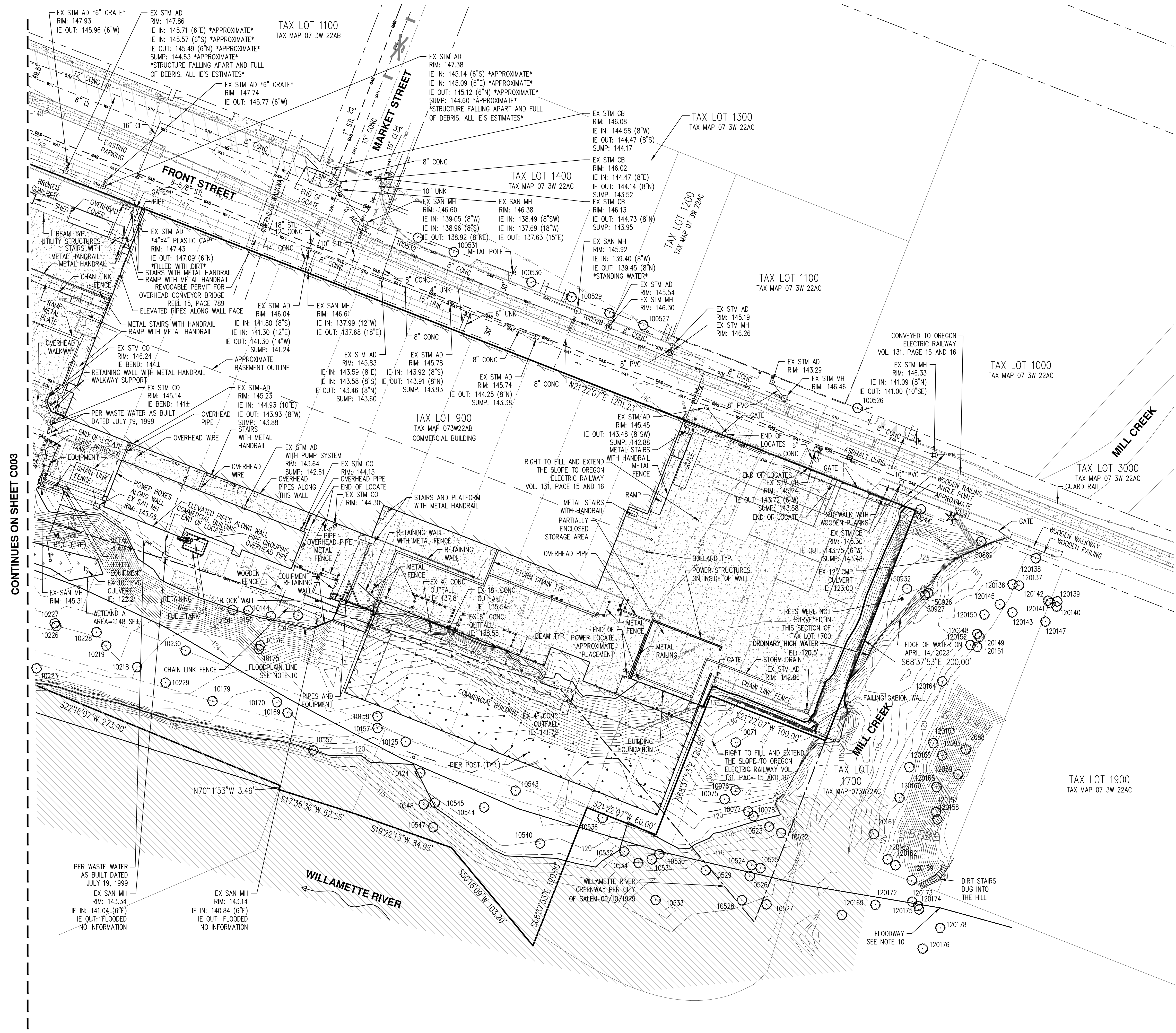
Benjamin R Huff
OREGON
MARCH 14, 2017
BENJAMIN R HUFF
84738PLS
RENEWS: 6/30/25

REVISIONS

JOB NUMBER
5968-01

SHEET

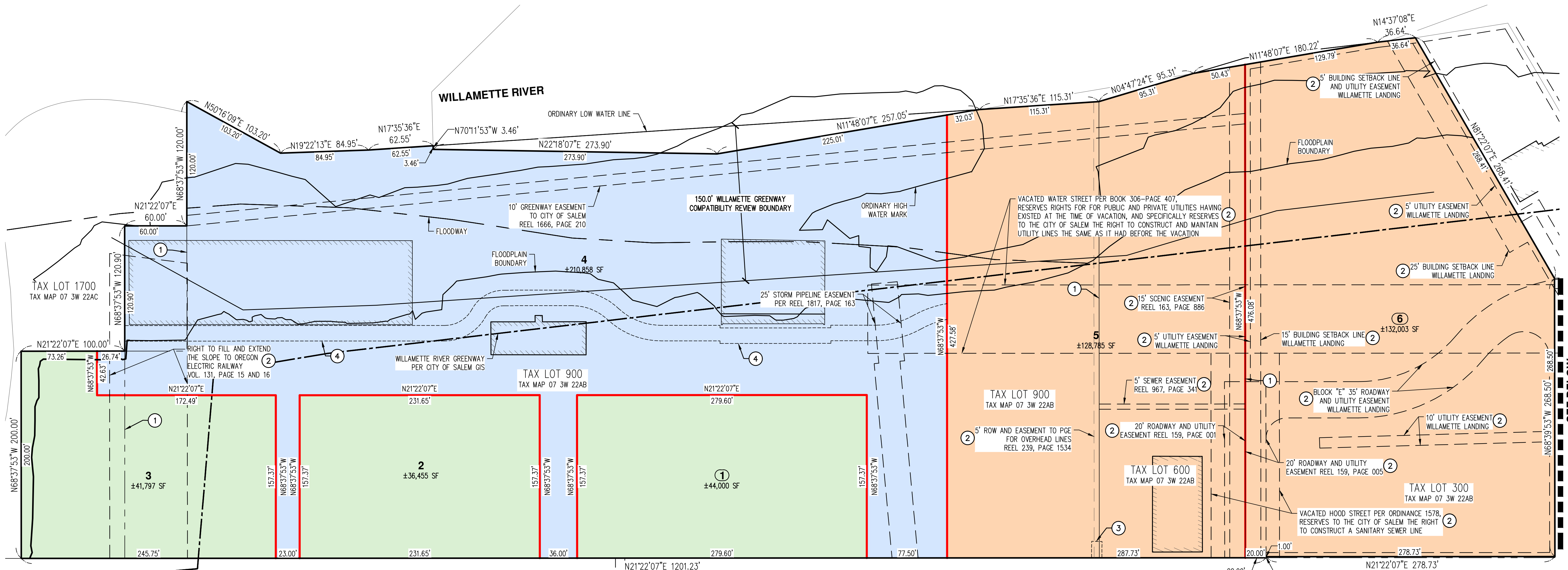
C003



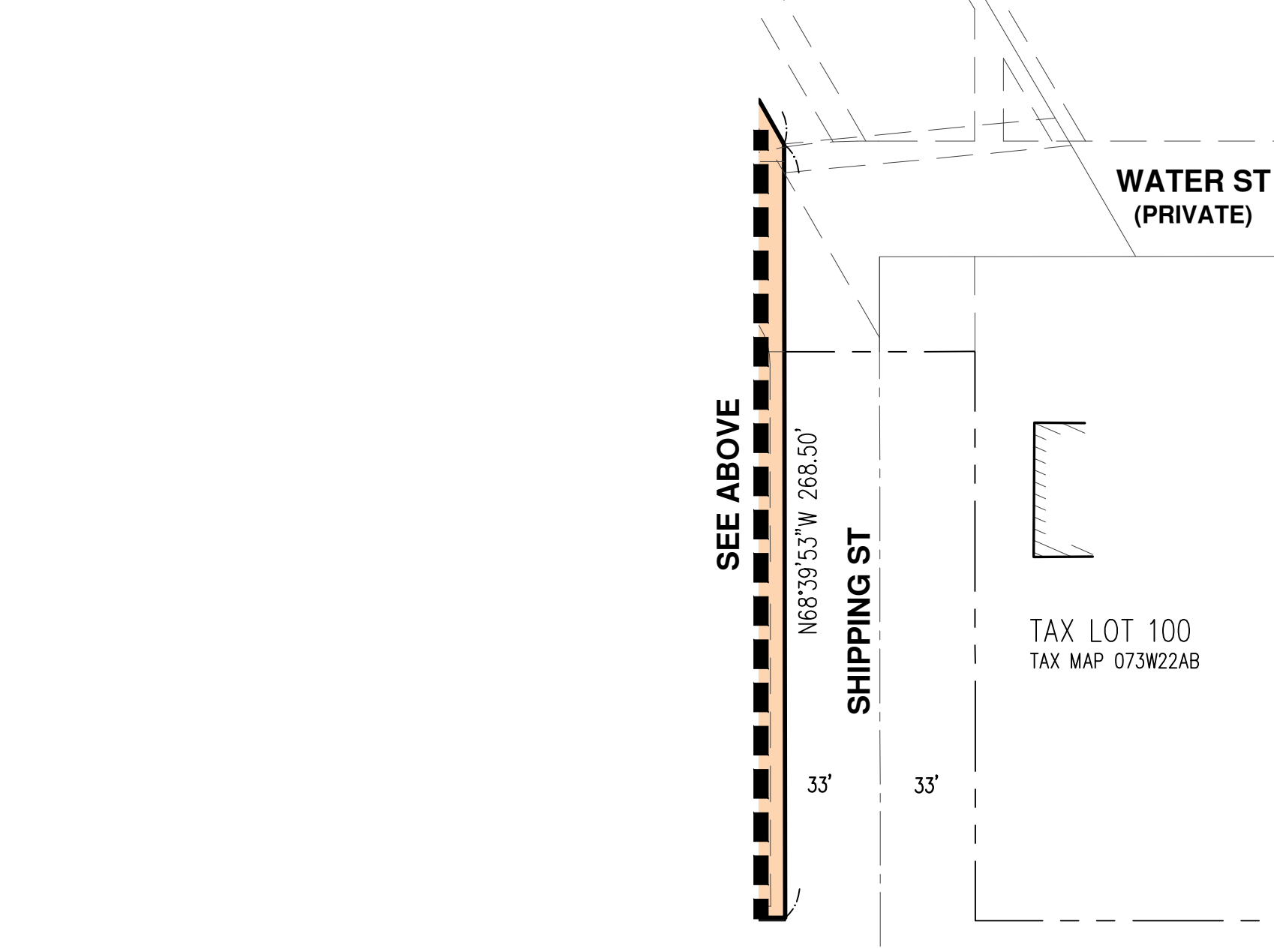
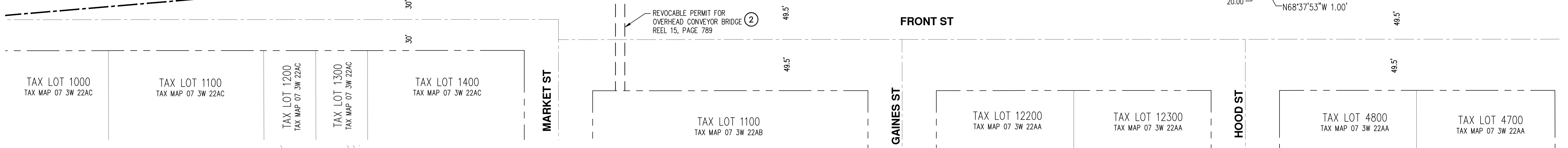
CONTINUES ON SHEET C003

AKS DRAWING FILE: 5968-01EXCOND.DWG | LAYOUT: C003

PROPERTY DESCRIPTION
 TAX MAP 07.3W.22AB
 TAX LOTS 300, 600, 900
 CONTRACT PURCHASER:
 FUND
 15017 THOMAS RD,
 CHARLOTTE, NC 28278



SEE BELOW



KEYED NOTES: #

- EXISTING TAX LOT LINE TO BE REMOVED.
- EXISTING EASEMENT OR ROW TO BE QUITCLAIMED/VACATED.
- PROPOSED EASEMENT FOR EXISTING POWER POLE BETWEEN HOOD AND GAINES STREETS, SEE PLAN VIEW.
- 15' WILLAMETTE RIVER GREENWAY PATH EASEMENT DEDICATION. FINAL CONFIGURATION TO BE DETERMINED AT TIME OF BUILDING PERMITS.

EASEMENT NOTES

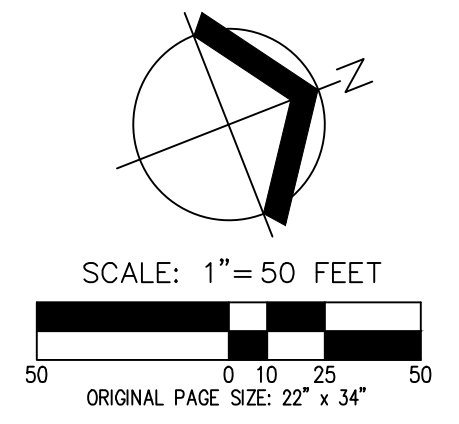
- CROSS ACCESS, DRAINAGE AND UTILITY EASEMENTS AMONG ALL PARCELS TO BE RECORDED PRIOR TO FINAL PLAT.
- ADDITIONAL EASEMENTS ARE REQUIRED FOR UTILITY IMPROVEMENTS. REFER TO UTILITY PLAN.

THE PURPOSE OF THIS PRELIMINARY PLAT IS TO SHOW PLANNED LOT DIMENSIONS AND AREAS FOR PLANNING PURPOSES. THIS IS NOT AN OFFICIAL PLAT AND IS NOT TO BE USED FOR SURVEY PURPOSES.

LEGEND

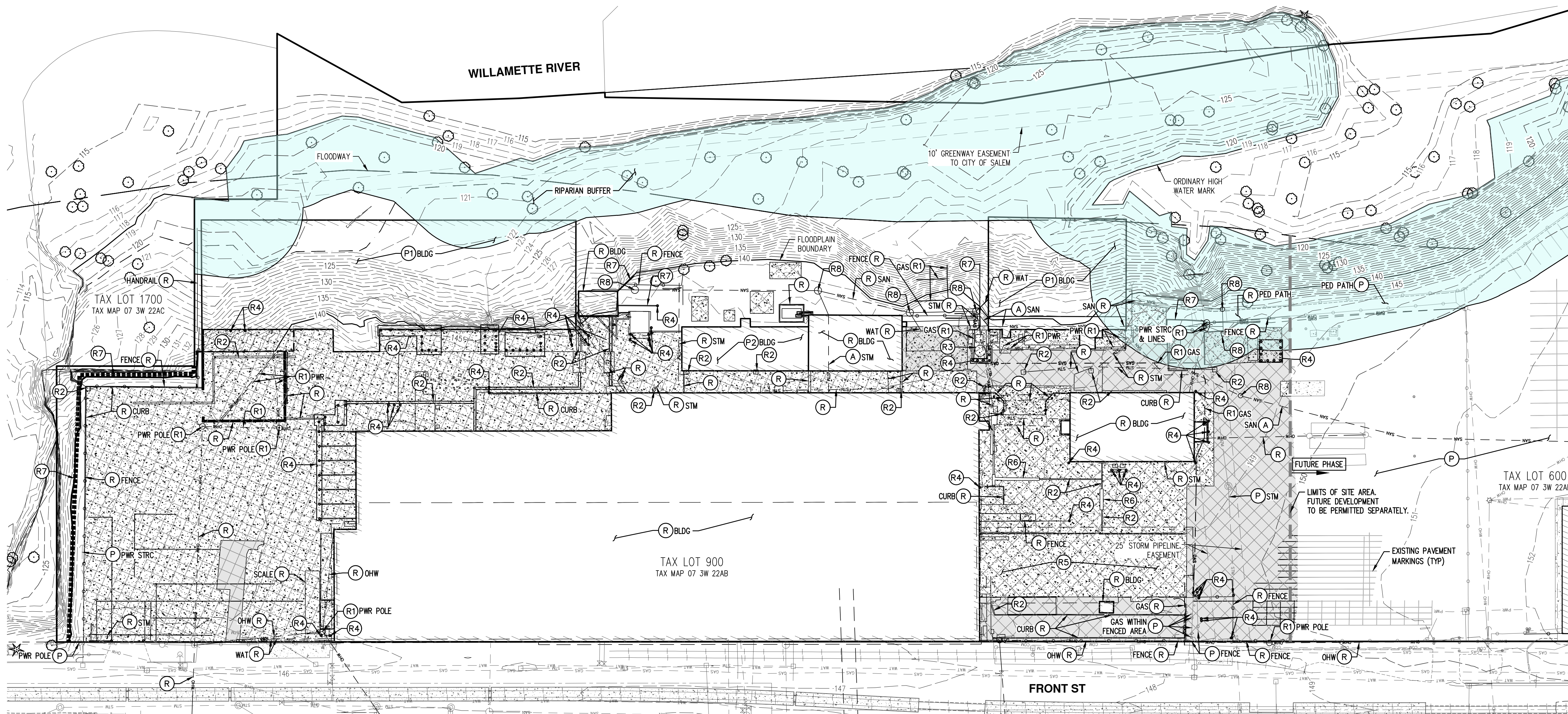
FUTURE DEVELOPMENT PARCEL	
MIXED-USE BUILDING PARCEL	
ADAPTIVE REUSE BUILDINGS PARCEL (COMMERCIAL)	

NOTE: THIS SHEET IS PRINTED WITH COLOR



**TENTATIVE PLAT
 THE CANNERY
 FUND
 SALEM, OREGON**

REGISTERED PROFESSIONAL ENGINEER
 COVER D. ROTH
 RENEWS: DECEMBER 31, 2024
 JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: MJM
 CHECKED BY: TDR

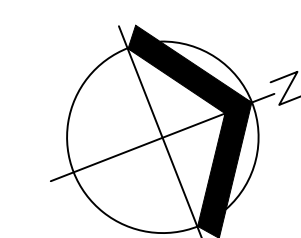


DEMOLITION KEYED NOTES: (TR)

- A PLUG, ABANDON, AND/OR REMOVE EXISTING UTILITY LINES PER CITY AND/OR UTILITY COMPANY STANDARDS.
- P PROTECT AT ALL TIMES DURING CONSTRUCTION. ADJUST TO NEW FINISHED GRADE AS REQUIRED. ANY DAMAGE SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE.
- P1 REMOVE EXISTING WALLS AND PROTECT THE PIER AND PLATFORM. REFER TO BUILDING PLANS FOR BUILDING AND STRUCTURAL IMPROVEMENTS.
- P2 REFER TO BUILDING PLANS FOR BUILDING IMPROVEMENTS.
- R CONTRACTOR TO REMOVE AND HAUL OFF SITE FOR DISPOSAL.
- R1 COORDINATE WITH FRANCHISE UTILITY PROVIDER FOR RELOCATION/REMOVAL. IF NO RELOCATION IS REQUIRED CONTRACTOR SHALL PROTECT AT ALL TIMES DURING CONSTRUCTION.
- R2 REMOVE EXISTING STORM STRUCTURE AND HAUL OFF SITE FOR DISPOSAL.
- R3 REMOVE EXISTING NITROGEN TANK. REMOVE AND DECOMMISSION IN ACCORDANCE WITH APPLICABLE STATE REGULATIONS.
- R4 REMOVE EXISTING BOLLARDS AND ATTACHED STRUCTURES.
- R5 REMOVE EXISTING COVERED AREA. METAL TRUSSES TO BE SAVED FOR FUTURE REUSE.
- R6 REMOVING EXISTING METAL PLATE.
- R7 EXISTING RETAINING WALL. STRUCTURAL IMPROVEMENTS REQUIRED TO BE DETERMINED AT TIME OF BUILDING PERMIT.
- R8 REMOVE EXISTING SANITARY STRUCTURE AND PUMP STATION. HAUL OFF SITE FOR DISPOSAL IN ACCORDANCE WITH STATE REGULATIONS.

LEGEND

EXISTING GROUND CONTOUR (1 FT)	---	149
EXISTING GROUND CONTOUR (5 FT)	---	150
EXISTING TREE TO REMAIN		
EXISTING PAVEMENT/CONCRETE TO BE REMOVED		



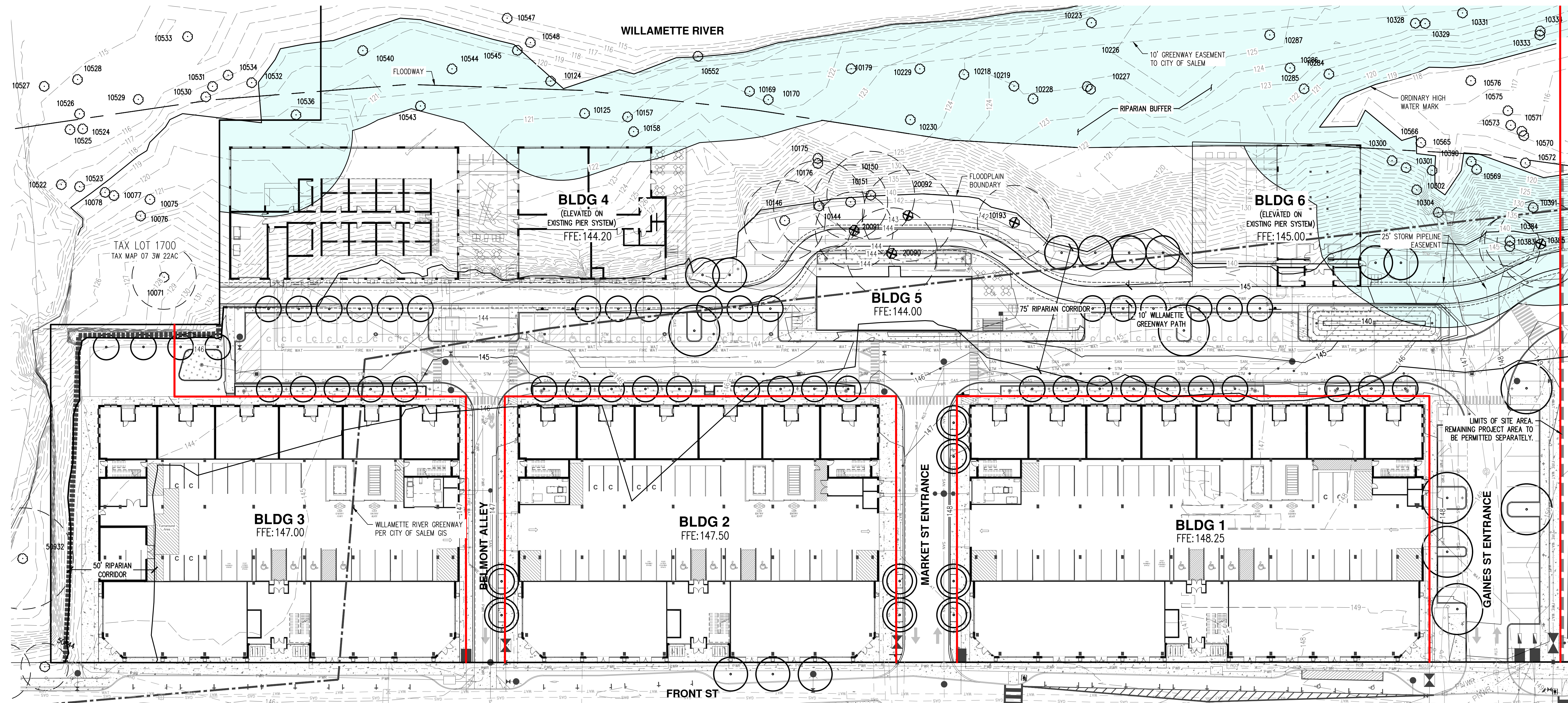
SCALE: 1" = 40 FEET
 ORIGINAL PAGE SIZE: 22" x 34"

PRELIMINARY ONSITE DEMOLITION PLAN
THE CANNERY
FUND
SALEM, OREGON



RENEWAL DATE: DECEMBER 31, 2024
 JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: M.M.
 CHECKED BY: TDR

AKS DRAWING FILE: 5968-01 PRELIM TREE PLANNING LAYOUT: P6



TREE SUMMARY:

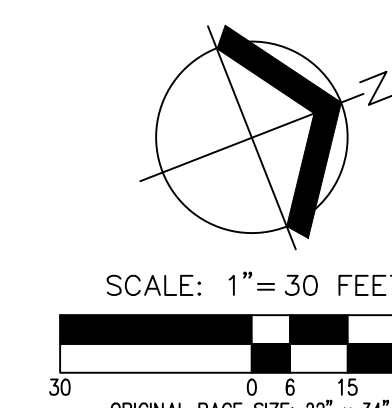
TREES REMOVED FOR GREENWAY PATH = 5

GENERAL NOTES:

1. CRITICAL ROOT ZONES SHOWN ARE FOR ANTICIPATED TREE IMPACTS ONLY.
2. TREES BELOW TOP OF BANK ARE NOT ANTICIPATED TO BE IMPACTED.
3. REFER TO ARBORIST LETTER FOR TREE SPECIES AND MORE INFORMATION REGARDING TREE REMOVAL.
4. NO SIGNIFICANT TREES PER CITY OF SALEM REQUIREMENTS ARE PROPOSED TO BE REMOVED.

LEGEND

EXISTING GROUND CONTOUR (1 FT)	---	149
EXISTING GROUND CONTOUR (5 FT)	---	150
FINISHED GRADE CONTOUR (1 FT)	---	149
FINISHED GRADE CONTOUR (5 FT)	---	150
INTERIOR PROPERTY LINE	---	---
EXISTING TREE TO REMAIN	⊗	
EXISTING TREE TO BE REMOVED	⊗	
CRITICAL TREE ROOT ZONE 1" DBH = 1'-0" RADIUS	○	
PLANNED TREE (REFER TO LANDSCAPE PLANS BY OTHERS)	○	





AKS ENGINEERING & FORESTRY, LLC
 3700 RIVER RD. N. STE. 1
 KEIZER, OR 97103
 503.400.6028
 WWW.AKS-ENG.COM

ENGINEERING · SURVEYING · NATURAL RESOURCES
 FORESTRY · PLANNING · LANDSCAPE ARCHITECTURE

PROPERTY DESCRIPTION
 TAX MAP 07.W2248
 TAX LOTS 300, 600, 900
 CONTRACT PURCHASER:
 FUND
 15017 THOMAS RD.
 CHARLOTTE, NC 28278

PRELIMINARY TREE TABLE
THE CANNERY
FUND
SALEM, OREGON

PRELIMINARY
NOT FOR
CONSTRUCTION

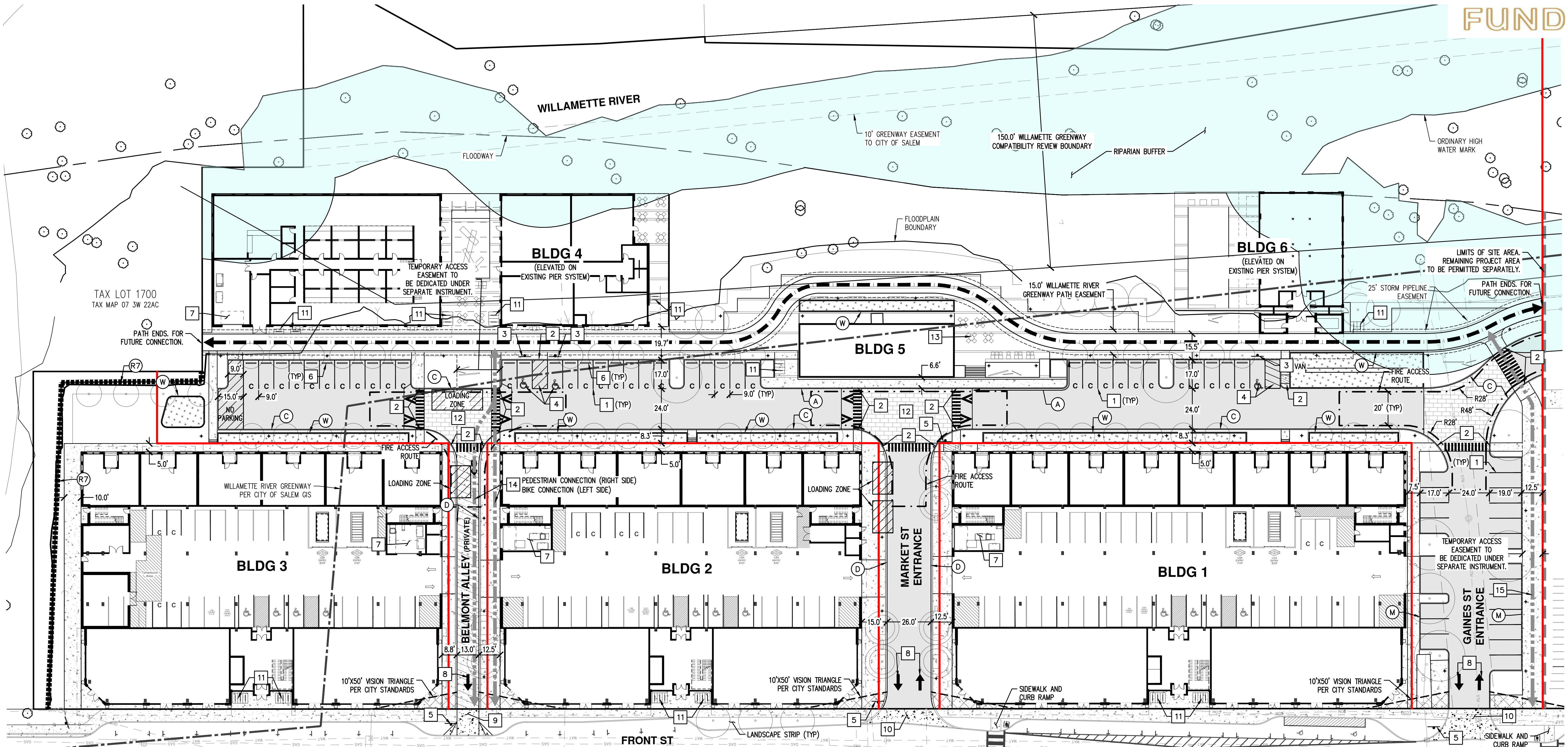
JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: M.M
 CHECKED BY: TDR

TREE TABLE			
TREE NUMBER	TYPE	DBH (IN.)	PRESERVE/REMOVE
10071	DECIDUOUS	19 18	PRESERVE
10075	DECIDUOUS	12	PRESERVE
10076	DECIDUOUS	24	PRESERVE
10077	DECIDUOUS	30	PRESERVE
10078	DECIDUOUS	34	PRESERVE
10124	DECIDUOUS	13	PRESERVE
10125	DECIDUOUS	16	PRESERVE
10144	DECIDUOUS	27	PRESERVE
10146	DECIDUOUS	24	PRESERVE
10150	DECIDUOUS	22	PRESERVE
10151	DECIDUOUS	26	PRESERVE
10157	DECIDUOUS	15 11	PRESERVE
10158	DECIDUOUS	59	PRESERVE
10169	DECIDUOUS	15	PRESERVE
10170	DECIDUOUS	47	PRESERVE
10175	DECIDUOUS	12	PRESERVE
10176	DECIDUOUS	13 11	PRESERVE
10179	DECIDUOUS	12	PRESERVE
*10193	DECIDUOUS	22	REMOVE
10218	DECIDUOUS	12	PRESERVE
10219	DECIDUOUS	41	PRESERVE
10223	DECIDUOUS	11 10	PRESERVE
10226	DECIDUOUS	15	PRESERVE
10227	DECIDUOUS	12	PRESERVE
10228	DECIDUOUS	12 10	PRESERVE
10229	DECIDUOUS	14	PRESERVE
10230	DECIDUOUS	11	PRESERVE
10284	DECIDUOUS	39	PRESERVE
10285	DECIDUOUS	17	PRESERVE
10286	DECIDUOUS	10	PRESERVE
10287	DECIDUOUS	13	PRESERVE
10300	DECIDUOUS	52	PRESERVE
10301	DECIDUOUS	54	PRESERVE
10302	DECIDUOUS	19	PRESERVE
10304	DECIDUOUS	20 18	PRESERVE
10328	DECIDUOUS	16 12	PRESERVE
10329	DECIDUOUS	15	PRESERVE
10331	DECIDUOUS	42	PRESERVE
10333	DECIDUOUS	43	PRESERVE
10334	DECIDUOUS	10	PRESERVE
10383	DECIDUOUS	18 17	PRESERVE

TREE TABLE			
TREE NUMBER	TYPE	DBH (IN.)	PRESERVE/REMOVE
10384	DECIDUOUS	15	PRESERVE
*10385	DECIDUOUS	26 21	REMOVE
10390	DECIDUOUS	UNK	PRESERVE
10391	DECIDUOUS	13	PRESERVE
10522	DECIDUOUS	28 23 16	PRESERVE
10523	DECIDUOUS	34	PRESERVE
10524	DECIDUOUS	32	PRESERVE
10525	DECIDUOUS	36	PRESERVE
10526	DECIDUOUS	44	PRESERVE
10527	DECIDUOUS	15 14	PRESERVE
10528	DECIDUOUS	11	PRESERVE
10529	DECIDUOUS	40	PRESERVE
10530	DECIDUOUS	17	PRESERVE
10531	DECIDUOUS	42	PRESERVE
10532	DECIDUOUS	41	PRESERVE
10533	DECIDUOUS	12	PRESERVE
10534	DECIDUOUS	10	PRESERVE
10536	DECIDUOUS	44	PRESERVE
10540	DECIDUOUS	14 11	PRESERVE
10543	DECIDUOUS	40	PRESERVE
10544	DECIDUOUS	28 22 21 19 16 11	PRESERVE
10545	DECIDUOUS	11	PRESERVE
10547	DECIDUOUS	13 10	PRESERVE
10548	DECIDUOUS	11 11	PRESERVE
10552	DECIDUOUS	16	PRESERVE
10565	DECIDUOUS	19 14 14 13 12 12	PRESERVE
10566	DECIDUOUS	52	PRESERVE
10569	DECIDUOUS	73	PRESERVE
10570	DECIDUOUS	17	PRESERVE
10571	DECIDUOUS	17	PRESERVE
10572	DECIDUOUS	10	PRESERVE
10573	DECIDUOUS	15	PRESERVE
10576	DECIDUOUS	11	PRESERVE
*20090	DECIDUOUS	27	REMOVE
*20091	DECIDUOUS	23	REMOVE
*20092	DECIDUOUS	27	REMOVE
50844	DECIDUOUS	8 12 14	PRESERVE
50932	DECIDUOUS	30	PRESERVE

NOTE:

- * REMOVAL NECESSARY TO ACCOMMODATE PUBLIC WILLAMETTE RIVER GREENWAY TRAIL, EXEMPT PER SRC 808.030(a)(2)(G).



CURB KEYED NOTES: (TR)

- (A) TYPE 'A' CURB AND GUTTER
- (C) TYPE 'C' CURB
- (D) TYPE 'D' MOUNTABLE CURB
- (M) MONOLITHIC CURB AND SIDEWALK
- (W) PLANTER WALL
- (R7) EXISTING RETAINING WALL. STRUCTURAL IMPROVEMENTS REQUIRED TO BE DETERMINED AT TIME OF BUILDING PERMIT

SITE KEYED NOTES: #

1. PAINT 4-INCH WIDE WHITE STRIPE PER CITY STANDARDS.
2. ACCESSIBLE CURB RAMP AND DETECTABLE WARNING SURFACE.
3. ACCESSIBLE PARKING SIGN. "VAN" INDICATES VAN ACCESSIBLE SIGN.
4. ACCESSIBLE PARKING STALLS AND AISLE STRIPING.
5. INSTALL 30"x30" STOP SIGN AND STOP BAR. (36"x36" WHEN ENTERING PUBLIC ROW)
6. CONCRETE WHEEL STOP.
7. TRASH ENCLOSURE. SEE ARCHITECTURAL PLANS FOR DETAILS.
8. DIRECTIONAL ARROW STRIPE.
9. COMMERCIAL DRIVEWAY APPROACH PER CITY DETAIL NO.302.
10. COMMERCIAL DRIVEWAY APPROACH PER CITY DETAIL NO.315.
11. BIKE RACK. SEE ARCHITECTURAL PLANS FOR DETAILS.
12. RAISED SPEED TABLE PEDESTRIAN CROSSING.
13. OUTDOOR SEATING. SEE LANDSCAPE PLANS FOR DETAILS.
14. SHARED ACCESS FROM WILLAMETTE GREENWAY PATH TO FRONT STREET. EASEMENT TO BE QUITCLAIMED UPON CONSTRUCTION OF PATH TO THE SOUTH.
15. SHARED ACCESS FROM WILLAMETTE GREENWAY PATH TO FRONT STREET. EASEMENT TO BE QUITCLAIMED UPON CONSTRUCTION OF PHASE 2 IMPROVEMENTS.

SITE PLAN DATA:

ZONING = MU-R
SUBJECT PROPERTY AREA = ±593,899 SF (±13.6 ACRES)
SITE AREA = ±333,110 SF (±7.6 ACRES)

DENSITY:
MULTI-FAMILY = 382 UNITS
*RETAIL = 12,160 SF
*OFFICE = 5,885 SF
*EATING/DRINKING ESTABLISHMENT = 30,875 SF

*DISTRIBUTION OF RETAIL, OFFICE, AND EATING/DRINKING ESTABLISHMENTS SQUARE FOOTAGE ARE SUBJECT TO CHANGE.

PARKING SUMMARY:
MAXIMUM VEHICLE PARKING:
MULTI-FAMILY = 669 SPACES (1.75/UNIT)
RETAIL = 61 SPACES (1/200 SF)
OFFICE = 24 SPACES (1/250 SF)
EATING/DRINKING ESTABLISHMENT = 176 SPACES (1/175 SF)
MAX TOTAL = 930 SPACES

VEHICLE PARKING PROVIDED:
GARAGE PARKING
• AUTOMATED = 291 SPACES
• STANDARD = 51 SPACES
• COMPACT = 10 SPACES
• ACCESSIBLE = 12 SPACES
OFF-STREET PARKING
• STANDARD = 20 SPACES
• COMPACT = 35 SPACES
• ACCESSIBLE = 3 SPACES
TOTAL PARKING = 422 SPACES

BICYCLE PARKING REQUIRED:
MULTI-FAMILY = 382 SPACES (1/UNIT)
RETAIL = 4 SPACES (GREATER OF 4 OR 1/10,000 SF)
OFFICE = 4 SPACES (GREATER OF 4 OR 1/3,500 SF)
EATING/DRINKING ESTABLISHMENT = 31 SPACES (GREATER OF 4 OR 1/1,000 SF)
TOTAL REQUIRED = 421 SPACES

BICYCLE PARKING PROVIDED:
SHORT-TERM = 59 SPACES
LONG-TERM = 380 SPACES
TOTAL = 439 SPACES

LOADING ZONE REQUIRED/PROVIDED:
MULTI-FAMILY REQUIRED = 3 SPACES (12'WX19'L)
RETAIL SALES AND SERVICES REQUIRED = 1 SPACE (12'WX30'L)
OFFICE REQUIRED = 1 SPACE (OFF-STREET PARKING AREA USED FOR LOADING PER SRC 806.075(a))
TOTAL REQUIRED = 4 SPACES
TOTAL PROVIDED = 3 SPACES (12'WX19'L)
1 SPACE (12'WX30'L)

NOTE: SPACES TO BE SCHEDULED AND CONED OFF WITH SITE OPERATOR FOR LOADING AND UNLOADING.

SETBACKS:

ALONG FRONT ST BUILDINGS = 0 FT OR MAX 10 FT (IF SETBACK AREA IS USED FOR PEDESTRIAN AMENITIES)

VEHICLE USE AREA = 10 FT

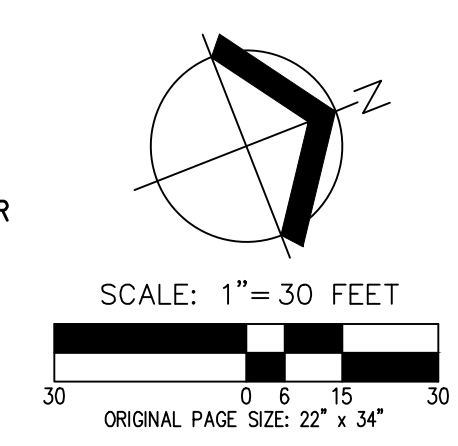
SIDE/REAR YARD BUILDINGS = NONE
VEHICLE USE AREA = 5 FT (NOT REQUIRED ABUTTING AN ALLEY)

GENERAL NOTES:

1. BUILDINGS 1, 2, AND 3 ARE ON SEPARATE PROPERTIES. REFER TO SHEET P4 FOR THE PROPOSED PROPERTY LINES.
2. THE FRONT STREET NE IMPROVEMENTS SHOWN ARE PRELIMINARY AND BASED ON CONCEPTUAL DESIGN WORK PROVIDED BY THE CITY'S RETAINED RAIL ENGINEER. REFINED FRONT STREET NE IMPROVEMENTS ARE ANTICIPATED AND WILL BE CONSTRUCTED IN ACCORDANCE WITH FEEDBACK RECEIVED FROM THE FINAL RAIL DIAGNOSTIC AND COORDINATION WITH THE CITY.

EV READY NOTE:

40% OF PARKING STALLS ARE REQUIRED TO BE EV READY PER STATE REQUIREMENTS. FINAL EV READY STALL LOCATION AND CONDUIT PLACEMENT WILL BE COORDINATED WITH PROJECT ELECTRICIAN AT THE TIME OF BUILDING PERMIT SUBMITTAL.



LEGEND

- 10' WILLAMETTE GREENWAY CONCRETE PATH (WITHIN 15' EASEMENT TO CITY OF SALEM)
- TEMPORARY GREENWAY ACCESS TO FRONT STREET UNTIL FUTURE CONNECTIONS ARE CONSTRUCTED.
- INTERIOR PROPERTY LINE
- ASPHALT PAVEMENT SECTION
- CONCRETE SIDEWALK (4" MIN THICKNESS)
- CONCRETE PAVEMENT SECTION (8" MIN THICKNESS)
- CONCRETE PAVERS (REFER TO PLANS BY OTHERS)
- STORMWATER FACILITY
- PLANNED TREE (REFER TO LANDSCAPE PLANS BY OTHERS)

PRELIMINARY SITE PLAN
THE CANNERY
Fund
SALEM, OREGON



REVISIONS: DECEMBER 31, 2024
JOB NUMBER: 5968-01
DATE: 07/26/2024
DESIGNED BY: TDR
DRAWN BY: MJM
CHECKED BY: TDR

ABBREVIATIONS:

- EXISTING:
 (RIM): EXISTING RIM ELEVATION
 (CONC): EXISTING CONCRETE ELEVATION
- PROPOSED:
 FFE: FINISHED FLOOR ELEVATION
 FGE: FINISHED GRADE ELEVATION
 RIM: RIM ELEVATION
 AC: ASPHALT CONCRETE ELEVATION
 TC: TOP OF CURB ELEVATION
 BSE: BOTTOM OF STAIR ELEVATION
 TSE: TOP OF STAIR ELEVATION
 TW: TOP OF WALL ELEVATION
 BW: BOTTOM OF WALL ELEVATION
 SW: SIDEWALK ELEVATION
 TD: TRENCH DRAIN RIM ELEVATION
 GUT: GUTTER ELEVATION

DOWNWARD SLOPE: X.X%

GENERAL NOTES:

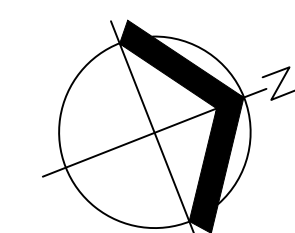
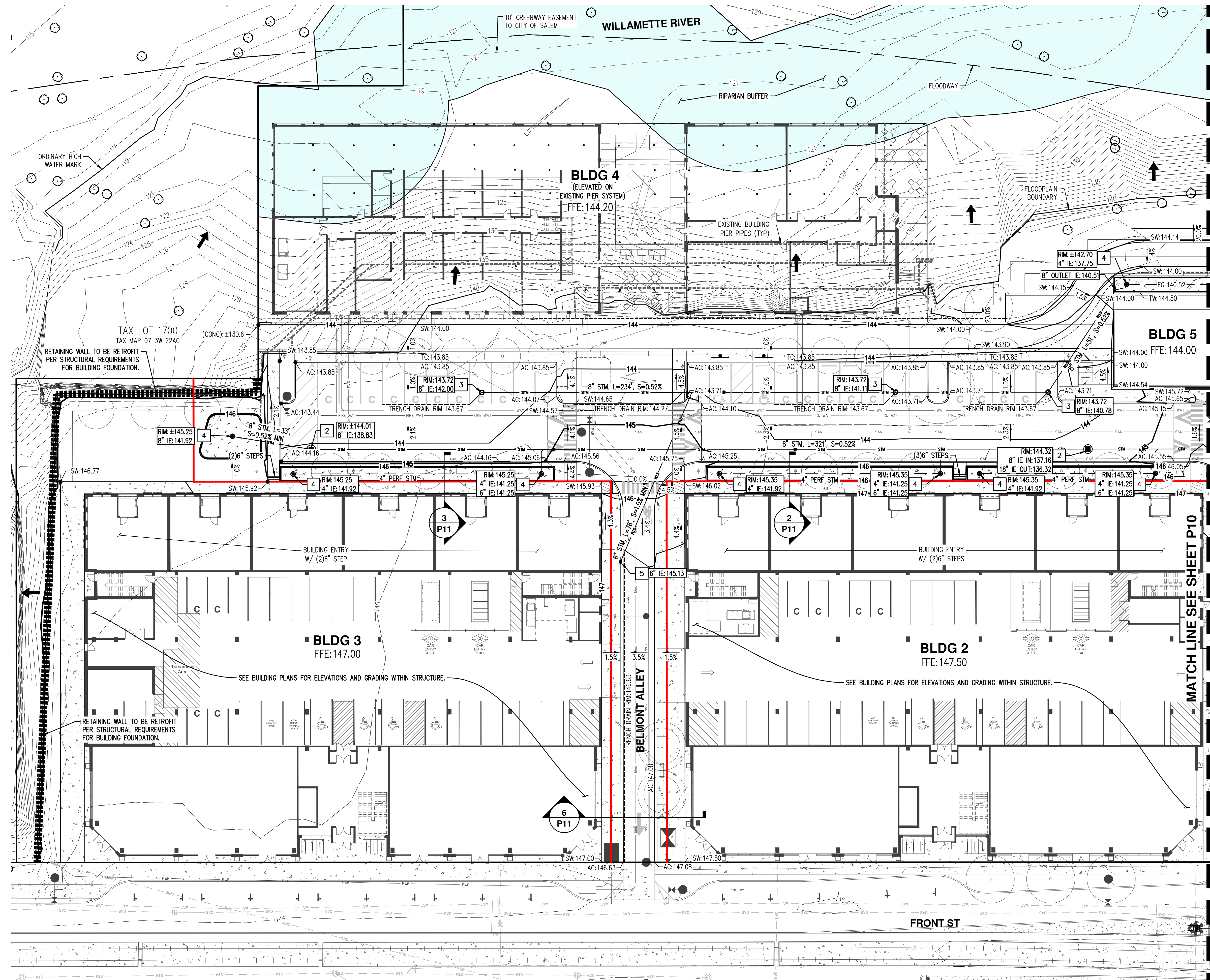
- (P) PRIOR TO CONSTRUCTION AND ORDERING PIPE MATERIALS, CONTRACTOR SHALL POTHOLE EXISTING UTILITIES TO VERIFY EXACT LOCATION, ALIGNMENT, DEPTH, AND SIZE. CONTACT ENGINEER IF ADJUSTMENT IS REQUIRED.

STORM DRAIN (STM) KEYED NOTES: #

- CONNECT TO EXISTING 24" CONCRETE PUBLIC STORM MAIN WITH NEW 48" MANHOLE. RIM AND INVERT ELEVATION (IE) PER PLANS.
- 48" STM MANHOLE. RIM AND IE PER PLAN.
- 24" STM MINI MANHOLE. RIM AND IE PER PLAN.
- STM BEEHIVE OVERFLOW.
- STM CLEANOUT (CO). IE PER PLAN.
- STM AREA DRAIN. RIM AND IE PER PLAN.
- ADJUST EXISTING MANHOLE RIM TO FINISHED GRADE ELEVATION.
- STUB FOR FUTURE LOT 5 AND 6 CONNECTIONS.

LEGEND

EXISTING GROUND CONTOUR (1 FT)	---	149
EXISTING GROUND CONTOUR (5 FT)	---	150
FINISHED GRADE CONTOUR (1 FT)	---	149
FINISHED GRADE CONTOUR (5 FT)	---	150
INTERIOR PROPERTY LINE	---	
TRENCH DRAIN	---	
PROPOSED MANHOLE (MH)	●	
PROPOSED CLEANOUT (CO)\DOWNSPOUT (DS)	•	
PROPOSED CATCH BASIN (CB)	■	
BEEHIVE OVERFLOW DRAIN (BH)	○	
MINI MANHOLE (MMH)	○	
STORMWATER FACILITY	+	
ADA RAMP LANDING AREA (2% MAX ANY DIRECTION)	▨	
EXISTING SLOPE GREATER THAN 15%	→	

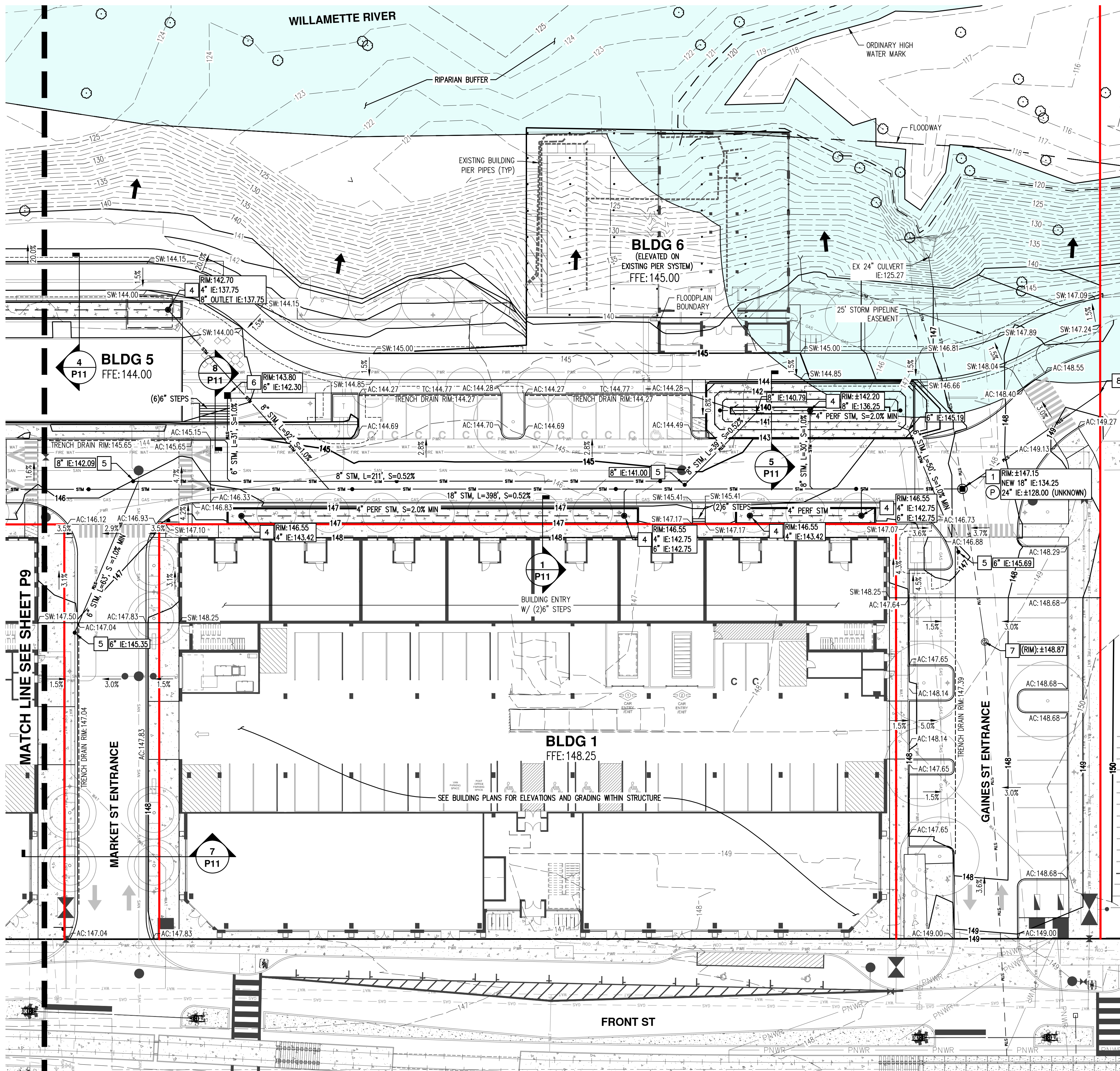


SCALE: 1" = 20 FEET
 ORIGINAL PAGE SIZE: 22" x 34"

PRELIMINARY ONSITE GRADING AND DRAINAGE PLAN
 THE CANNERY FUND
 SALEM, OREGON



JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: MJM
 CHECKED BY: TDR



ABBREVIATIONS:

- EXISTING:**
 (RIM): EXISTING RIM ELEVATION
 (CONC): EXISTING CONCRETE ELEVATION
- PROPOSED:**
 FFE: FINISHED FLOOR ELEVATION
 FG: FINISHED GRADE ELEVATION
 RIM: RIM ELEVATION
 AC: ASPHALT CONCRETE ELEVATION
 TC: TOP OF CURB ELEVATION
 BSE: BOTTOM OF STAIR ELEVATION
 TSE: TOP OF STAIR ELEVATION
 TW: TOP OF WALL ELEVATION
 BW: BOTTOM OF WALL ELEVATION
 SW: SIDEWALK ELEVATION
 TD: TRENCH DRAIN RIM ELEVATION
 GUT: GUTTER ELEVATION
- DOWNWARD SLOPE: X.X%

GENERAL NOTES:

(P) PRIOR TO CONSTRUCTION AND ORDERING PIPE MATERIALS, CONTRACTOR SHALL POTHOLE EXISTING UTILITIES TO VERIFY EXACT LOCATION, ALIGNMENT, DEPTH, AND SIZE. CONTACT ENGINEER IF ADJUSTMENT IS REQUIRED.

STORM DRAIN (STM) KEYED NOTES: #

- CONNECT TO EXISTING 24" CONCRETE PUBLIC STORM MAIN WITH NEW 48" MANHOLE. RIM AND INVERT ELEVATION (IE) PER PLANS.
- 48" STM MANHOLE. RIM AND IE PER PLAN.
- 24" STM MINI MANHOLE. RIM AND IE PER PLAN.
- STM BEEHIVE OVERFLOW.
- STM CLEANOUT (CO). IE PER PLAN.
- STM AREA DRAIN. RIM AND IE PER PLAN.
- ADJUST EXISTING MANHOLE RIM TO FINISHED GRADE ELEVATION.
- STUB FOR FUTURE LOT 5 AND 6 CONNECTIONS.

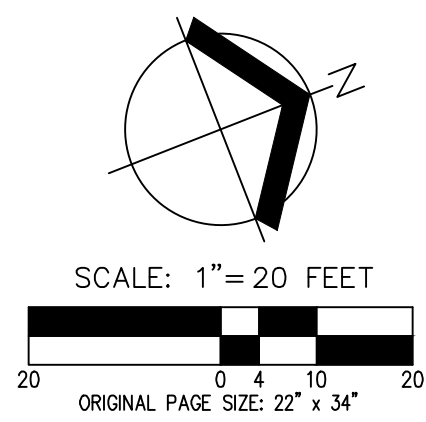
LEGEND

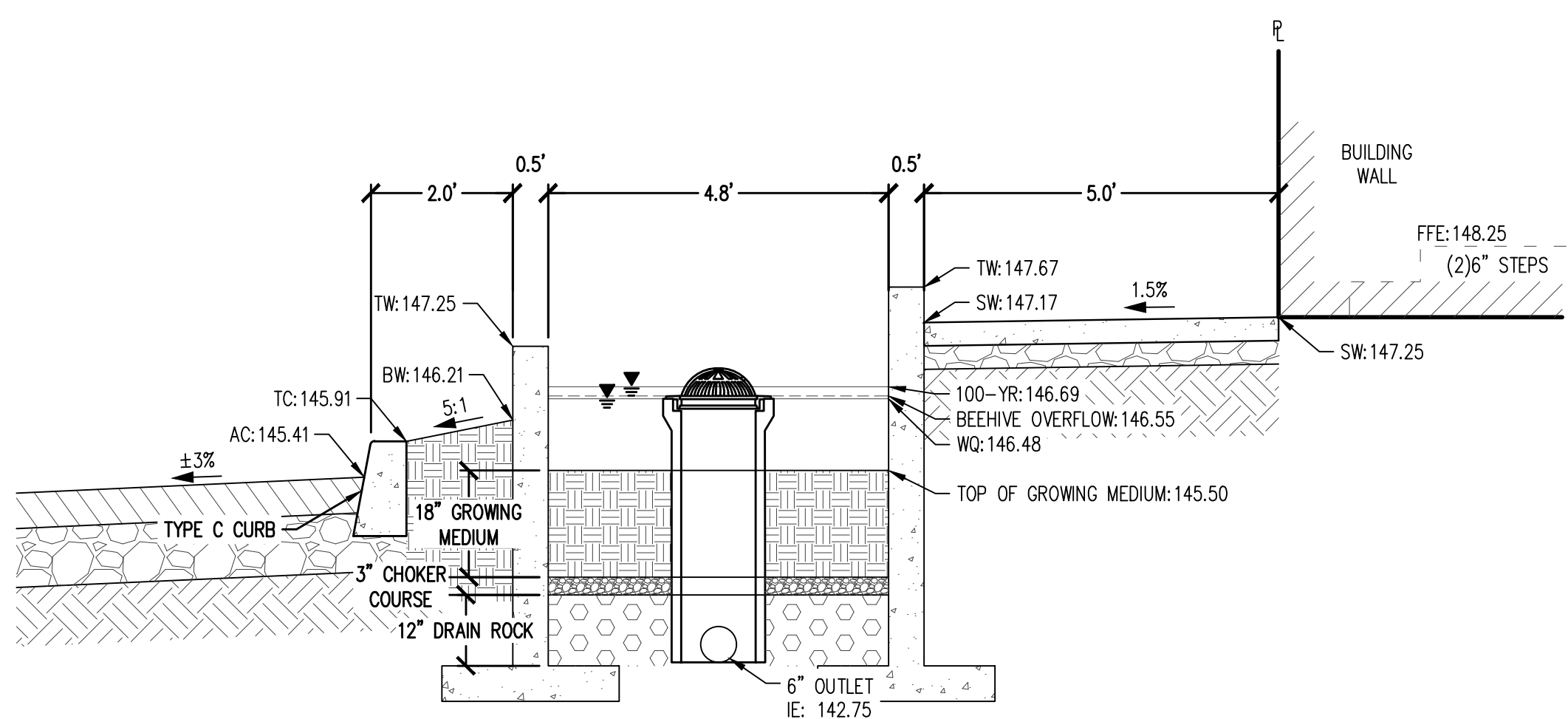
EXISTING GROUND CONTOUR (1 FT)	---	149
EXISTING GROUND CONTOUR (5 FT)	---	150
FINISHED GRADE CONTOUR (1 FT)	---	149
FINISHED GRADE CONTOUR (5 FT)	---	150
INTERIOR PROPERTY LINE	---	
TRENCH DRAIN	---	
PROPOSED MANHOLE (MH)	⊙	
PROPOSED CLEANOUT (CO)\DOWNSPOUT (DS)	•	
PROPOSED CATCH BASIN (CB)	■	
BEEHIVE OVERFLOW DRAIN (BH)	⊙	
MINI MANHOLE (MMH)	○	
STORMWATER FACILITY	+	
ADA RAMP LANDING AREA (2% MAX ANY DIRECTION)	▨	
EXISTING SLOPE GREATER THAN 15%	→	

**PRELIMINARY ONSITE GRADING AND DRAINAGE PLAN
 THE CANNERY
 FUND
 SALEM, OREGON**

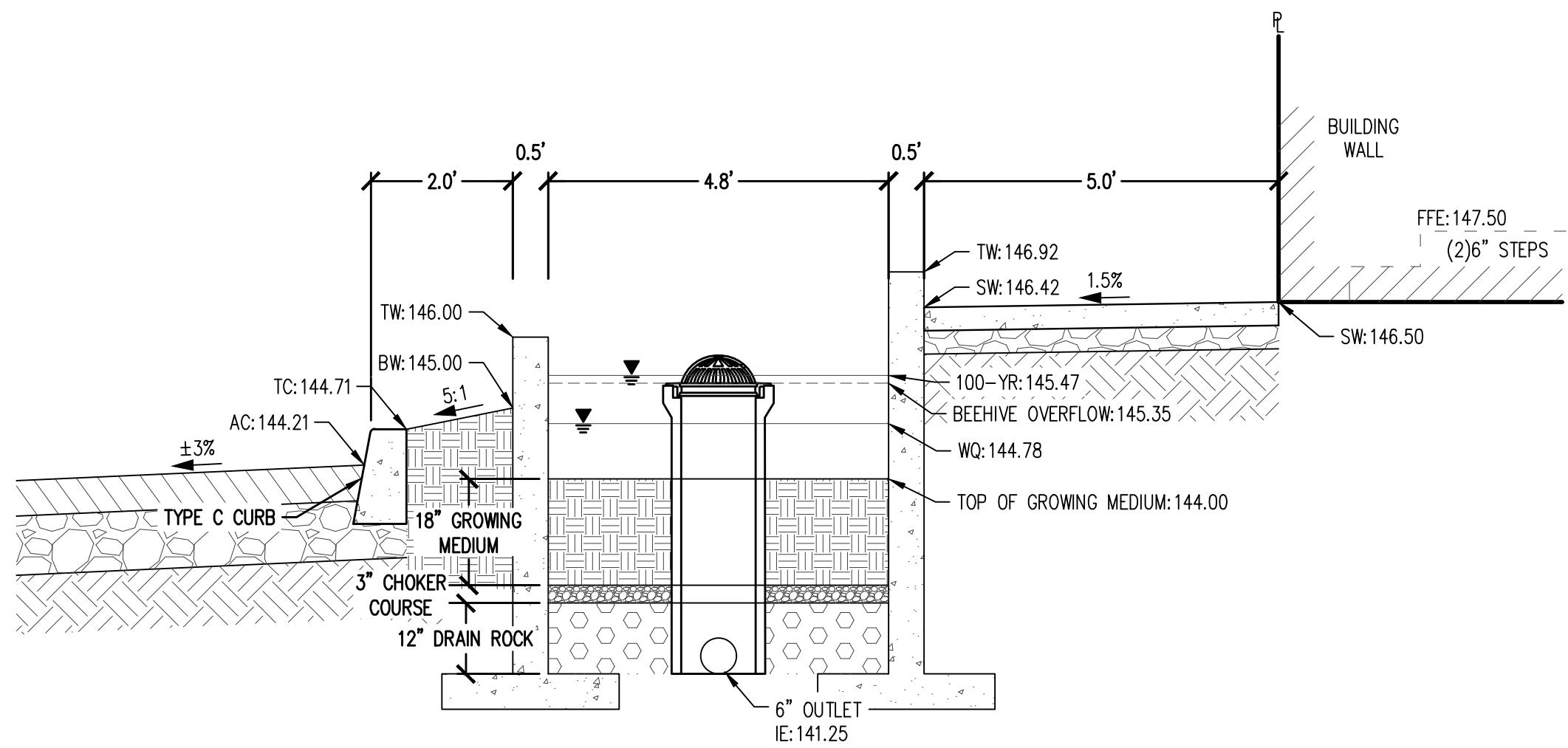


RENEWED: DECEMBER 31, 2024
 JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: MJM
 CHECKED BY: TDR

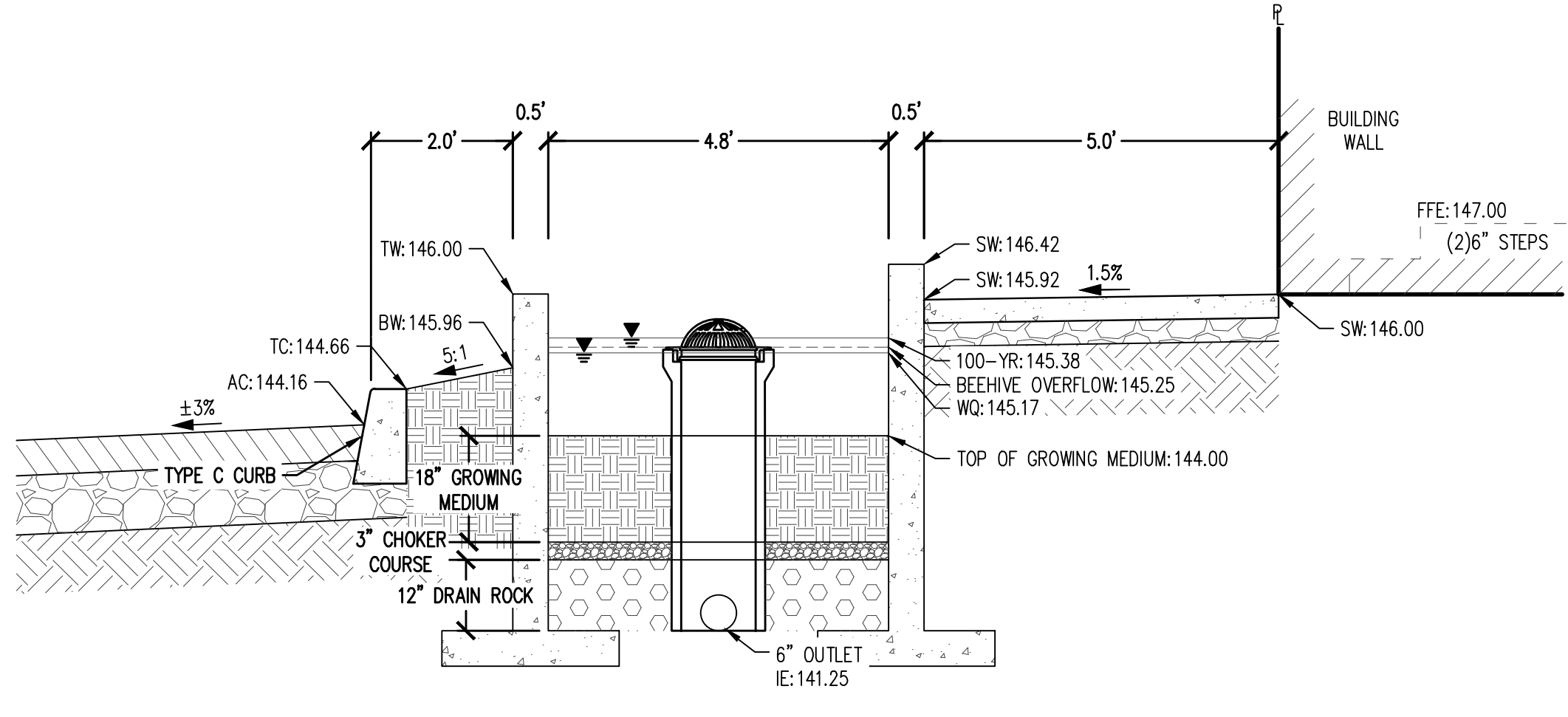




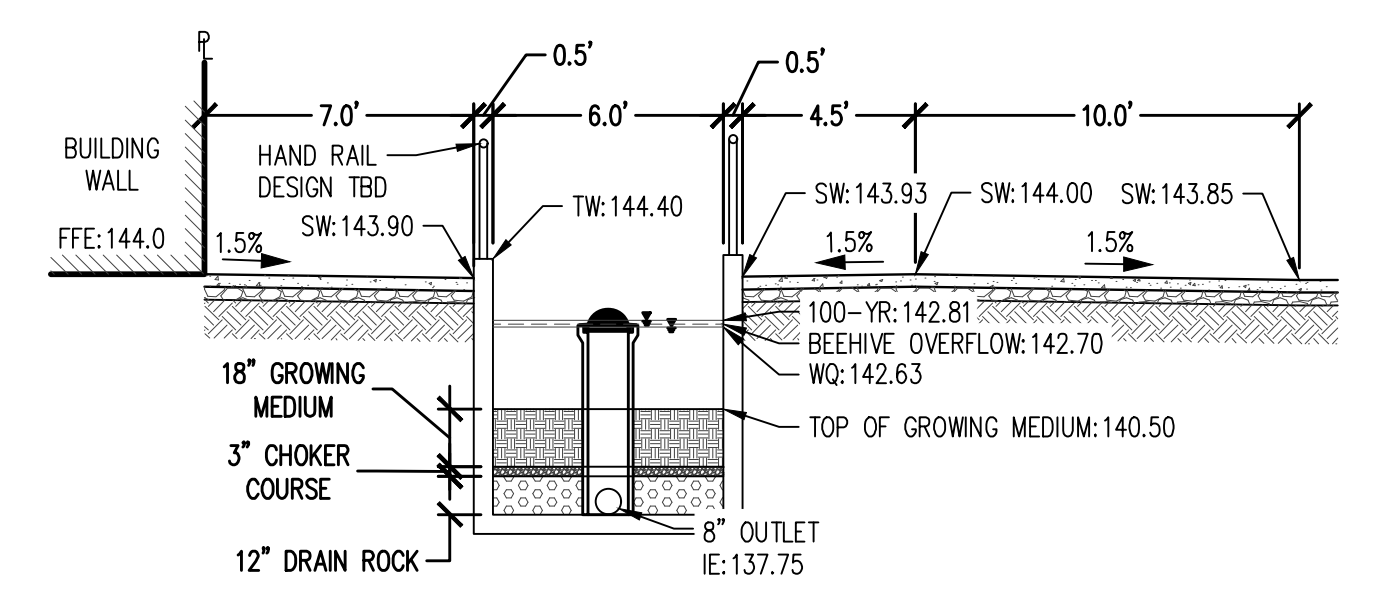
1 BUILDING 1 PLANTER CROSS-SECTION
 1" = 2'



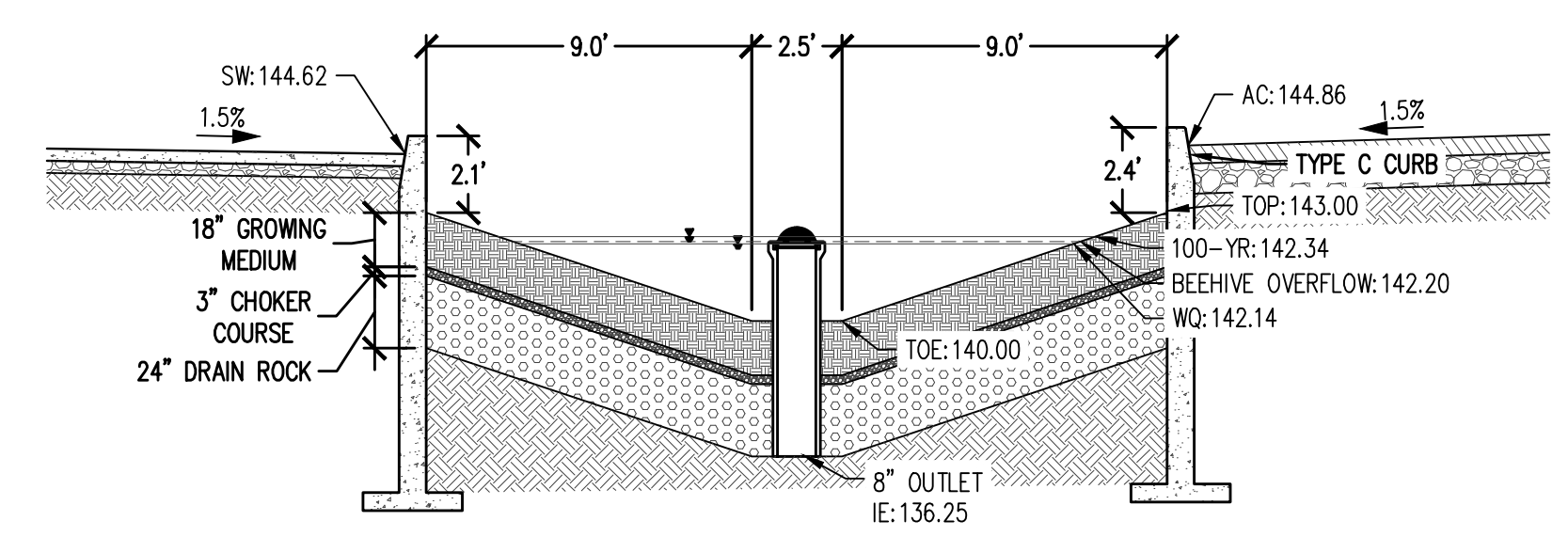
2 BUILDING 2 PLANTER CROSS-SECTION
 1" = 2'



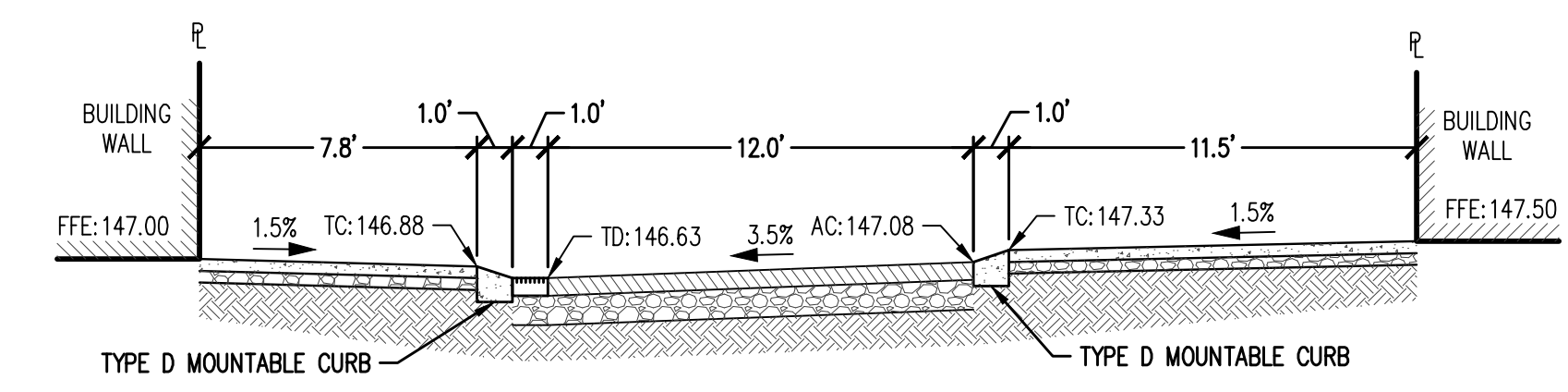
3 BUILDING 3 PLANTER CROSS-SECTION
 1" = 2'



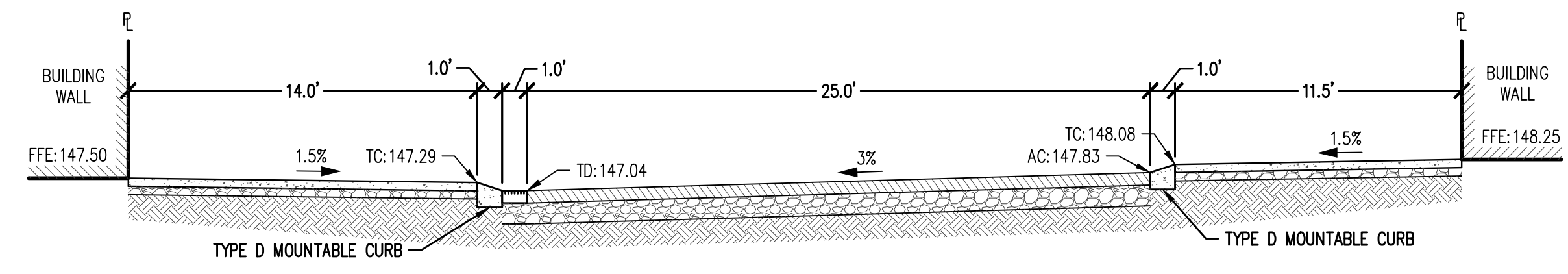
4 WINERY PLANTER CROSS-SECTION
 1" = 5'



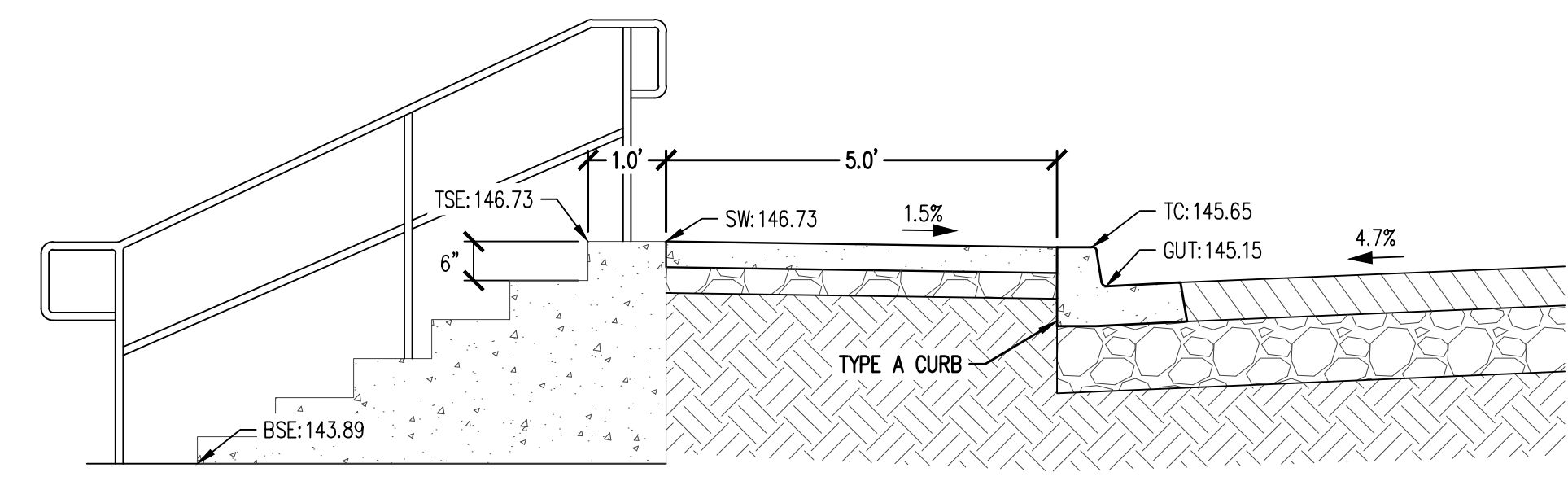
5 RAIN GARDEN CROSS-SECTION
 1" = 5'



6 BELMONT ALLEY CROSS-SECTION
 1" = 5'



7 MARKET ST ENTRANCE CROSS-SECTION
 1" = 5'

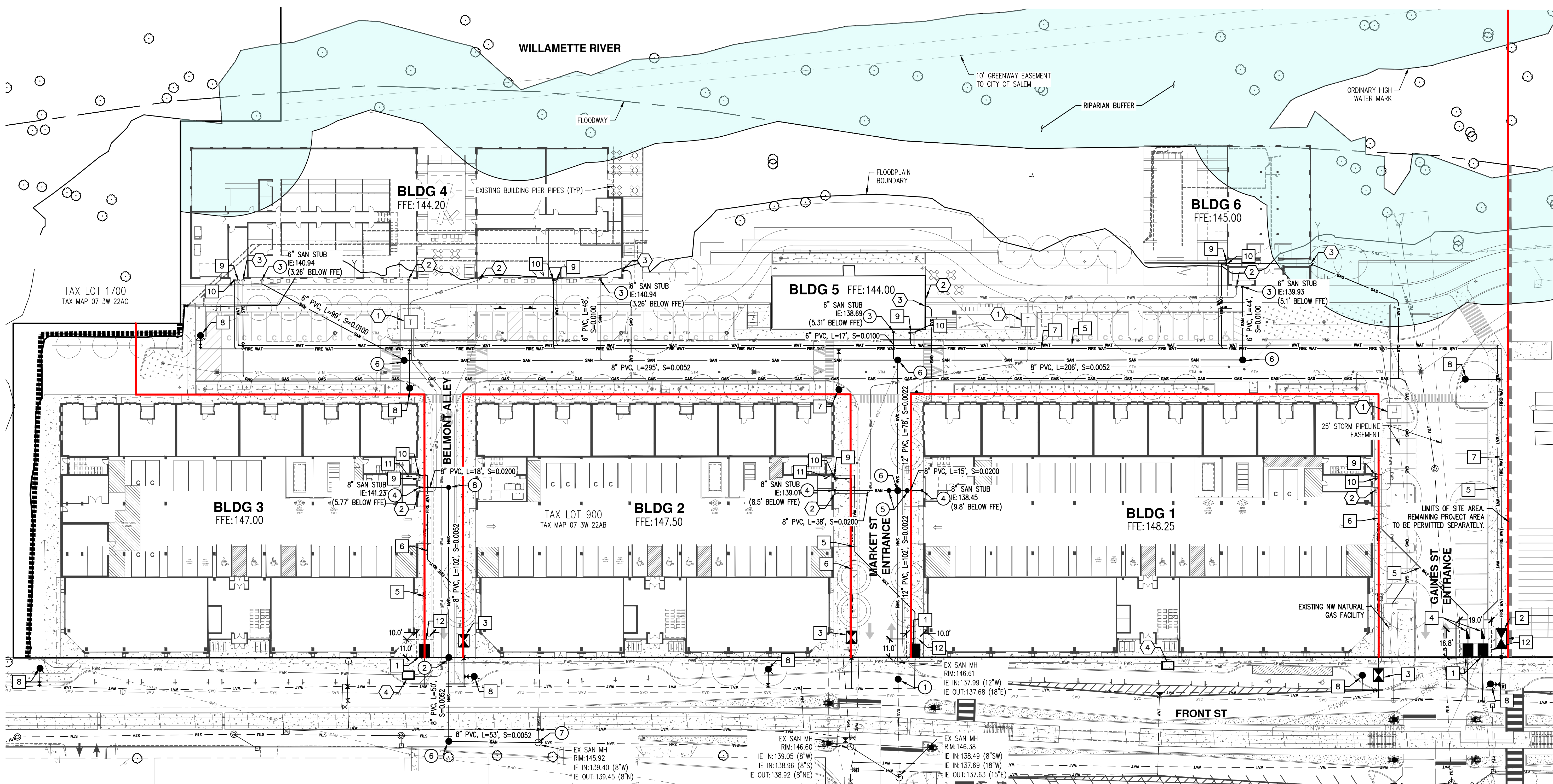


8 WINERY BUILDING STAIR STEP CONCEPT
 1" = 2'

ABBREVIATIONS:

- EXISTING:
 (RIM): EXISTING RIM ELEVATION
 (CONC): EXISTING CONCRETE ELEVATION
- PROPOSED:
 FFE: FINISHED FLOOR ELEVATION
 FG: FINISHED GRADE ELEVATION
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 BW: BOTTOM OF WALL ELEVATION
 SW: SIDEWALK ELEVATION
 TD: TRENCH DRAIN RIM ELEVATION
 GUT: GUTTER ELEVATION

DOWNWARD SLOPE: X.X%



WATER AND FIRE KEYED NOTES: #

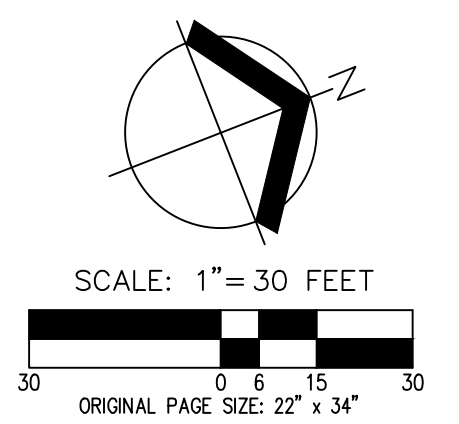
- 3" WATER METER PER CITY STANDARDS.
- 8" DOUBLE CHECK DETECTOR ASSEMBLY (DCDA) WITH FDC.
- 6" DCDA.
- ABOVE GROUND REDUCED PRESSURE (RP) ASSEMBLY.
- 4" DOMESTIC WATER SERVICE.
- 6" FIRE SERVICE.
- 8" FIRE SERVICE.
- FIRE HYDRANT ASSEMBLY.
- 4" DOMESTIC WATER SERVICE TO BUILDING. REFER TO PLANS BY OTHERS.
- 6" FIRE SERVICE TO BUILDING WITH FDC MOUNTED ON FACE OF BUILDING. REFER TO PLANS BY OTHERS.
- RP ASSEMBLY INSIDE BUILDING.
- CITY OF SALEM WATER EASEMENT.

SANITARY SEWER (SAN) KEYED NOTES: #

- CONNECT TO EXISTING SAN MAIN WITH NEW MANHOLE (MH).
- 48" SAN MONITORING MH.
- 6" SAN LATERAL TO BUILDING. REFER TO PLANS BY OTHERS.
- 8" SAN LATERAL TO BUILDING. REFER TO PLANS BY OTHERS.
- 24" MINI MH FOR MONITORING PER CITY OF SALEM STANDARDS.
- 48" SAN STANDARD MH.
- CONNECT TO EXISTING SAN MH.
- 24" MINI MH.

FRANCHISE UTILITY KEYED NOTES: #

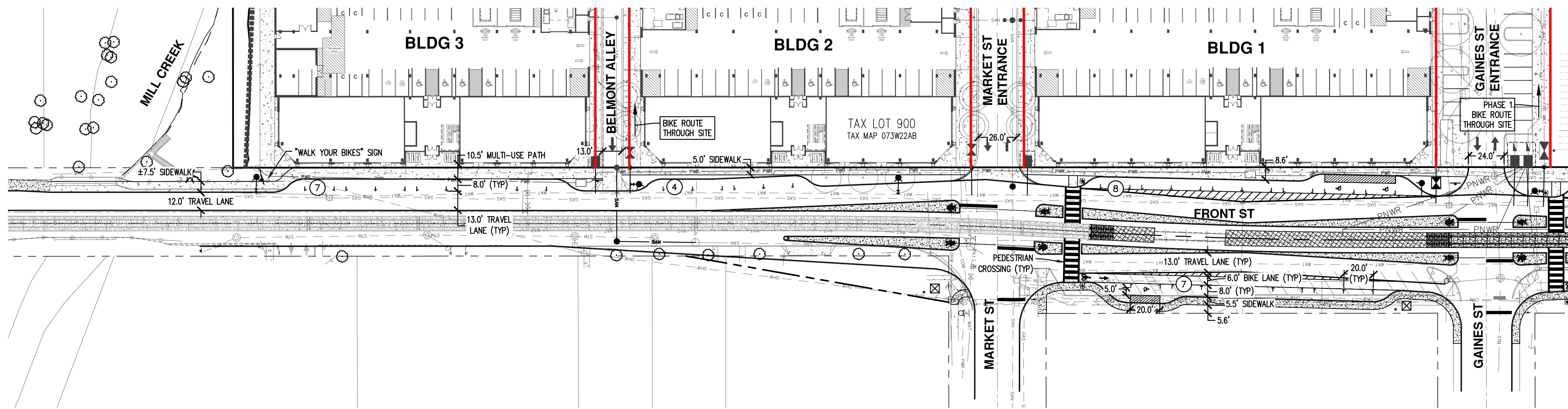
- CONCEPTUAL TRANSFORMER LOCATION.
- POWER CONDUIT TO PROPOSED BUILDINGS.
- GAS SERVICE TO BUILDINGS. COORDINATE WITH NORTHWEST NATURAL FOR FINAL SERVICE PLAN.
- CONCEPTUAL PULL VAULT LOCATION.



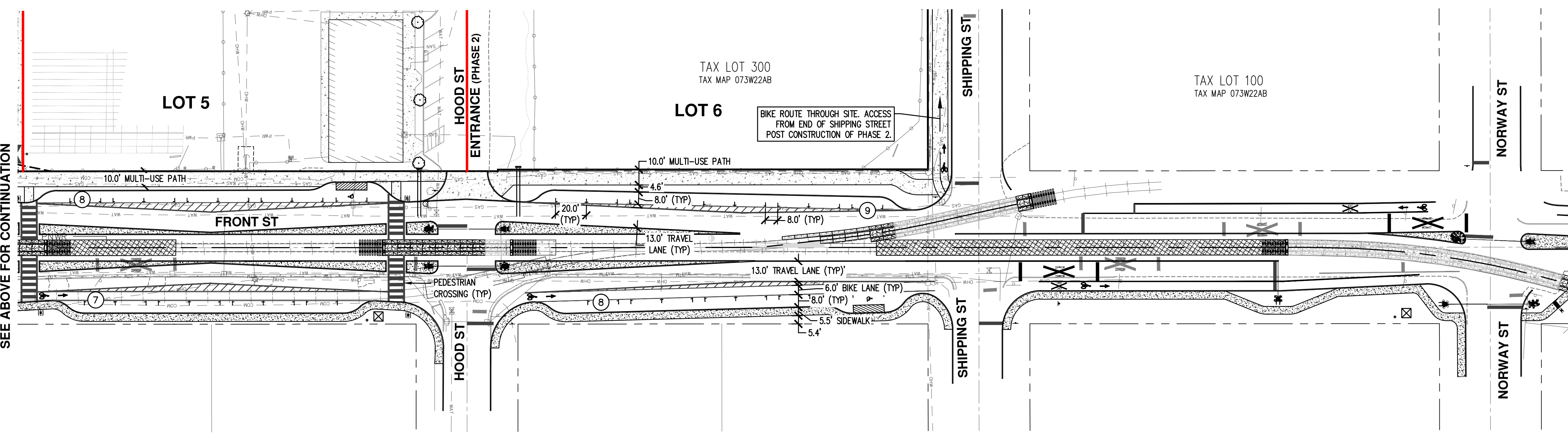
PRELIMINARY COMPOSITE UTILITY PLAN
THE CANNERY
Fund
SALEM, OREGON



REVISIONS: DECEMBER 31, 2024
JOB NUMBER: 5968-01
DATE: 07/26/2024
DESIGNED BY: TDR
DRAWN BY: MJM
CHECKED BY: TDR



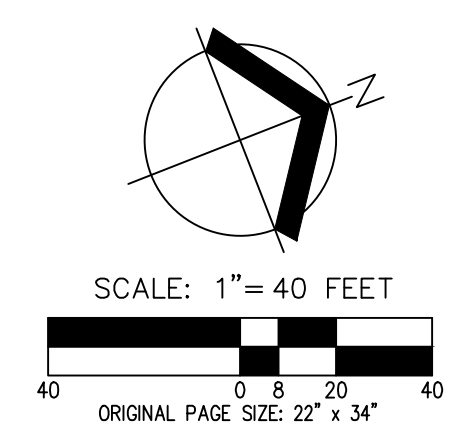
SEE BELOW FOR CONTINUATION



SEE ABOVE FOR CONTINUATION

LEGEND:
 # NUMBER OF PARKING STALLS IN ROW

GENERAL NOTE:
 PROPOSED FRONT STREET IMPROVEMENTS ARE SUBJECT TO CHANGE BASED ON RAIL AND CITY FEEDBACK. INFORMATION SHOWN IS BASED ON LATEST COORDINATION EFFORTS WITH THE CITY OF SALEM AND RAILROAD ENGINEER.

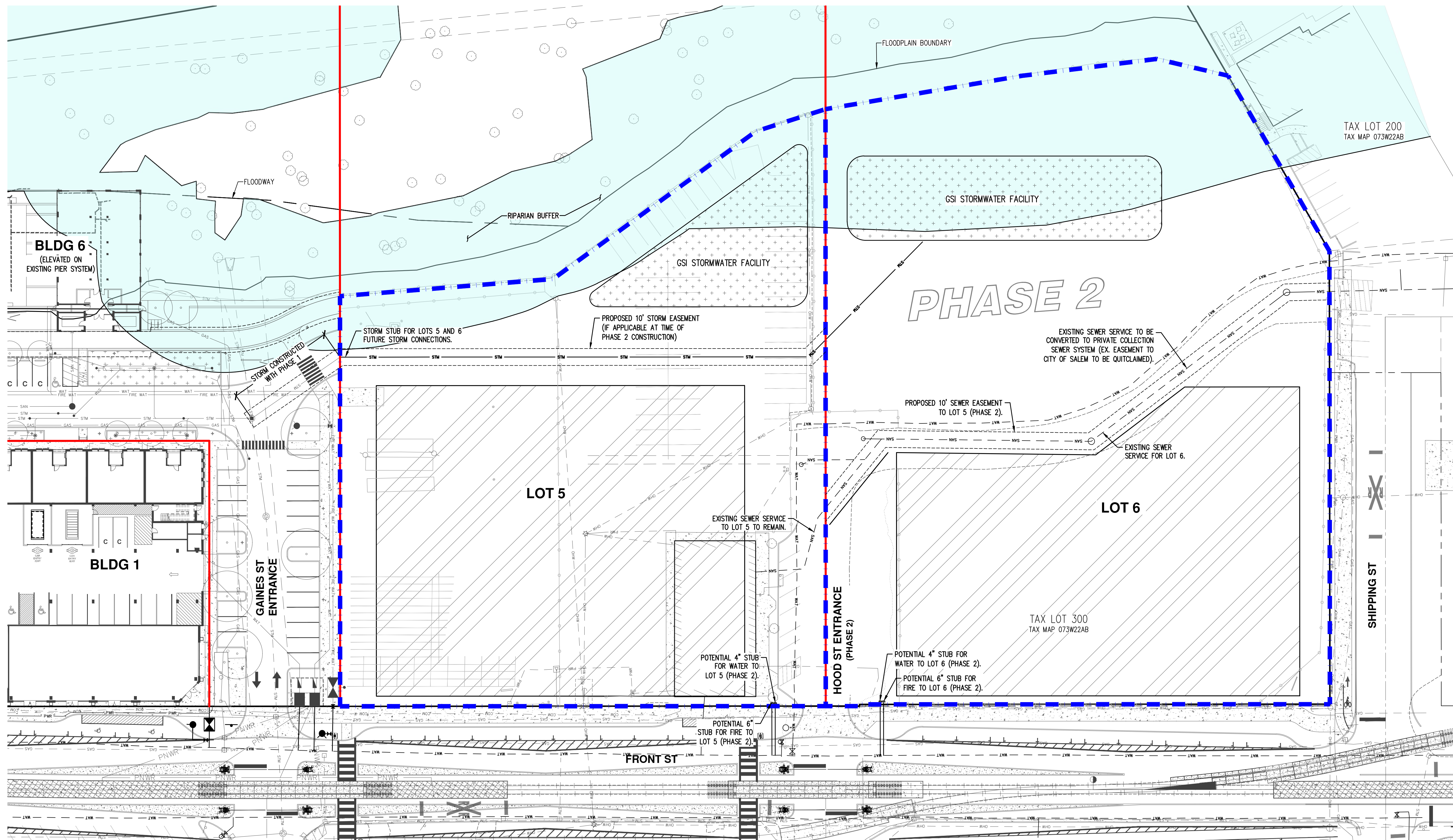


PRELIMINARY FRONT ST IMPROVEMENTS
 THE CANNERY
 FUND
 SALEM, OREGON



RENEWALS: DECEMBER 31, 2024

JOB NUMBER:	5968-01
DATE:	07/26/2024
DESIGNED BY:	TDR
DRAWN BY:	MJM
CHECKED BY:	TDR

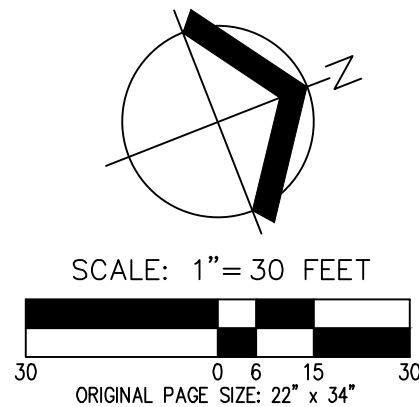


PHASE 2

LEGEND

- DEVELOPABLE AREA (ASSUMED FOR GSI STORMWATER FACILITY SIZING) [Blue dashed line]
- INTERIOR PROPERTY LINE [Red solid line]
- POTENTIAL BUILDING FOOTPRINTS TO BE FINALIZED WITH PHASE 2 SITE PLAN REVIEW [Hatched area]

- GENERAL NOTES:**
- 10% OF THE TOTAL NEW AND REPLACED IMPERVIOUS SURFACE HAS BEEN DEDICATED TO A GSI STORMWATER FACILITY ON LOTS 5 AND 6 PER THE CITY OF SALEM PUBLIC WORKS DESIGN STANDARDS SECTION 4E.6, AND FLOW CONTROL AND TREATMENT PERFORMANCE STANDARDS PER SRC CHAPTER 71.
 - THE PURPOSE OF THIS PLAN SHEET IS TO DEMONSTRATE THE POTENTIAL UTILITY LAYOUT FOR LOTS 5 AND 6. FINAL UTILITY LAYOUT AND SERVICE LOCATIONS TO BE CONFIRMED AT TIME OF PHASE 2 SITE PLAN REVIEW SUBMITTAL.



CONCEPTUAL LOT 5 AND 6 UTILITY PLAN
 THE CANNERY
 FUND
 SALEM, OREGON

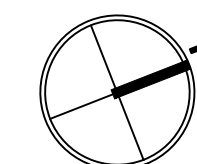
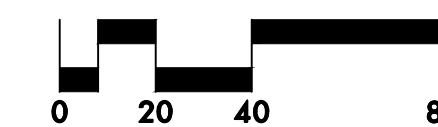
REGISTERED PROFESSIONAL ENGINEER
 COVER D. ROTH
 PRELIMINARY
 NOT FOR CONSTRUCTION
 RENEWS: DECEMBER 31, 2024
 JOB NUMBER: 5968-01
 DATE: 07/26/2024
 DESIGNED BY: TDR
 DRAWN BY: MJM
 CHECKED BY: TDR

Attachment D: Revised Preliminary Landscape Plans



1 ILLUSTRATIVE PLAN OVERALL

Plan
SCALE: 1" = 40'



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL

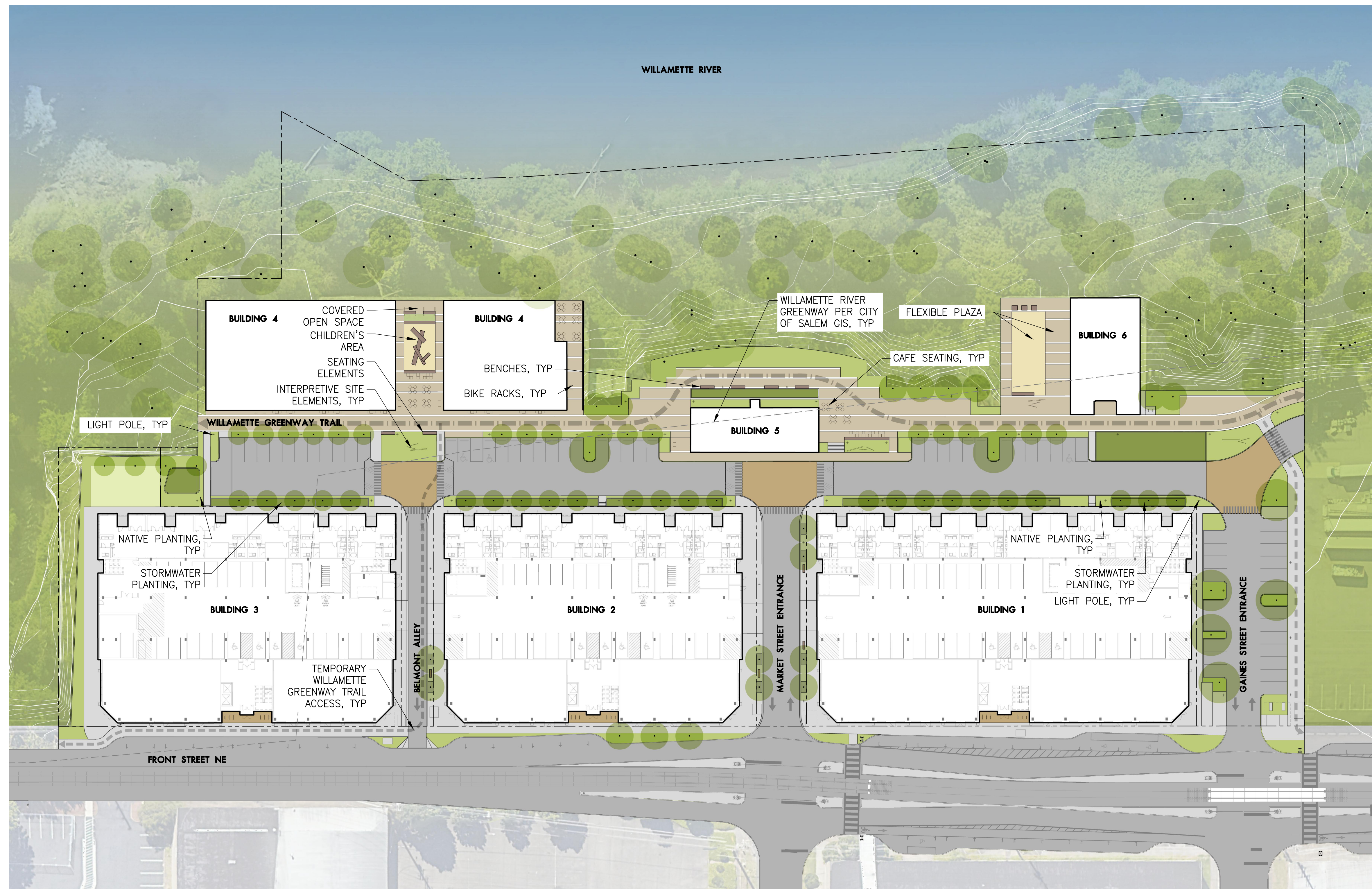
ILLUSTRATIVE PLAN
OVERALL

REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

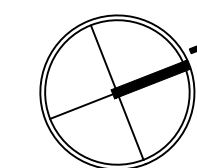
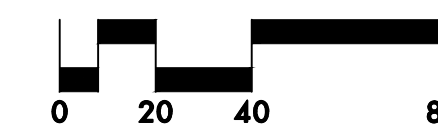
SHEET

L101



1 ILLUSTRATIVE PLAN LEVEL 1

Plan
SCALE: 1" = 40'



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

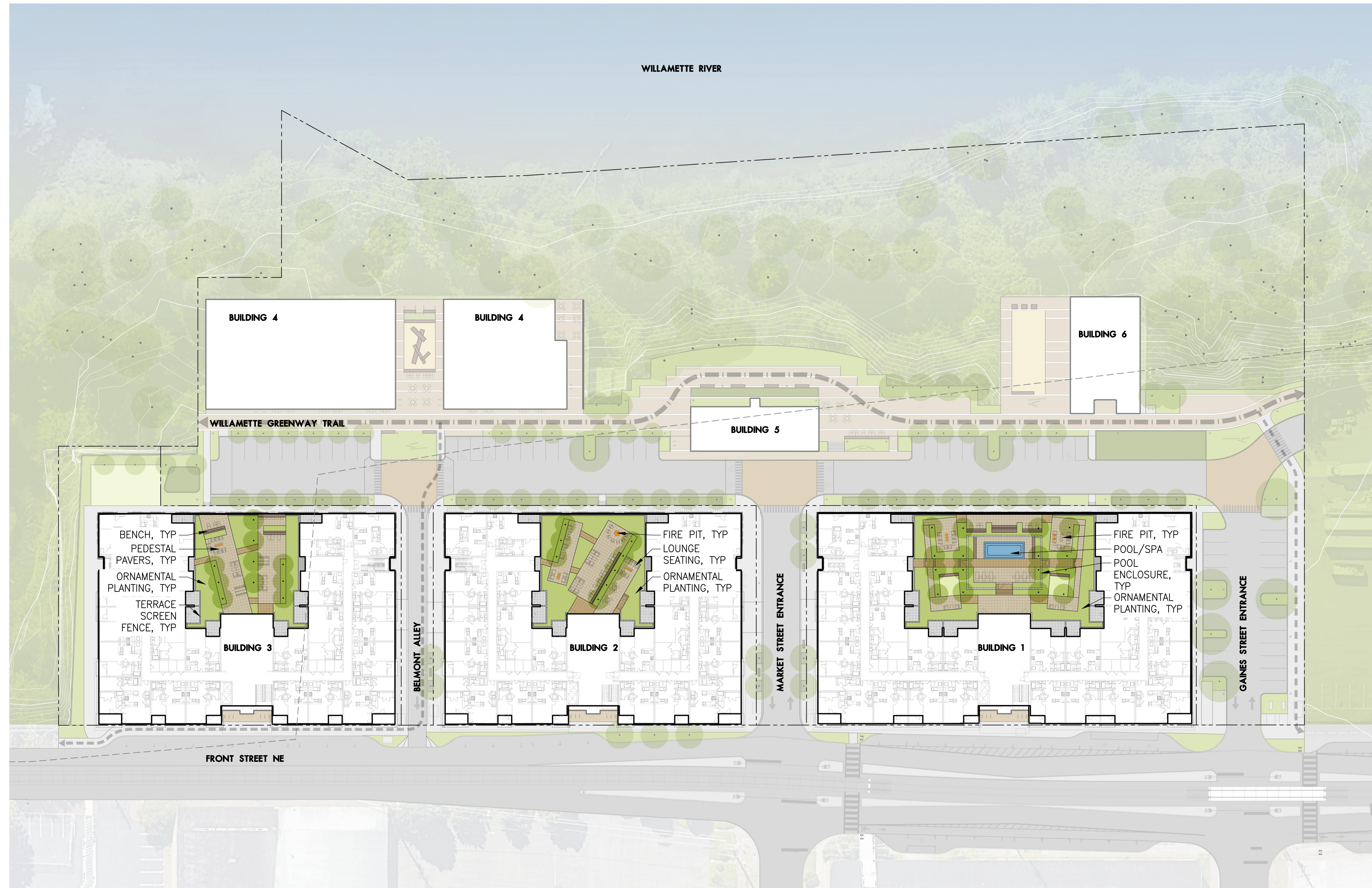
LAND USE SUBMITTAL
ILLUSTRATIVE PLAN LEVEL 1

REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

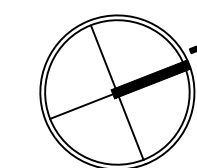
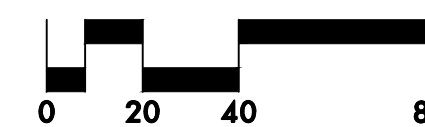
SHEET

L102



1 ILLUSTRATIVE PLAN LEVEL 2

Plan
SCALE: 1" = 40'



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL
ILLUSTRATIVE PLAN LEVEL 2

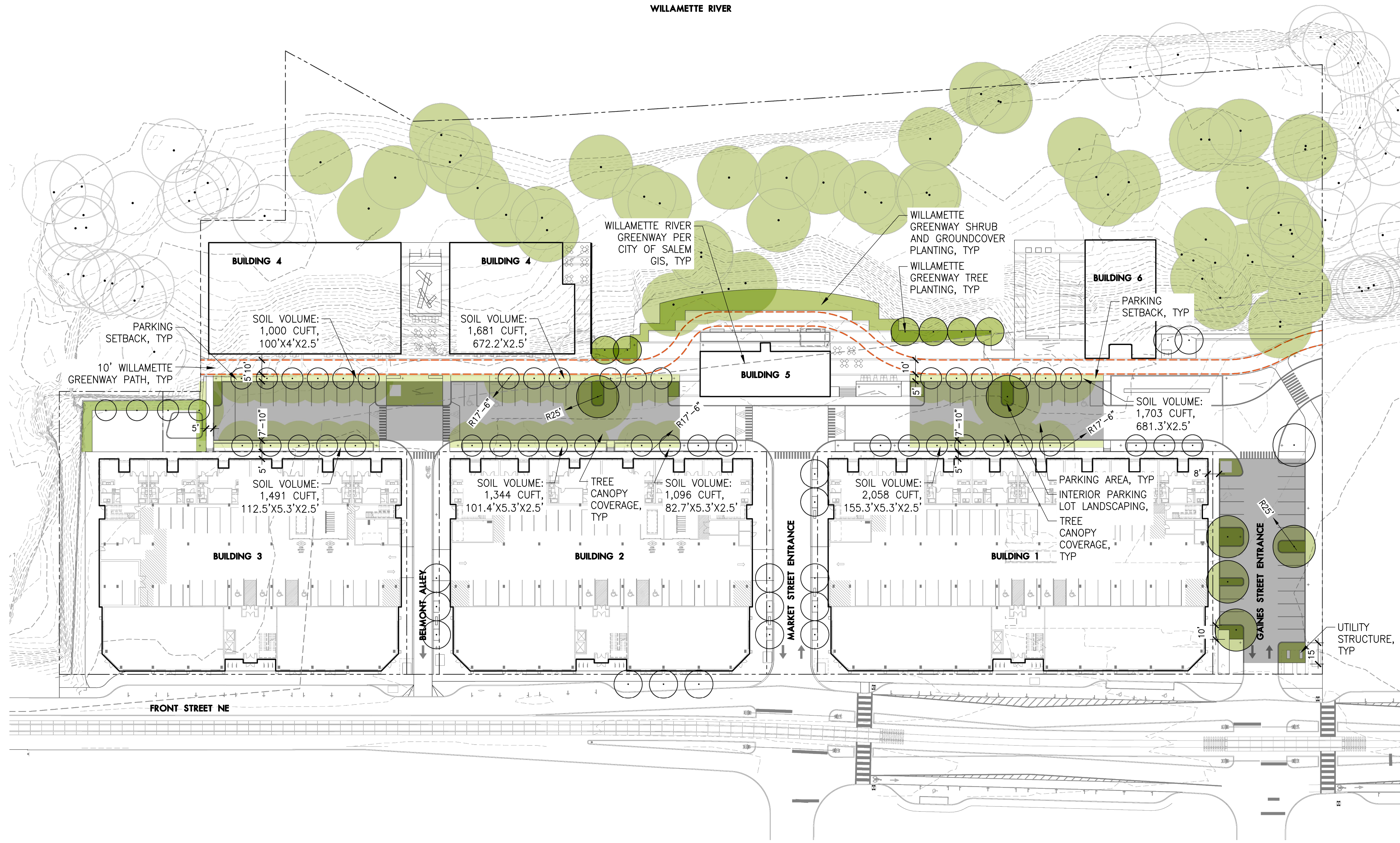
REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

SHEET

L103

WILLAMETTE RIVER



SITE LANDSCAPE DATA

SRC 600.025.B WILLAMETTE RIVER GREENWAY LANDSCAPING
 TOTAL LINEAR FEET OF RIVER FRONTAGE: 894 LF
 TREES REQUIRED (1 PER 20LF): 45 TREES
 TREES EXISTING: 57 TREES
 TREES PROPOSED: 6 TREES
 SHRUBS REQUIRED (1 PER 2LF): 447 SHRUBS
 SHRUBS PROPOSED: 851 SHRUBS

SRC 806.035.D.2 INTERIOR PARKING LOT LANDSCAPING
 TOTAL PARKING AREA: 28,090 SF
 INTERIOR LANDSCAPING REQUIRED: 5.0% (1,403 SF)
 INTERIOR LANDSCAPING PROPOSED: 7.2% (2,022 SF)

SRC 806.035.D.3 INTERIOR PARKING LOT TREES
 TOTAL PARKING STALLS: 58 STALLS
 INTERIOR TREES REQUIRED: 5 TREES (1 TREE PER 12 STALLS)
 INTERIOR TREES PROPOSED: 6 TREES

SRC 806.035.N PARKING LOT TREE CANOPY
 TOTAL PARKING AREA: 28,050 SF
 REQUIRED TREE CANOPY PERCENTAGE: 40%
 REQUIRED TREE CANOPY AREA: 11,220 SF
 PROPOSED TREE CANOPY AREA: 11,829 SF
 PROPOSED TREE CANOPY PERCENTAGE: 42%

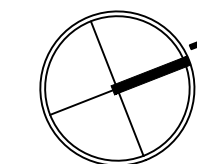
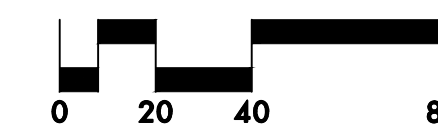
LANDSCAPE ARCHITECTS P.C.
lango.hansen
 1100 NW GLISAN #3A PORTLAND OR 97209 T. 503.295.2437

THE CANNERY

1105 FRONT ST NE, SALEM, OR 97301

1 LANDSCAPE LAND USE DIAGRAM

Plan
SCALE: 1" = 40'



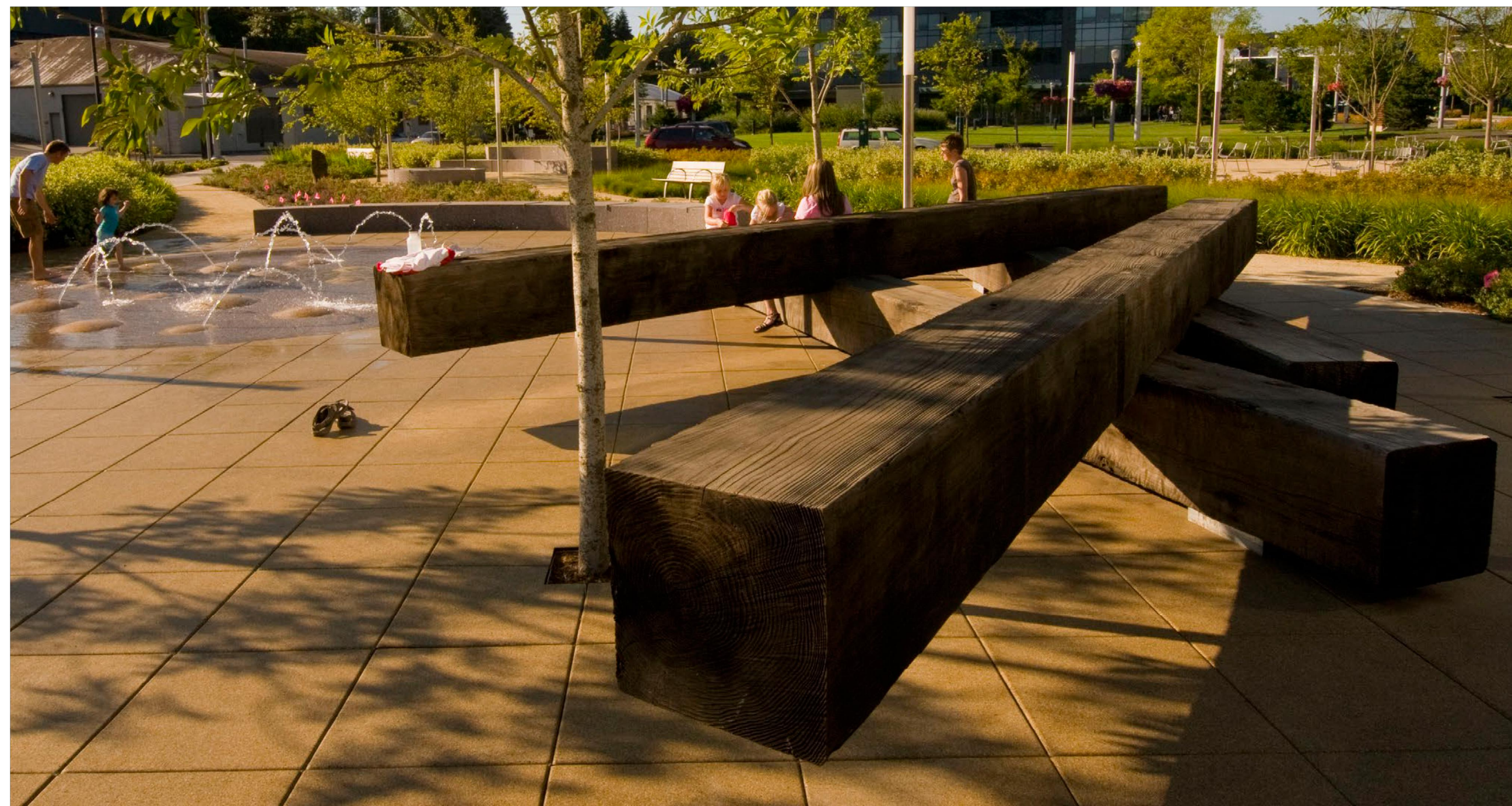
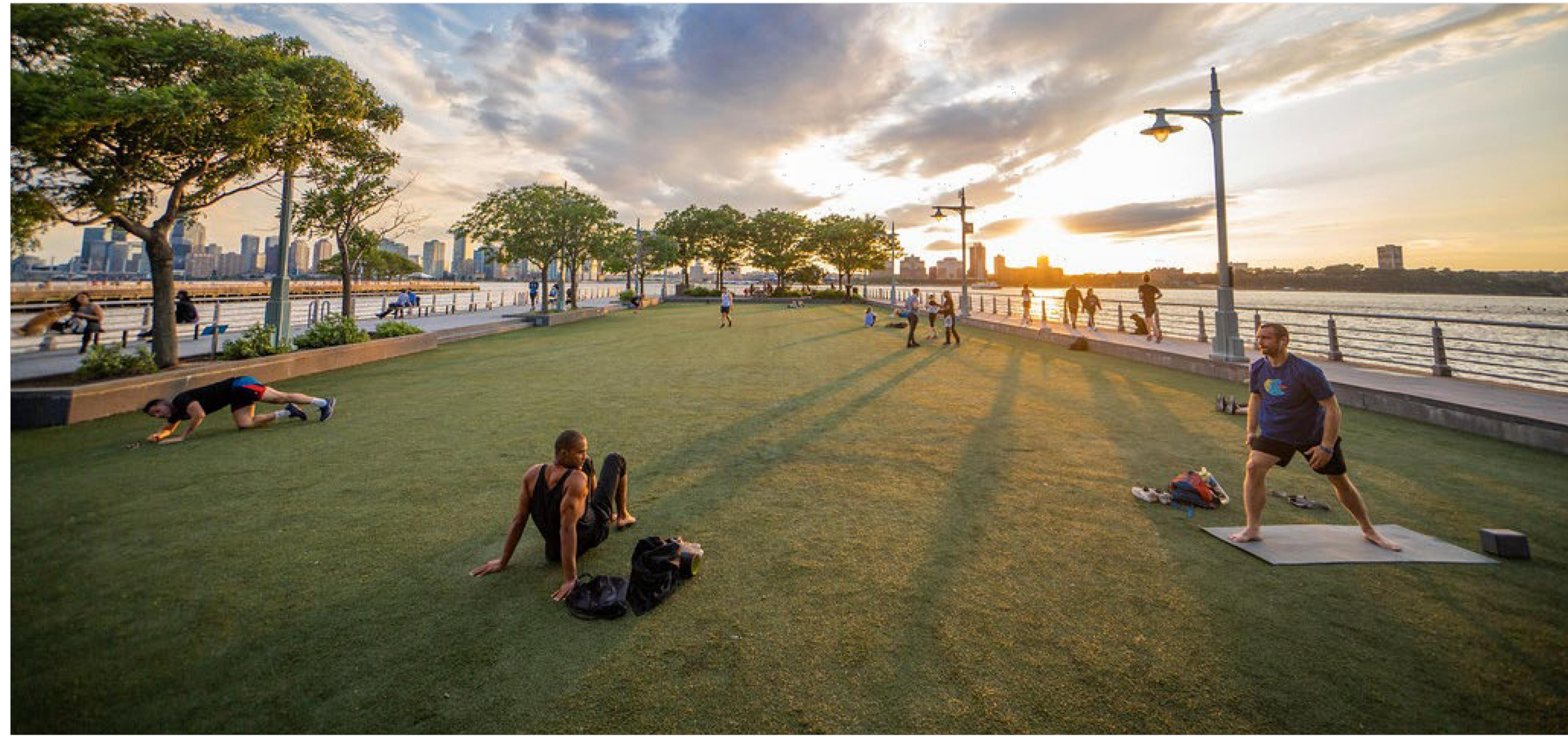
LAND USE SUBMITTAL
 LANDSCAPE LAND USE DIAGRAM

REVISIONS

SCALE
 DRAWN BY
 DATE 2024.03.15
 PROJECT NO. 2346-SAC

SHEET

L104



1 PRECEDENT IMAGES

LANDSCAPE ARCHITECTS PC
lango. hansen
 1100 nw glisan #3A portland OR 97209 T 503.295.2437

THE CANNERY

1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL
 PRECEDENT IMAGES

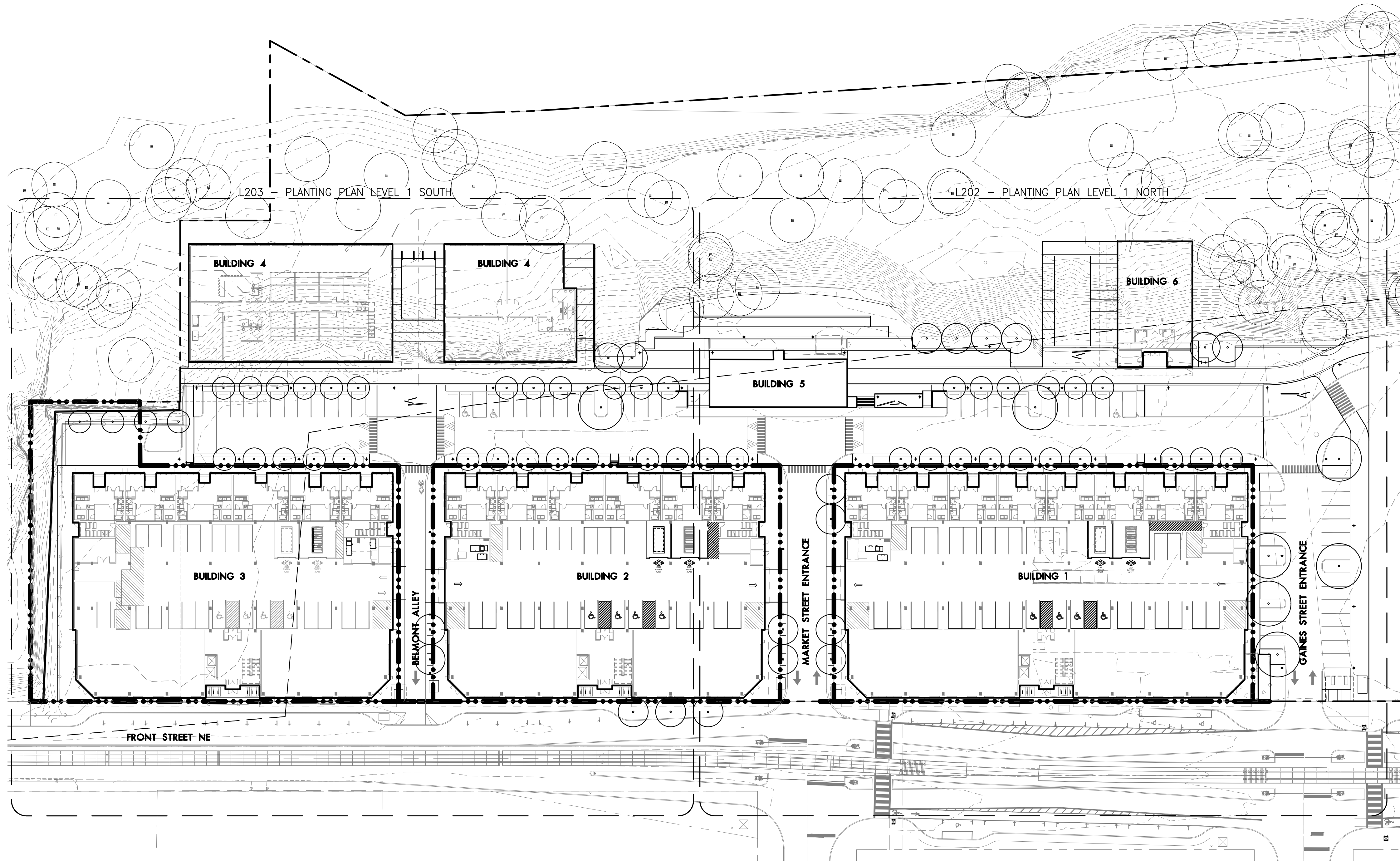
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SHEET

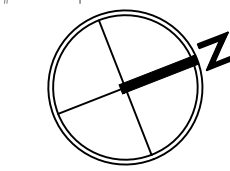
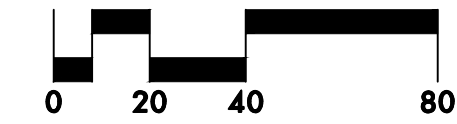
L105

WILLAMETTE RIVER



1 PLANTING PLAN LEVEL 1 OVERALL

Plan
SCALE: 1" = 40'



LEGEND

- WILLAMETTE RIVER GREENWAY
- - - PROPERTY LINE/RIGHT-OF-WAY
- - - - - INTERNAL PROPERTY LINE
- CONCRETE CURB
- BIKE RACK
- + LIGHT POLE
- ⊕ EXISTING TREE TO REMAIN

ABBREVIATIONS

- B&B BALLED & BURLAPPED
- CAL CALIPER
- CONT CONTAINER
- DIA DIAMETER
- DBH DIAMETER AT BREAST HEIGHT
- EQ EQUAL
- HT HEIGHT
- MIN MINIMUM
- MAX MAXIMUM
- NO NUMBER
- O.C. ON CENTER
- SIM SIMILAR
- SL SEEDED LAWN
- SPECS SPECIFICATIONS
- TYP TYPICAL
- # CONTAINER SIZE

PLANTING NOTES

1. THIS PLAN IS BASED ON A SURVEY BY AKS ENGINEERING AND FORESTRY DATED 2/17/2023. NOTIFY OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES IDENTIFIED ON SITE RELATED TO SURVEY INFORMATION PRIOR TO INSTALLATION.
2. PROTECT EXISTING VEGETATION TO REMAIN.
3. ALL PLANT MATERIAL SHALL BE NURSERY GROWN, WELL ROOTED, AND WELL BRANCHED. ALL TREES MUST BE FREE OF INSECTS, DISEASES, MECHANICAL INJURY, AND OTHER OBJECTIONABLE FEATURES WHEN PLANTED. ALL PLANT MATERIAL SHALL CONFORM TO "AMERICAN STOCK STANDARDS" LATEST EDITION.
4. ALL PLANT MATERIAL TO BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO INSTALLATION. SEE SPECIFICATIONS.
5. PLANT SPACING SHALL TAKE PRECEDENCE OVER VALVE BOX LOCATIONS. INSTALLED VALVE BOXES THAT CONFLICT WITH ACCEPTED PLANT LAYOUT SHALL BE MOVED TO POSITION BETWEEN PLANTS.
6. PLANT COUNTS FOR TREES AND SHRUBS ARE SUPPLIED FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR RESPONSIBLE FOR INSTALLING ALL PLANTS IN LOCATIONS AND QUANTITIES SHOWN.
7. CLEAR PLANT BEDS OF ALL GRAVEL AND DEBRIS PRIOR TO SOIL PREPARATION AND PLANTING, FOR APPROVAL BY LANDSCAPE ARCHITECT.
8. TREES TO BE RETAINED AND/OR PLANTED SHALL BE WATERED AS NECESSARY TO MINIMIZE STRESS TO THE TREE, PROMOTE ROOT GROWTH, AND ENSURE SURVIVAL, THROUGHOUT THE CONSTRUCTION PERIOD AND THE FIRST THREE GROWING SEASONS AFTER PLANTING. TREES SHALL BE MULCHED WITH COMPOST MULCH, SEE SPECIFICATIONS. PROTECTIVE BARRIERS SHALL STAY IN PLACE UNTIL PLANNING OFFICIAL AUTHORIZES THEIR REMOVAL OR A FINAL CERTIFICATE OF OCCUPANCY IS ISSUED, WHICHEVER OCCURS FIRST. STAKING & FERTILIZING SHALL BE REQUIRED WHERE NECESSARY BY PLANNING OFFICIAL. SEE SECTION 015639 FOR ADDITIONAL REQUIREMENTS.
9. ALL LANDSCAPE AREAS THAT HAVE A SLOPE GREATER THAN 1 VERTICAL FOOT IN 3 HORIZONTAL FEET SHALL RECEIVE JUTE MATTING, SEE SPECIFICATIONS.
10. ALL PLANTING AREAS ARE TO BE IRRIGATED WITH A PERMANENT AUTOMATIC IRRIGATION SYSTEM EXCEPT RESEDED DISTURBED AREAS, THOSE ARE ARE TO BE NON-IRRIGATED.
11. ALL PARKING LOT AND STREET TREES MUST HAVE 6' CLEAR HEIGHT TO LOWEST BRANCHES.
12. SEE L204 FOR PLANTING SCHEDULE.

LANDSCAPE ARCHITECTS P.C.
lango. hansen
 1100 nw glisan #3A portland OR 97209 T 503.295.2437

THE CANNERY
 1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL

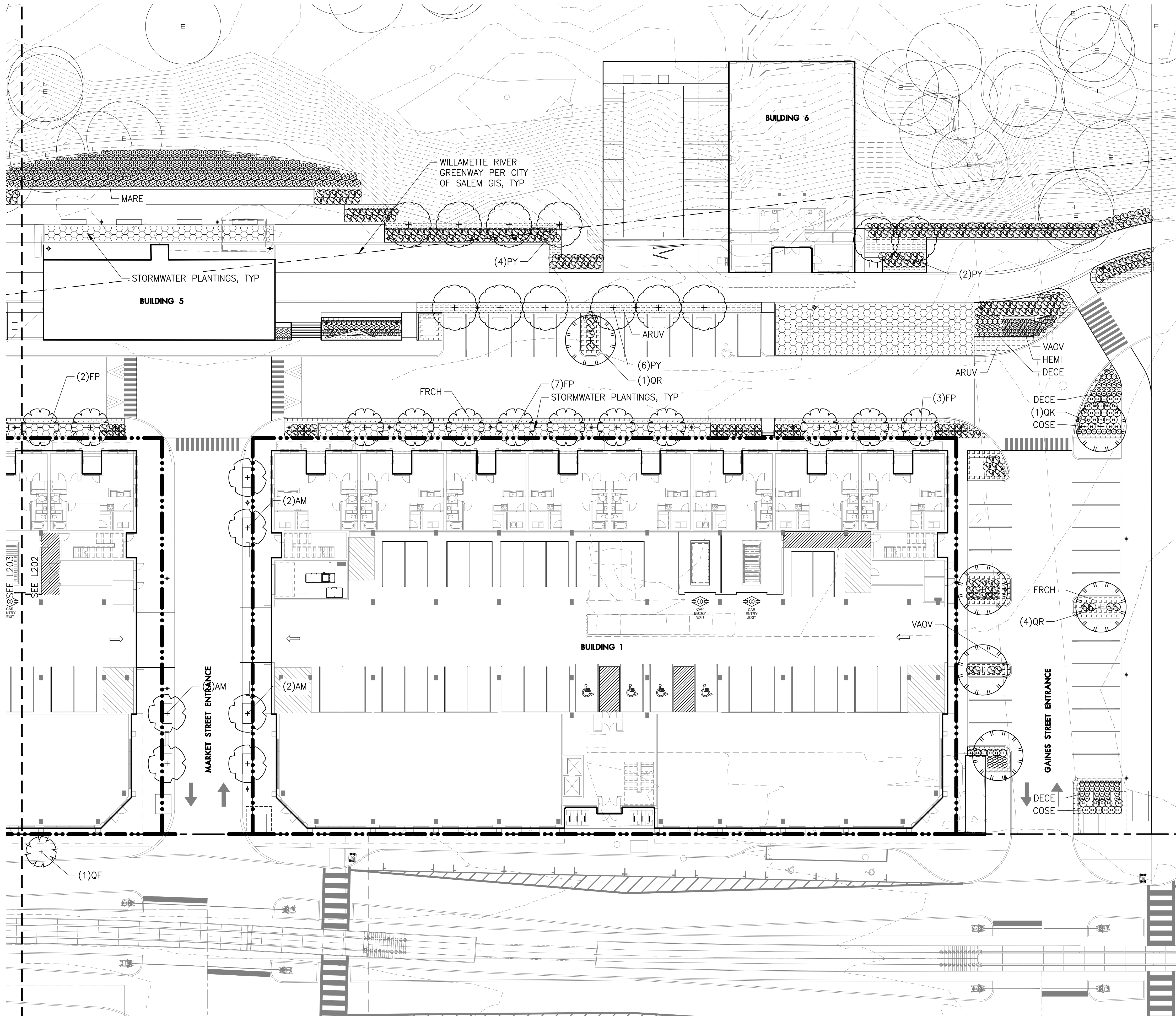
PLANTING PLAN LEVEL 1 OVERALL

REVISIONS

SCALE
 DRAWN BY
 DATE 2024.03.15
 PROJECT NO. 2346-SAC

SHEET

L201



- LEGEND**
- WILLAMETTE RIVER GREENWAY
 - - - PROPERTY LINE/RIGHT-OF-WAY
 - INTERNAL PROPERTY LINE
 - CONCRETE CURB
 - BIKE RACK
 - + LIGHT POLE
 - ⊕ EXISTING TREE TO REMAIN
 - ◻ STORMWATER FACILITY PLANTED TO CITY OF SALEM STANDARDS
- PLANTING NOTES**
1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.
 2. SEE L204 FOR PLANTING SCHEDULE.

LANDSCAPE ARCHITECTS P.C.
lango.hansen
 1100 NW GILMAN #3A, PORTLAND OR 97209 T 503.295.2437

THE CANNERY

1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL
 PLANTING PLAN LEVEL 1 NORTH

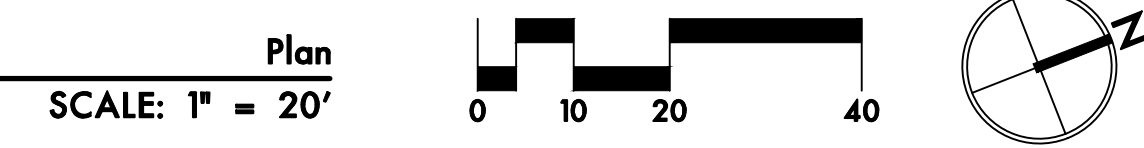
REVISIONS

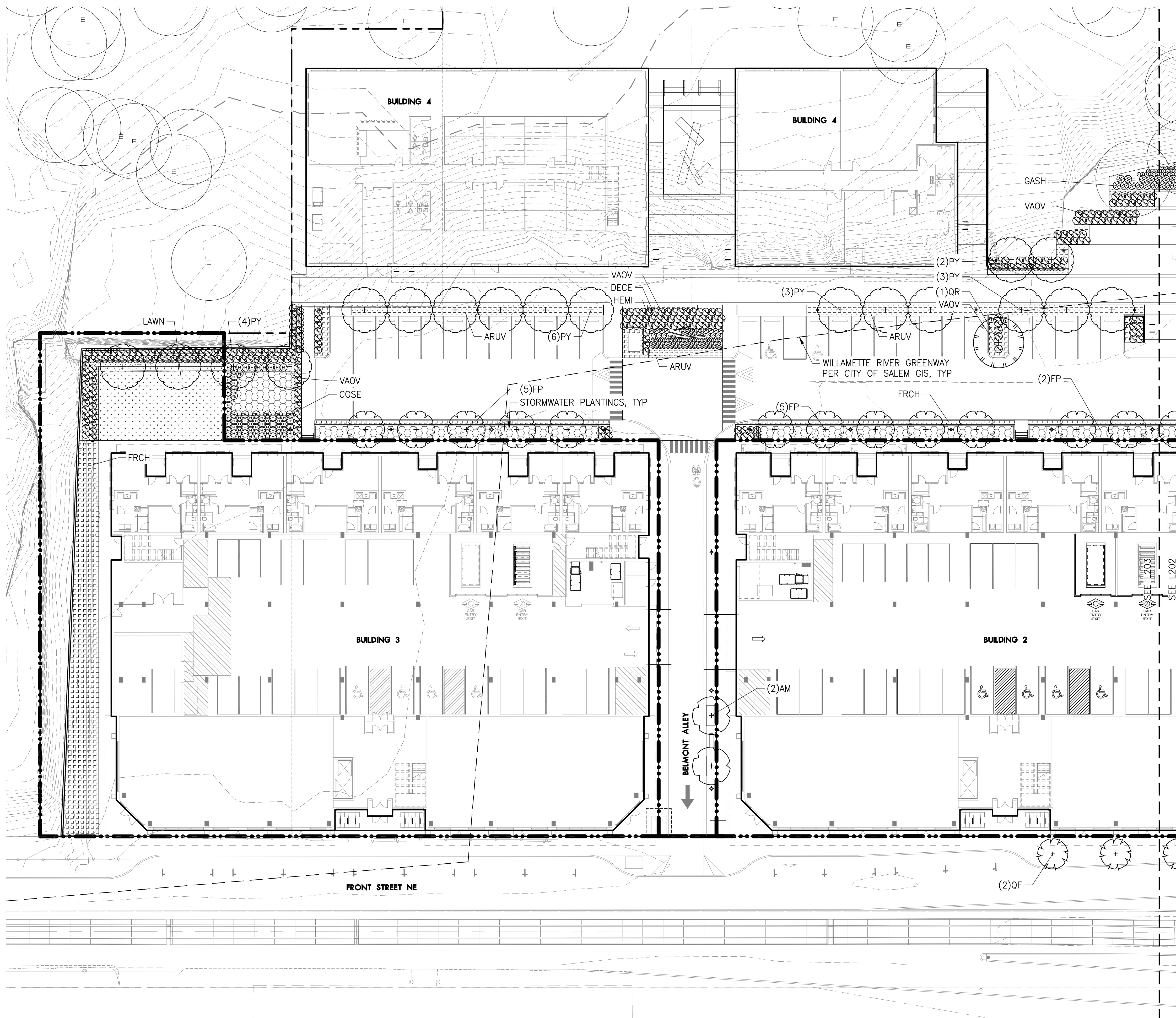
NO.	DATE	DESCRIPTION

SCALE
 DRAWN BY
 DATE 2024.03.15
 PROJECT NO. 2346-SAC

SHEET
L202

1 PLANTING PLAN LEVEL 1 NORTH





- LEGEND**
- WILLAMETTE RIVER GREENWAY
 - - - PROPERTY LINE/RIGHT-OF-WAY
 - - - INTERNAL PROPERTY LINE
 - CONCRETE CURB
 - BIKE RACK
 - LIGHT POLE
 - EXISTING TREE TO REMAIN
 - ◻ STORMWATER FACILITY PLANTED TO CITY OF SALEM STANDARDS
- PLANTING NOTES**
1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.
 2. SEE L204 FOR PLANTING SCHEDULE.

LANDSCAPE ARCHITECTS P.C.
lango. hansen
 1100 nw glisan #3A, portland OR 97209 T 503.295.2437

THE CANNERY

1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL
 PLANTING PLAN LEVEL 1
 SOUTH

REVISIONS

NO.	DESCRIPTION

SCALE
 DRAWN BY
 DATE 2024.03.15
 PROJECT NO. 2346-SAC

SHEET
L203

PLANT SCHEDULE – RIGHT-OF-WAY TREES							
SYMBOL	ABBR	BOTANICAL NAME	COMMON NAME	NATIVE	SIZE/ CONDITION	SPACING	QUANTITY
TREES							
	QF	Quercus robur 'Fastigiata'	Columnnar English Oak	NO	3" CAL B&B	AS SHOWN	3

PLANT SCHEDULE – ONSITE TREES							
SYMBOL	ABBR	BOTANICAL NAME	COMMON NAME	NATIVE	SIZE/ CONDITION	SPACING	QUANTITY
TREES							
	AM	Acer macrophyllum	Big Leaf Maple	YES	3" CAL B&B	AS SHOWN	8
	FP	Frangula purshiana	Cascara	YES	3" CAL B&B (SMALL)	AS SHOWN	24
	PY	Prunus x Yedoensis	Yoshino Cherry	NO	3" CAL B&B (SMALL)	AS SHOWN	30
	QR	Quercus rubra	Red Oak	NO	3" CAL B&B (LARGE)	AS SHOWN	7

PLANT SCHEDULE – ONSITE PLANTINGS							
SYMBOL	ABBR	BOTANICAL NAME	COMMON NAME	NATIVE	SIZE/ CONDITION	SPACING	QUANTITY
SHRUBS							
	COSE	Cornus sericea 'Arctic Fire'	Arctic Fire Redosier Dogwood	YES	#5/CONT. 6'+HT (MATURE)	AS SHOWN	113
	MARE	Mahonia repens	Creeping Oregon Grape	YES	#1/CONT.	AS SHOWN	462
	GASH	Gaultheria shallon	Salal	YES	#1/CONT.	AS SHOWN	100
	VAOV	Vaccinium ovatum	Evergreen Huckleberry	YES	#5/CONT. 6'+HT (MATURE)	AS SHOWN	609
GROUNDCOVER AND GRASSES							
	ARUV	Arctostaphylos uva-ursi	Kinnikinnick	YES	#1/CONT.	18" O.C.	1430
	DECE	Deschampsia cespitosa	Tufted Hairgrass	YES	#3/CONT.	AS SHOWN	146
	FRCH	Fragaria chiloensis	Coast Strawberry	YES	#1/CONT.	18" O.C.	2023
	HEMI	Heuchera micrantha	Smallflowered Alumroot	YES	#1/CONT.	AS SHOWN	159
STORMWATER PLANTINGS – 5,233 SF							
	CADE	Carex densa	Dense Sedge	YES	#1/CONT.	12" O.C.	
	DECE	Deschampsia cespitosa	Tufted Hair Grass	YES	#1/CONT.	12" O.C.	
	JUPA	Juncus patens	Spreading Rush	YES	#1/CONT.	12" O.C.	

THE CANNERY

1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL
 PLANTING SCHEDULE
 LEVEL 1

REVISIONS

SCALE
 DRAWN BY
 DATE 2024.03.15
 PROJECT NO. 2346-SAC

SHEET

L204

LEGEND

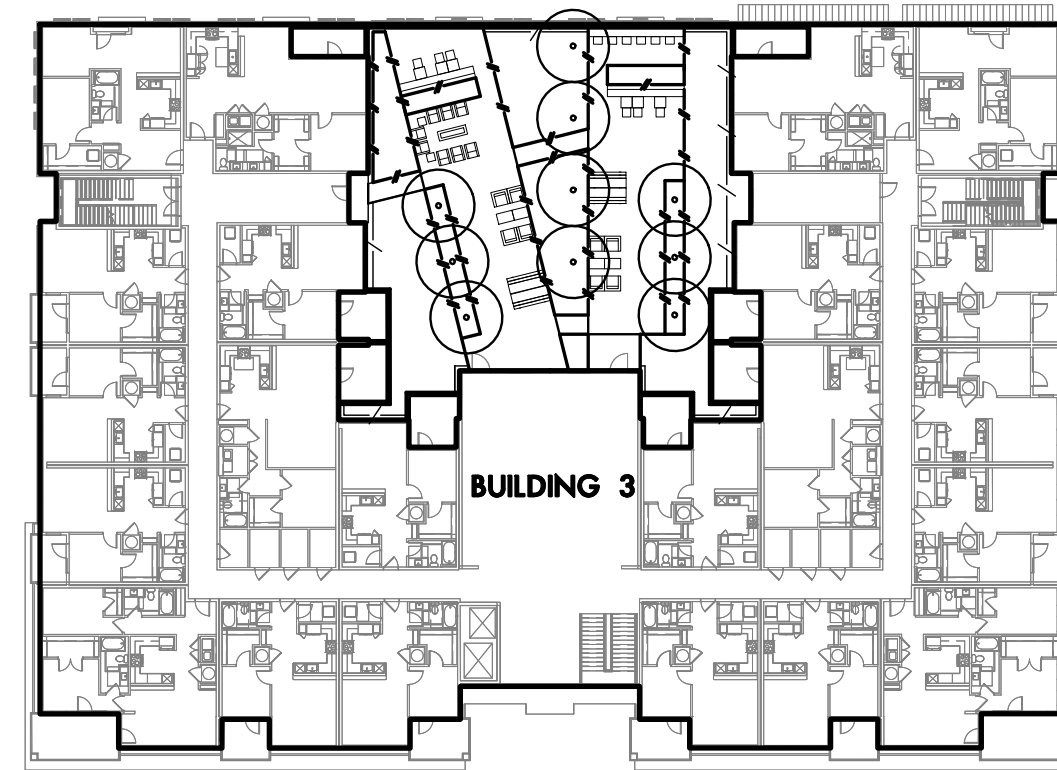
- — — — — LIMIT OF WORK
- - - - - METAL EDGE
- / / / — — RAISED PLANTER
- · · · · · SCREEN FENCE

PLANTING NOTES

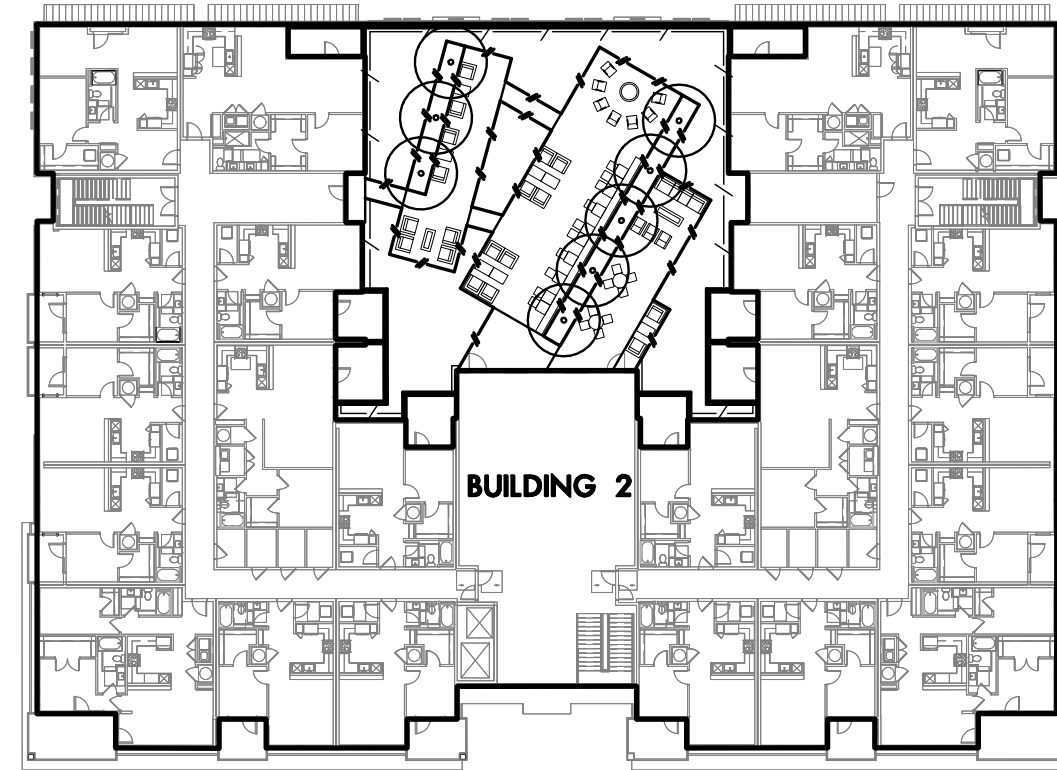
1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.

LANDSCAPE ARCHITECTS P.C.
lango. hansen
 1100 NW GILMAN #3A PORTLAND OR 97209 T 503.295.2437

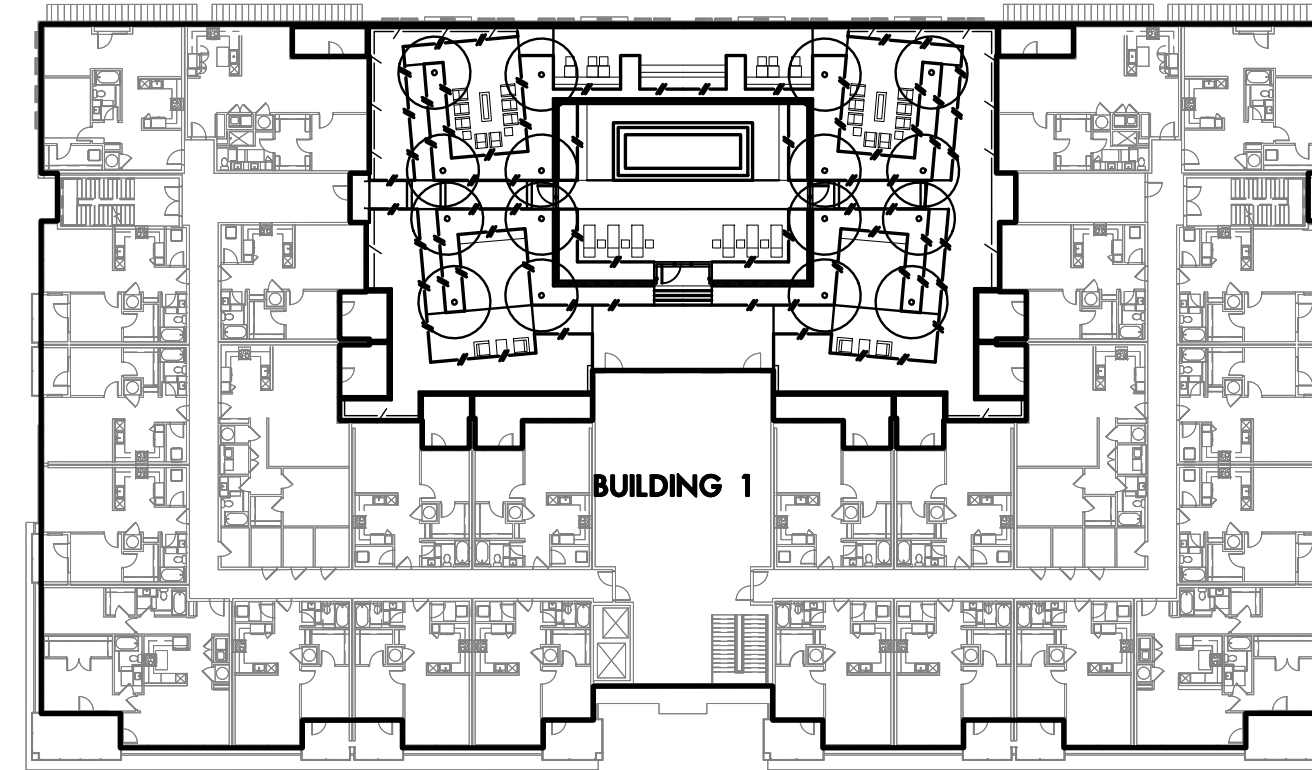
L208 — PLANTING PLAN LEVEL 2 BUILDING 3



L207 — PLANTING PLAN LEVEL 2 BUILDING 2

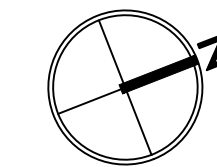
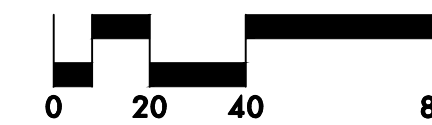


L206 — PLANTING PLAN LEVEL 2 BUILDING 1



1 PLANTING PLAN LEVEL 2 OVERALL

Plan
SCALE: 1" = 40'



THE CANNERY

1105 FRONT ST NE,
 SALEM, OR 97301

LAND USE SUBMITTAL

PLANTING PLAN LEVEL 2
OVERALL

REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

SHEET

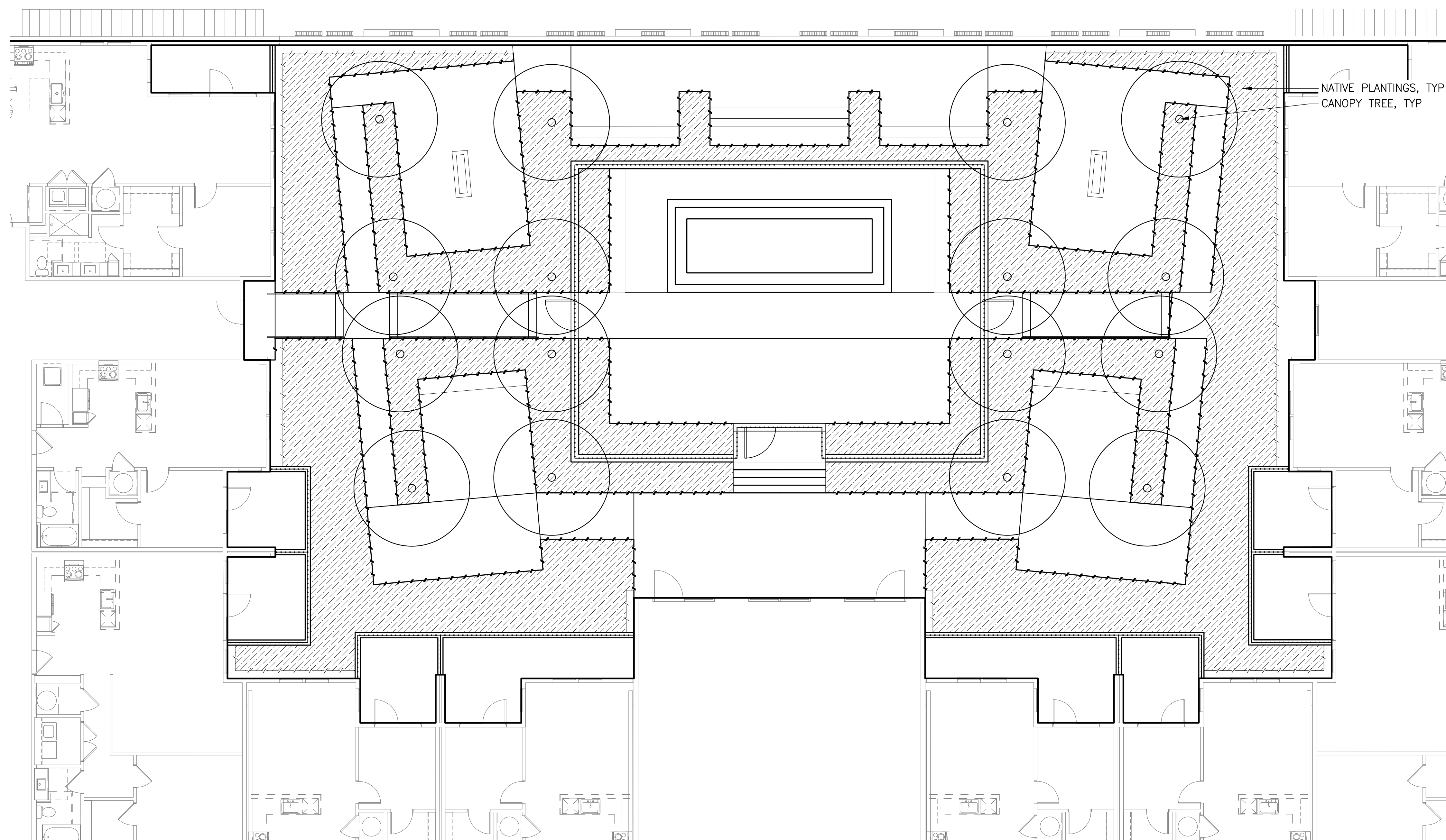
L205

LEGEND

- — — — — LIMIT OF WORK
- / — / — / — METAL EDGE
- / — / — / — / — RAISED PLANTER
- — — — — SCREEN FENCE
- ▨ NATIVE PLANTINGS
- ● CANOPY TREE

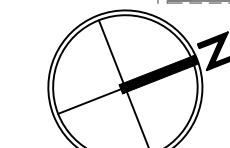
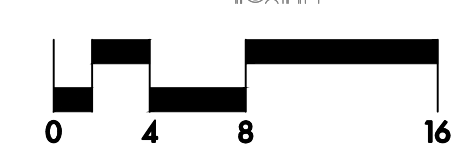
PLANTING NOTES

1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.



1 PLANTING PLAN LEVEL 2 BUILDING 1

Plan
SCALE: 1/8" = 1'-0"



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL

PLANTING PLAN LEVEL 2
BUILDING 1

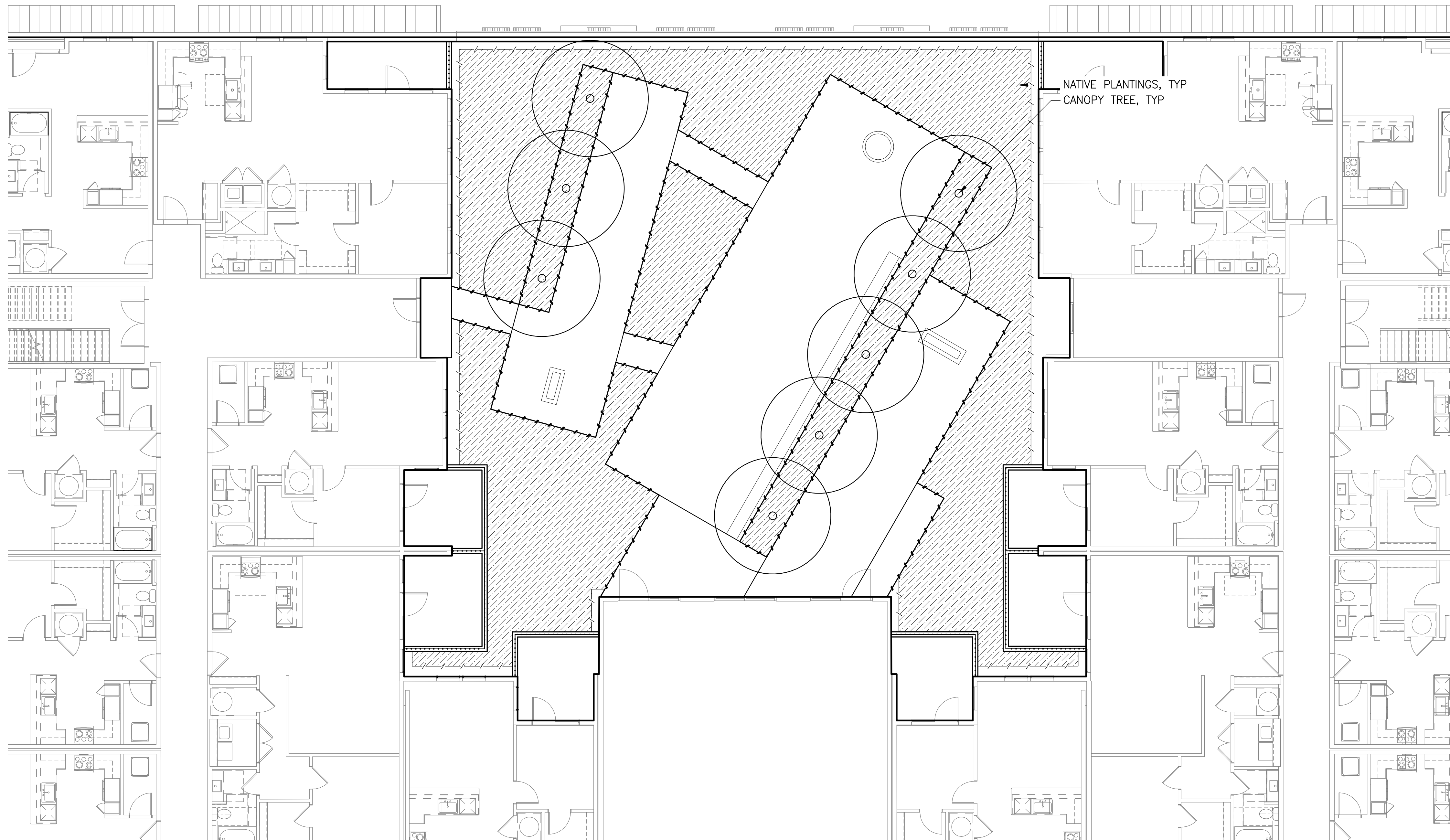
REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

SHEET

L206

LANDSCAPE ARCHITECTS PC
lango.hansen
1100 nw glisan #3A portland OR 97209 T 503.295.2437



NATIVE PLANTINGS, TYP
CANOPY TREE, TYP

LEGEND

- LIMIT OF WORK
- - - METAL EDGE
- - - RAISED PLANTER
- - - SCREEN FENCE
- ▨ NATIVE PLANTINGS
- CANOPY TREE

PLANTING NOTES

1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.

1 PLANTING PLAN LEVEL 2 BUILDING 2

Plan
SCALE: 1/8" = 1'-0"



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LANDSCAPE ARCHITECTS PC
lango. hansen
1100 nw glisan #3A portland OR 97209 T 503.295.2437

LAND USE SUBMITTAL
PLANTING PLAN LEVEL 2
BUILDING 2

REVISIONS

SCALE
DRAWN BY
DATE 2024.03.15
PROJECT NO. 2346-SAC

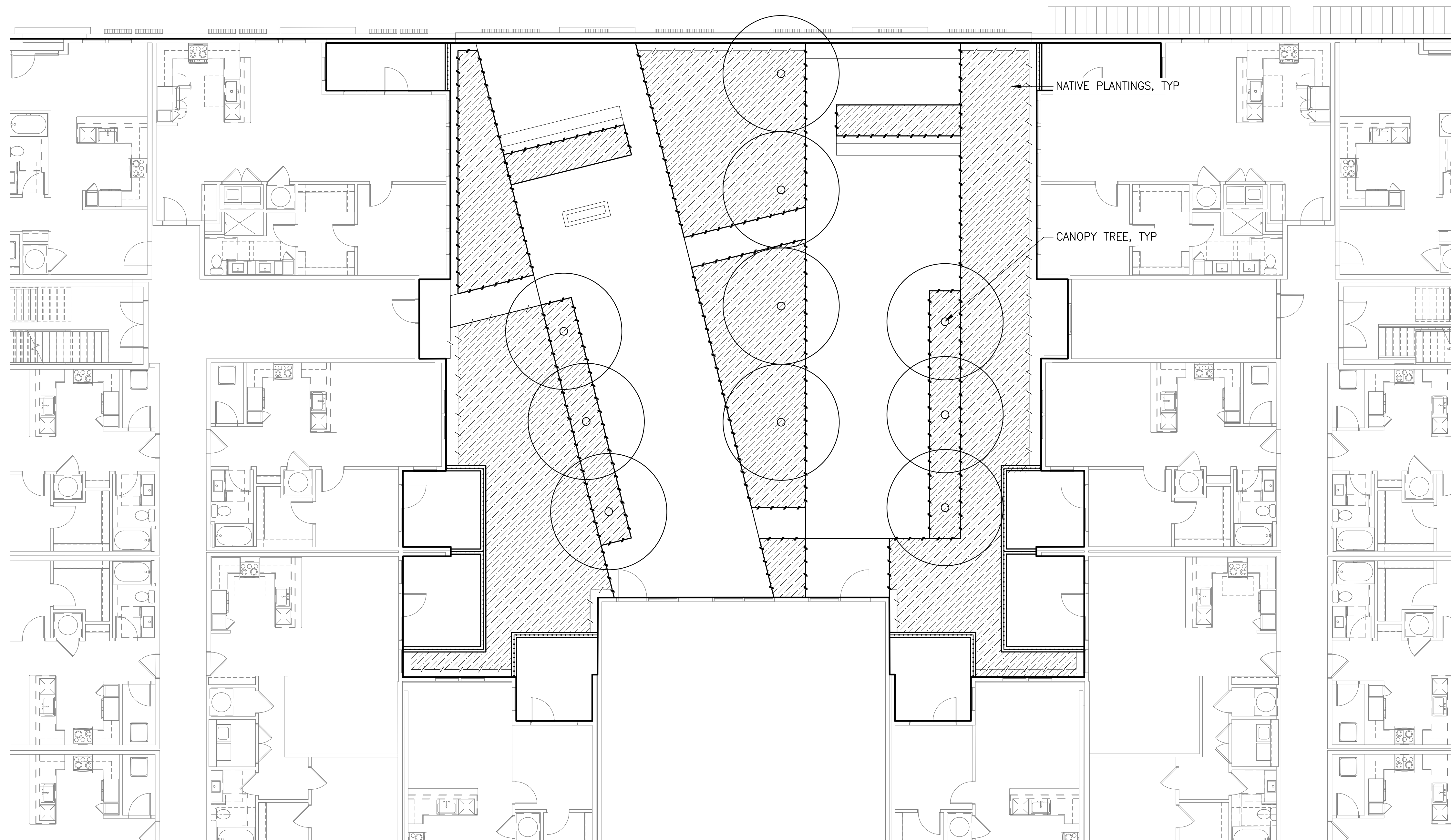
SHEET
L207

LEGEND

- — — — — LIMIT OF WORK
- - - - - METAL EDGE
- - - - - RAISED PLANTER
- - - - - SCREEN FENCE
- ▨ NATIVE PLANTINGS
- CANOPY TREE

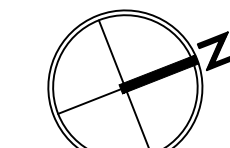
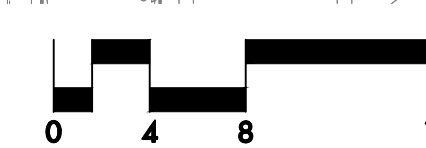
PLANTING NOTES

1. SEE L201 FOR GENERAL NOTES AND ABBREVIATIONS.



1 PLANTING PLAN LEVEL 2 BUILDING 3

Plan
SCALE: 1/8" = 1'-0"



THE CANNERY

1105 FRONT ST NE,
SALEM, OR 97301

LAND USE SUBMITTAL

PLANTING PLAN LEVEL 2
BUILDING 3

REVISIONS

SCALE
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PROJECT NO. 2346-SAC

SHEET

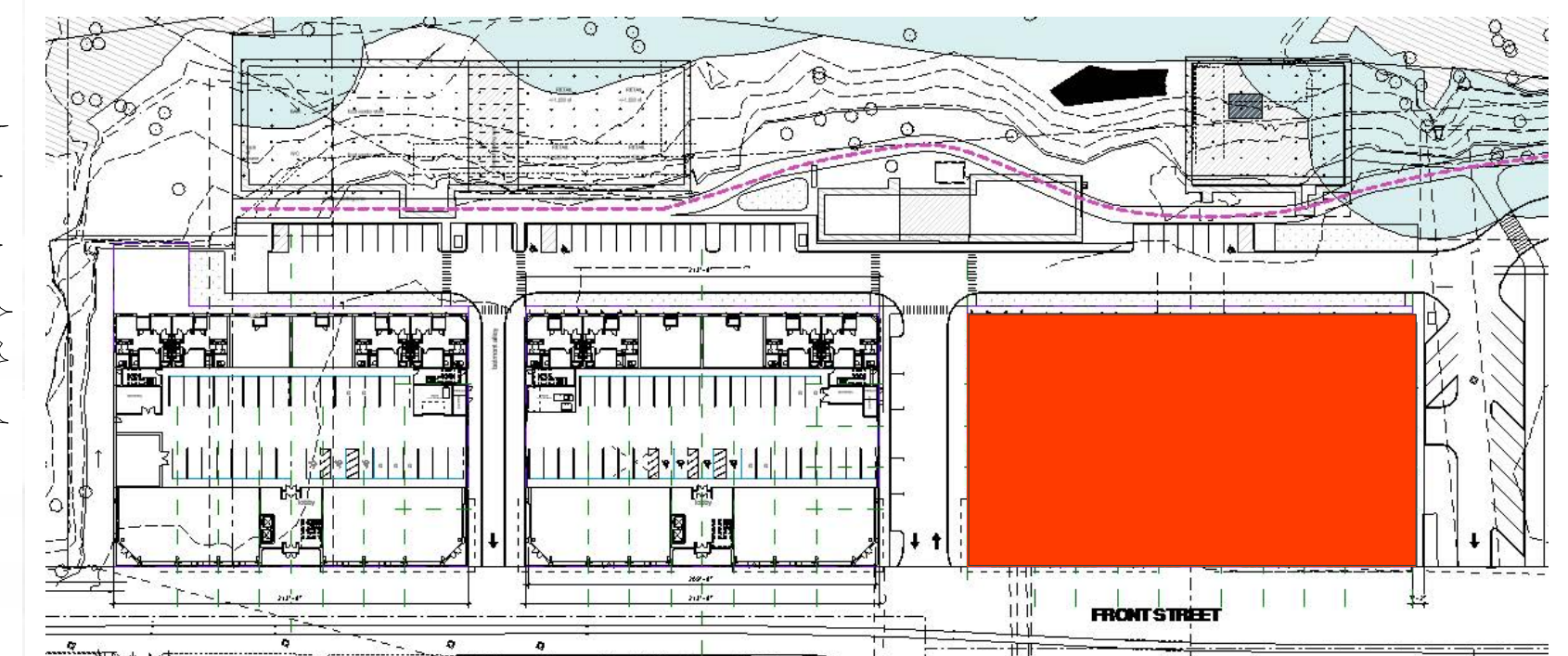
L208

Attachment E: Revised Preliminary Building Elevations and Floor Plans



FLOOR	BLDG I	BLDG II	BLDG III
1ST	8	6	6
2ND	26	22	22
3RD	27	23	23
4TH	27	23	23
5TH	27	23	23
6TH	27	23	23
TOTAL	142	120	120

TOTAL NO. OF UNITS: 382





#	REVISION	DATE
1	Revision 1	05/07/2024

81% GLAZING*
91% WEATHERPROTECTION

*Calculated per SRC 112.030 (b)



#	REVISION	DATE
2	Unit Changes	06/19/2024

60% GLAZING*
56% WEATHERPROTECTION

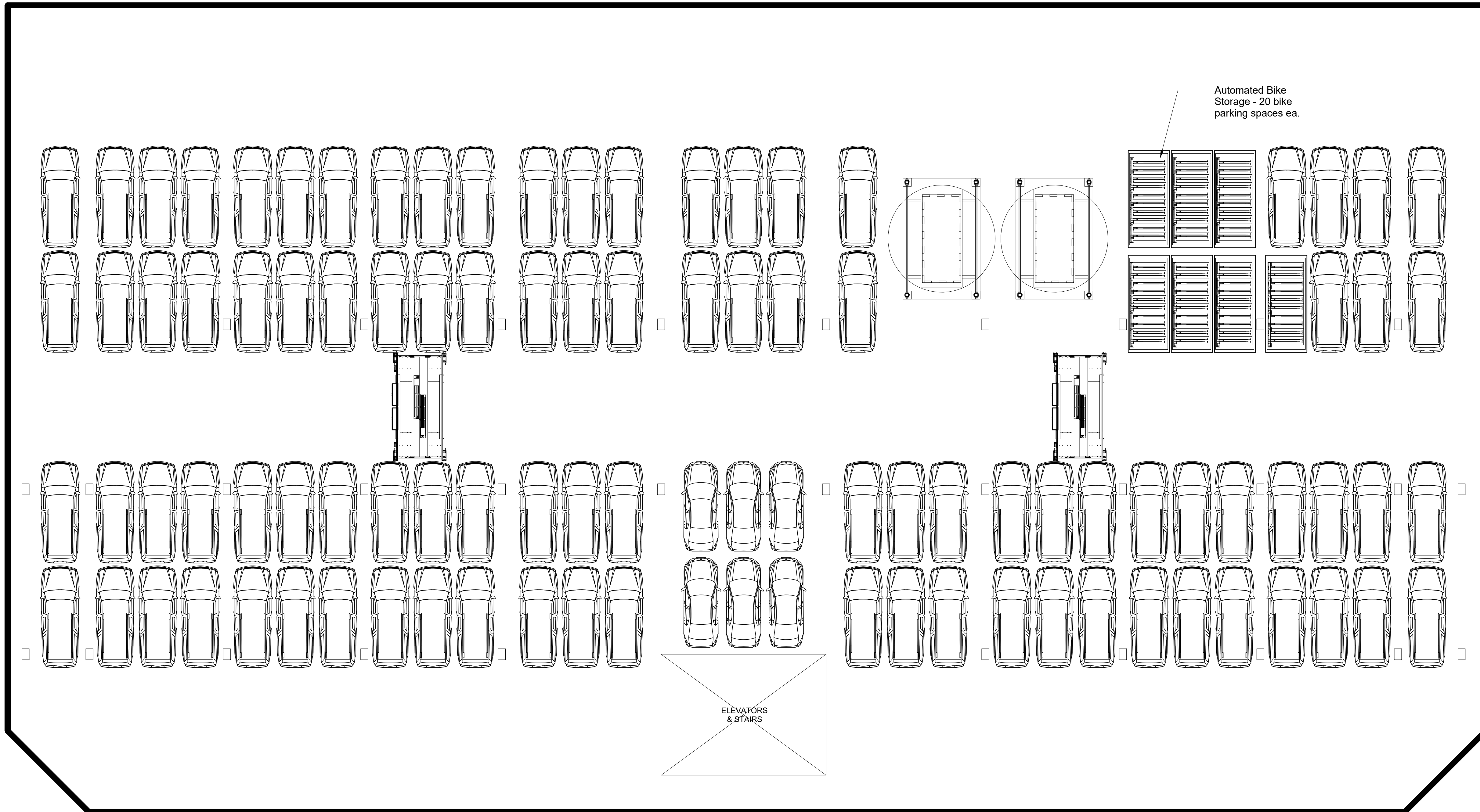
*Calculated per SRC 112.030 (b)



#	REVISION	DATE
2	Unit Changes	06/19/2024

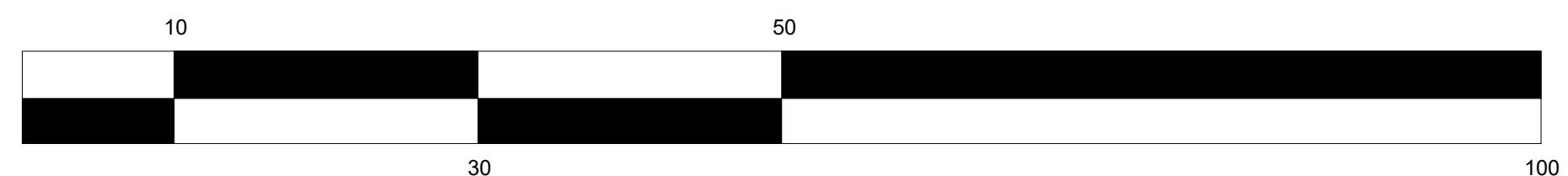


#	REVISION	DATE
2	Unit Changes	06/19/2024

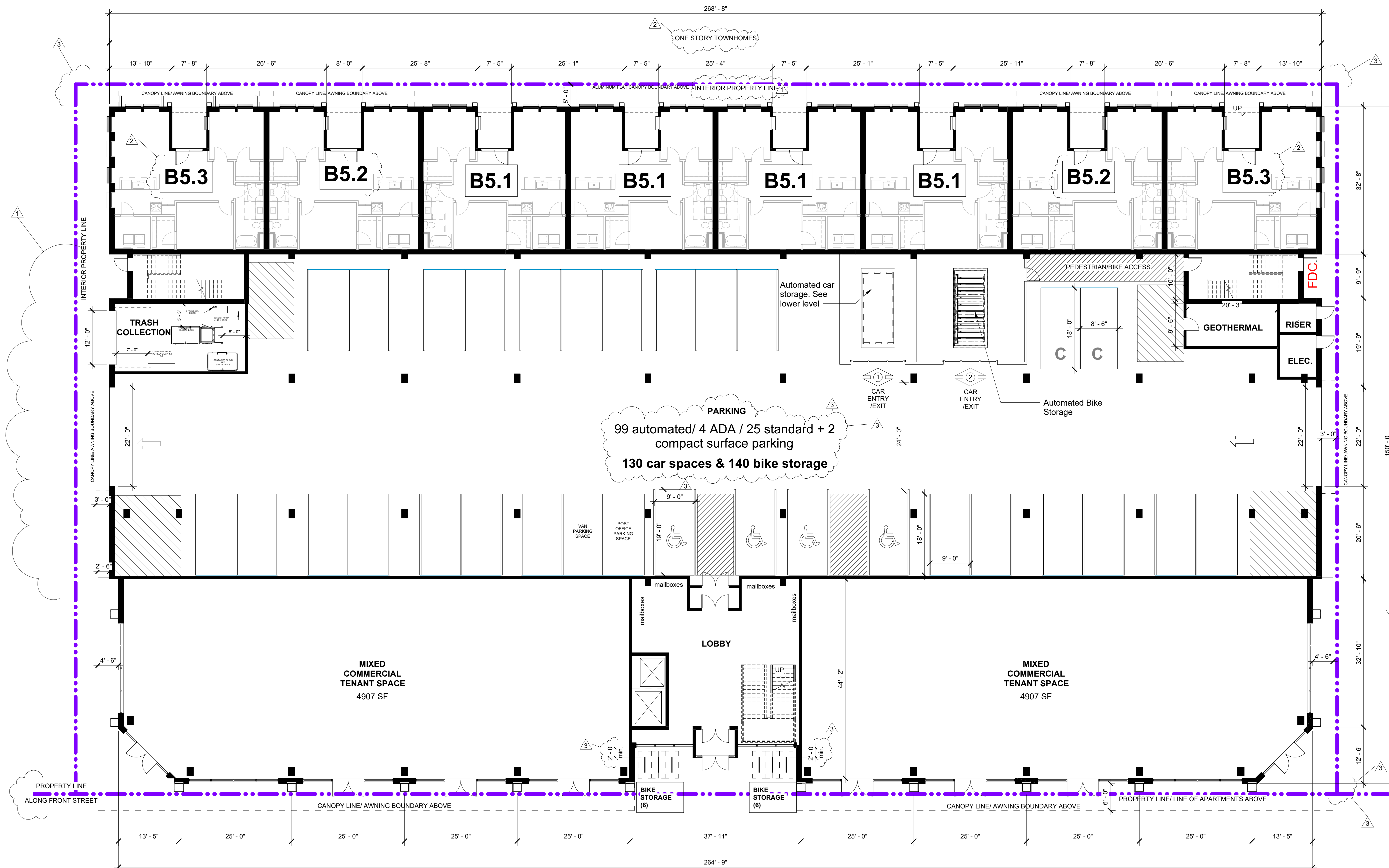


Automated Bike Storage - 20 bike parking spaces ea.

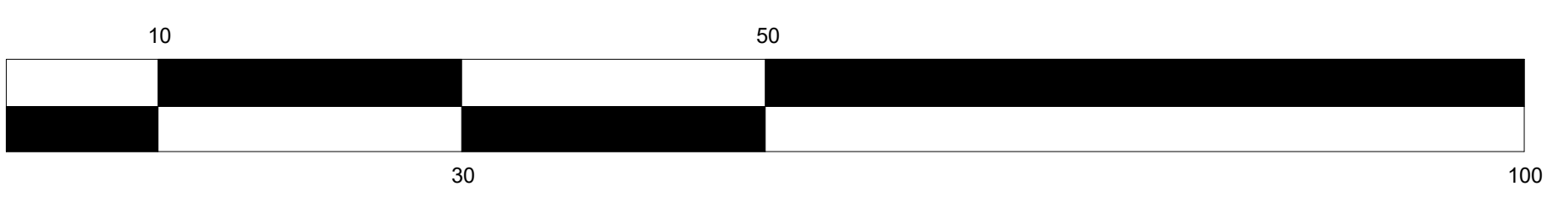
ELEVATORS & STAIRS



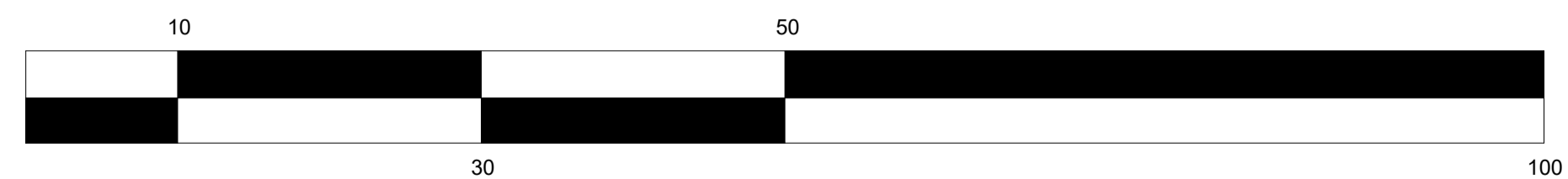
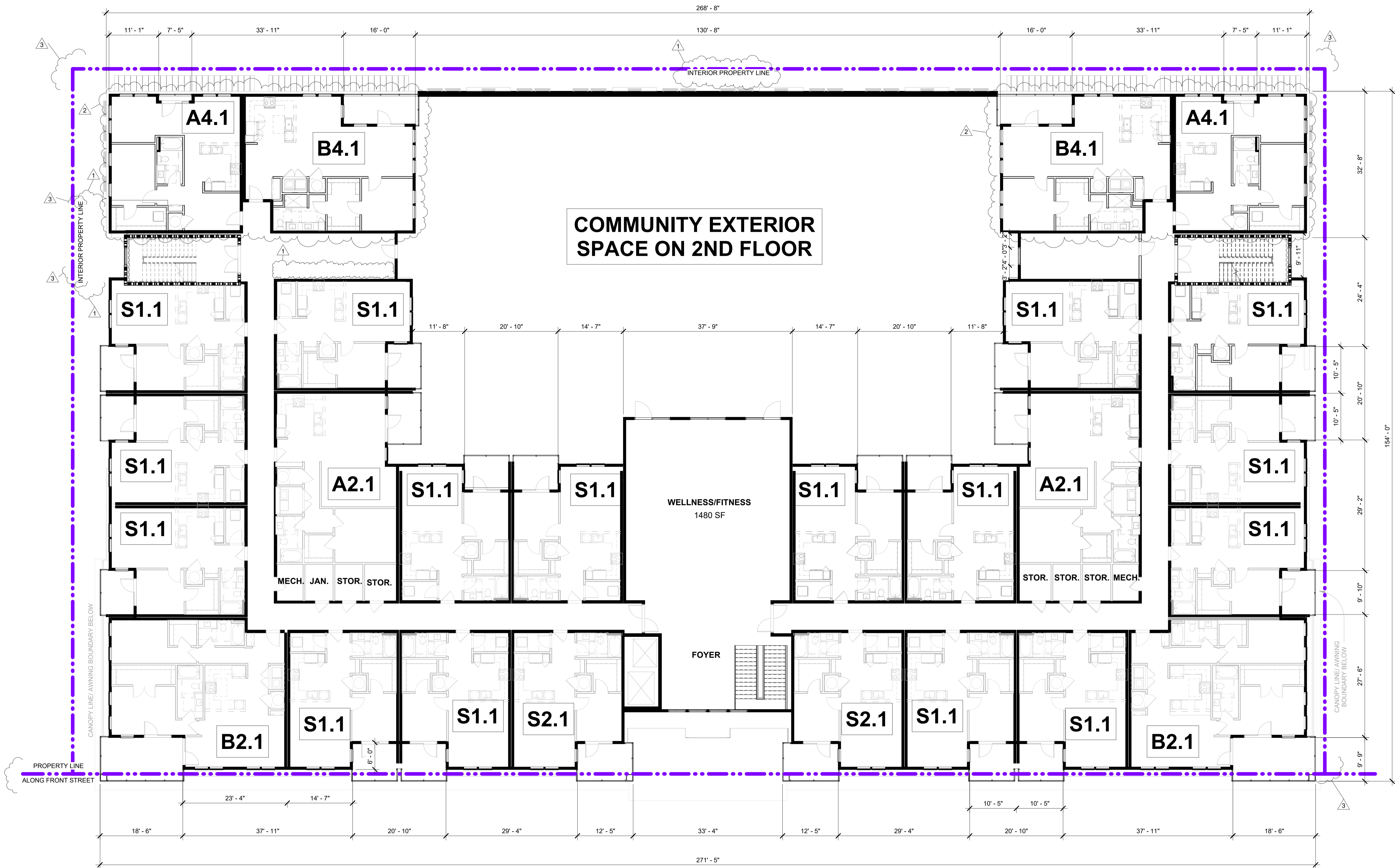
#	REVISION	DATE
1	Revision 1	05/07/2024



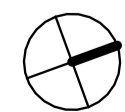
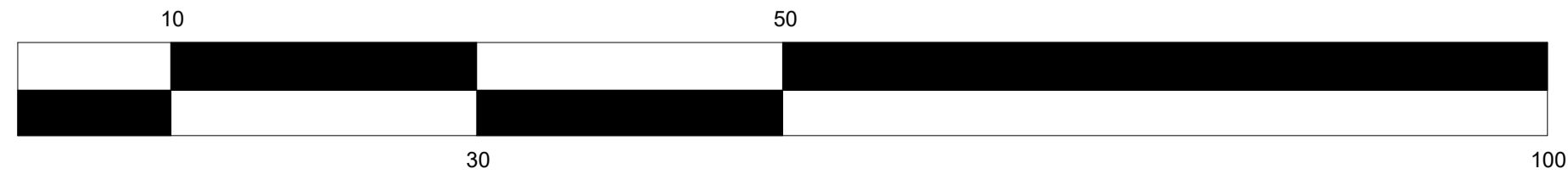
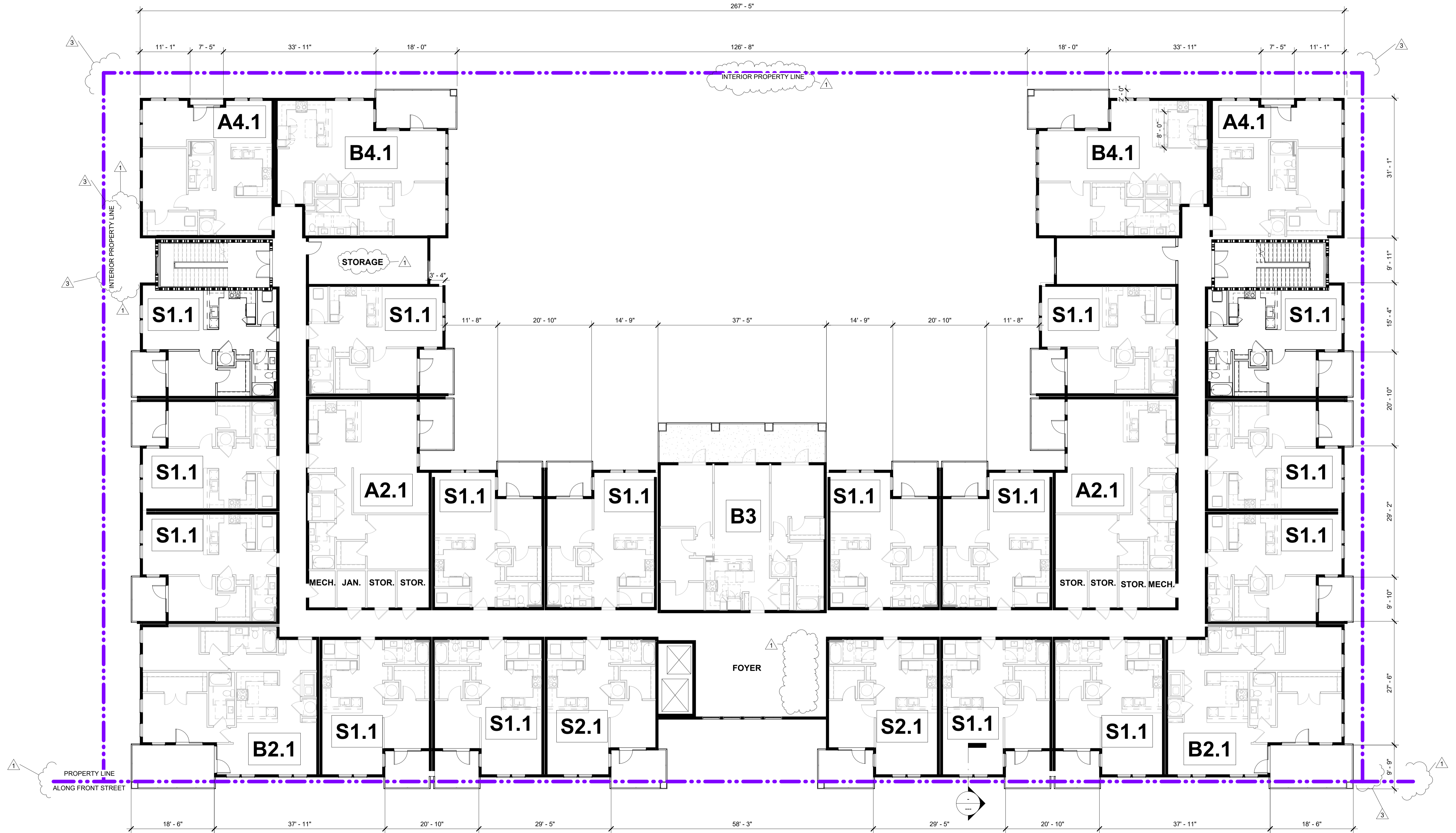
PARKING
 99 automated / 4 ADA / 25 standard + 2 compact surface parking
 130 car spaces & 140 bike storage



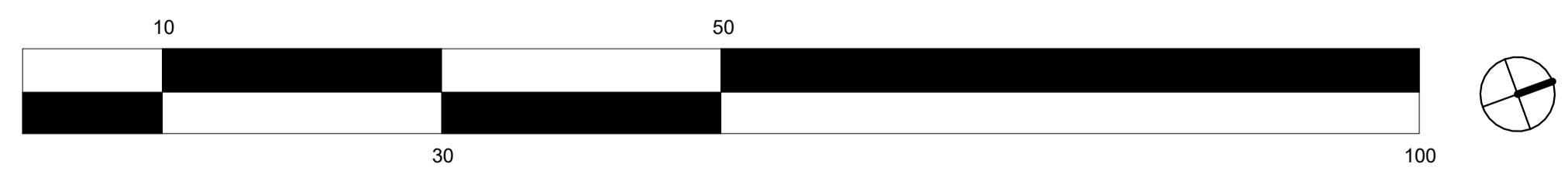
#	REVISION	DATE
1	Revision 1	05/07/2024
2	Unit Changes	06/19/2024
3	Revision 3	07/23/2024



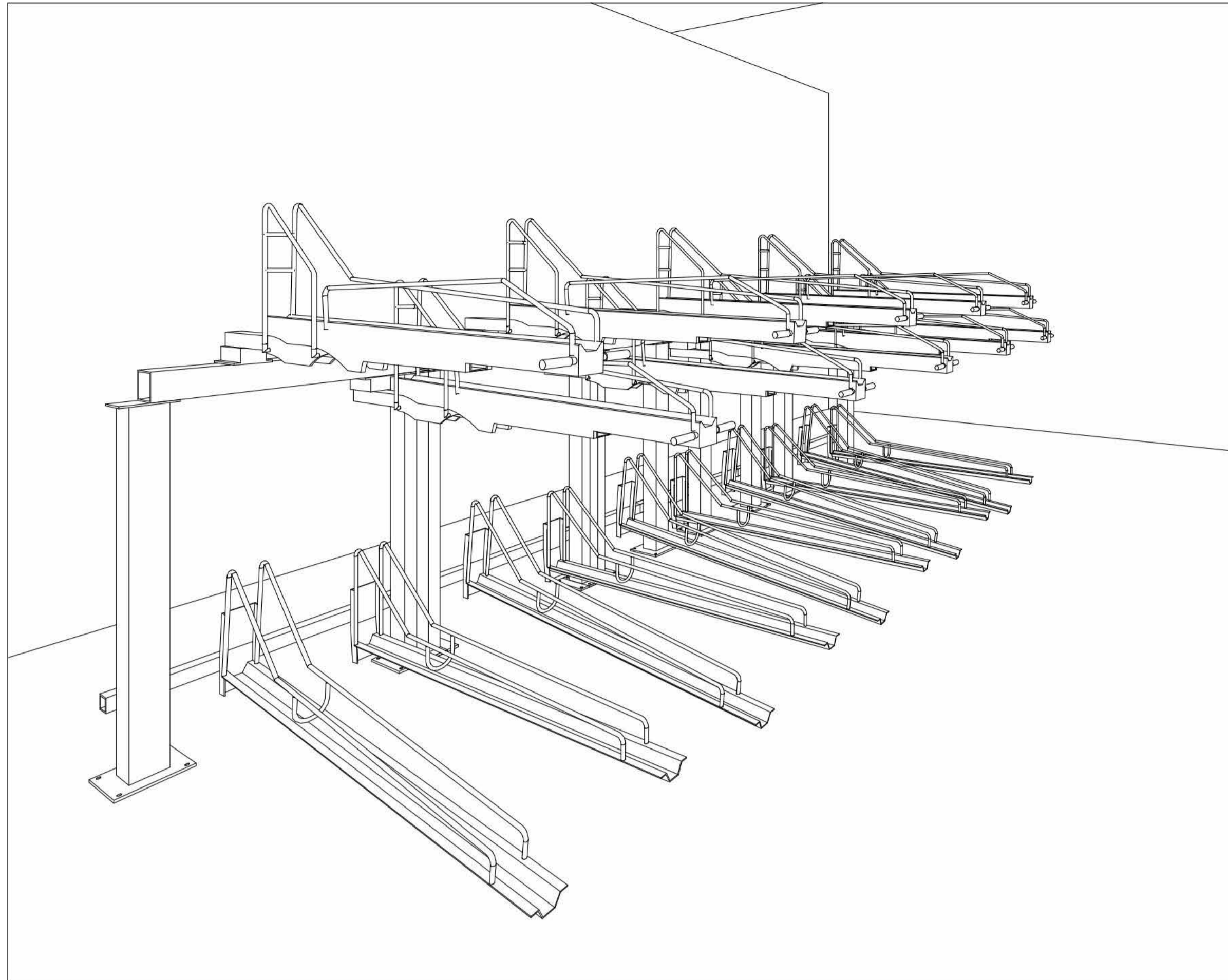
#	REVISION	DATE
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2	Unit Changes	06/19/2024
3	Revision 3	07/23/2024



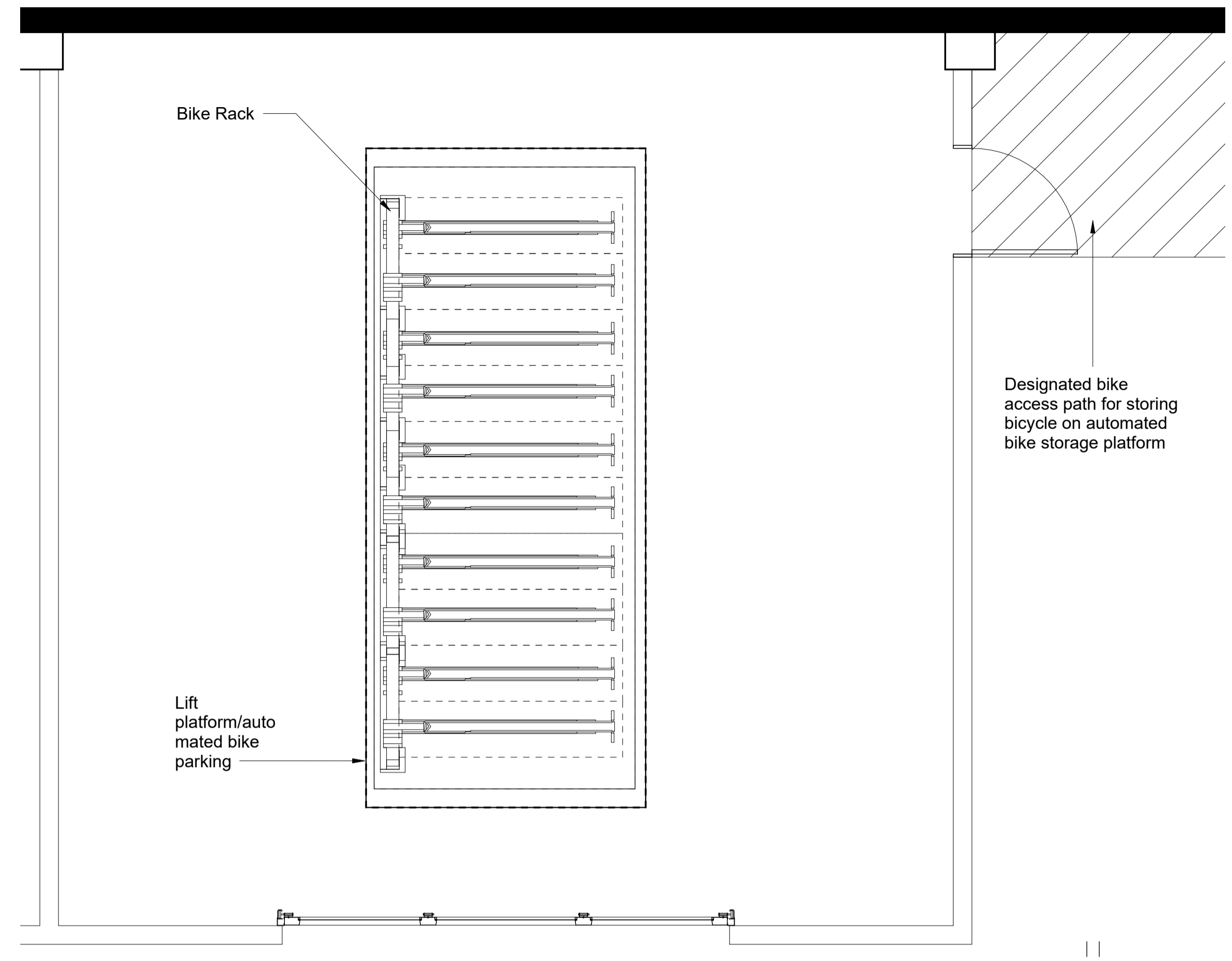
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3	Revision 3	07/23/2024



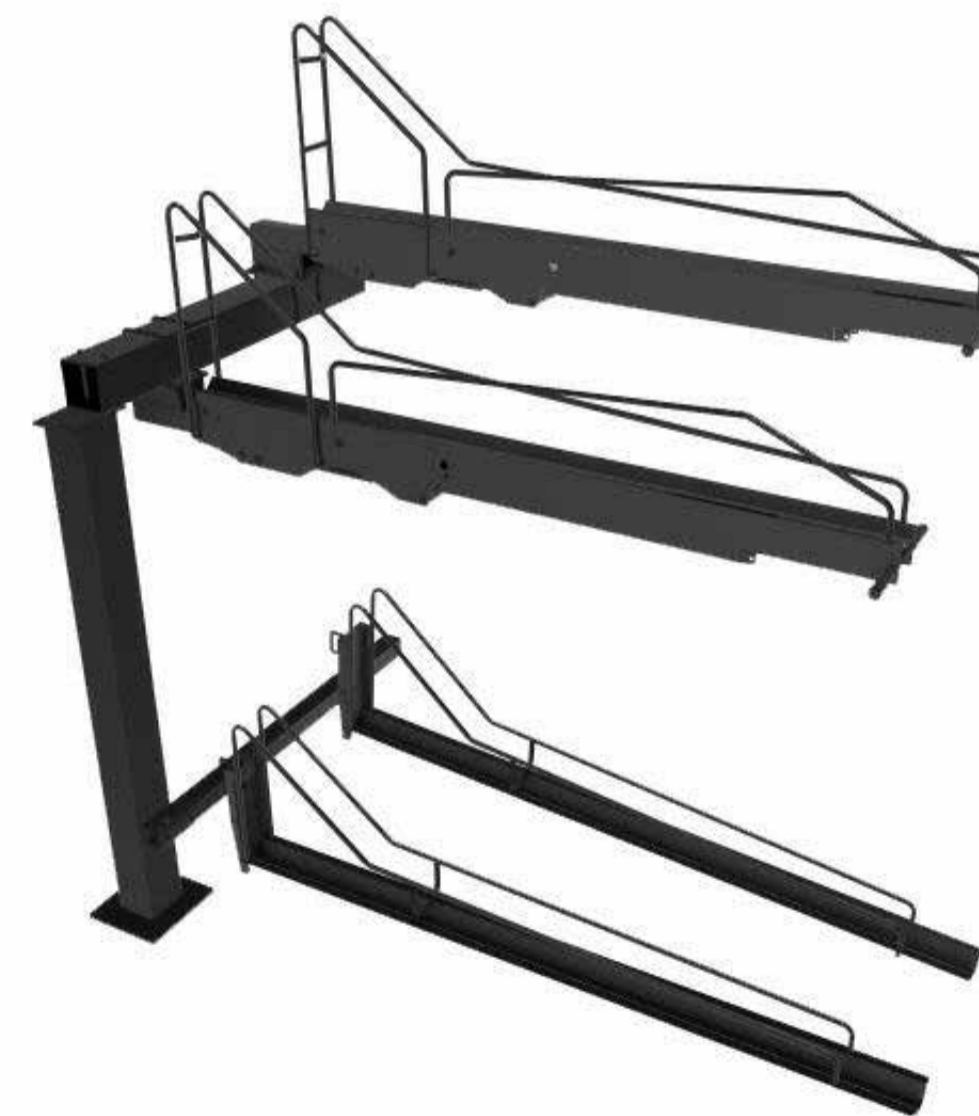
#	REVISION	DATE
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3	Revision 3	07/23/2024



3D SKETCH OF BIKE STORAGE PLATFORM - 20 BIKES PER CAR SPACE.



1 ENLARGED BIKE PLAN
3/8" = 1'-0"



BIKE BOOST STORAGE SYSTEM BY MADDRAX



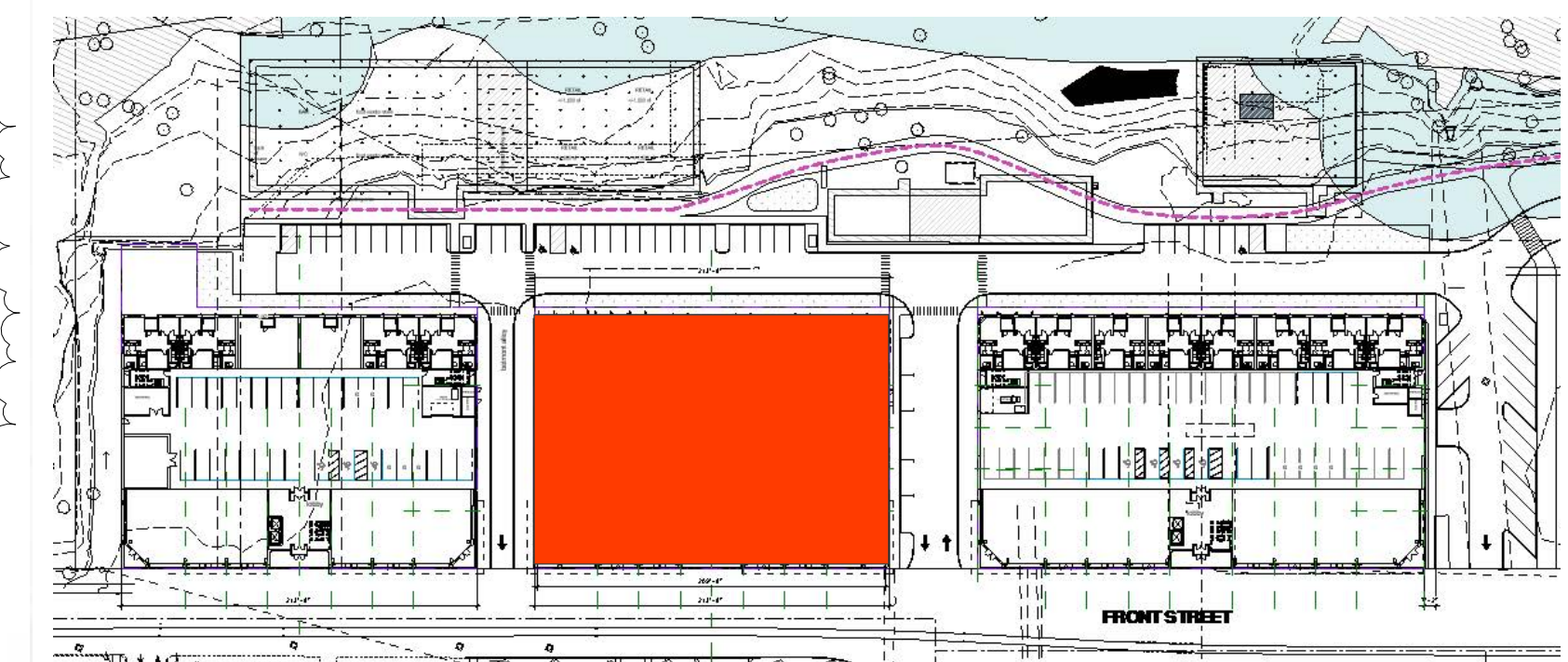
<https://www.madrax.com/bike-boost-storage-bbs>

#	REVISION	DATE
1	Revision 1	05/07/2024



FLOOR	BLDG I	BLDG II	BLDG III
1ST	8	6	6
2ND	26	22	22
3RD	27	23	23
4TH	27	23	23
5TH	27	23	23
6TH	27	23	23
TOTAL	142	120	120

TOTAL NO. OF UNITS: 382





REVISION DATE
 1 Revision 1 05/07/2024

83% GLAZING*
 89% WEATHERPROTECTION

*Calculated per SRC 112.030 (b)



#	REVISION	DATE
2	Unit Changes	06/19/2024

56% GLAZING*
59% WEATHERPROTECTION

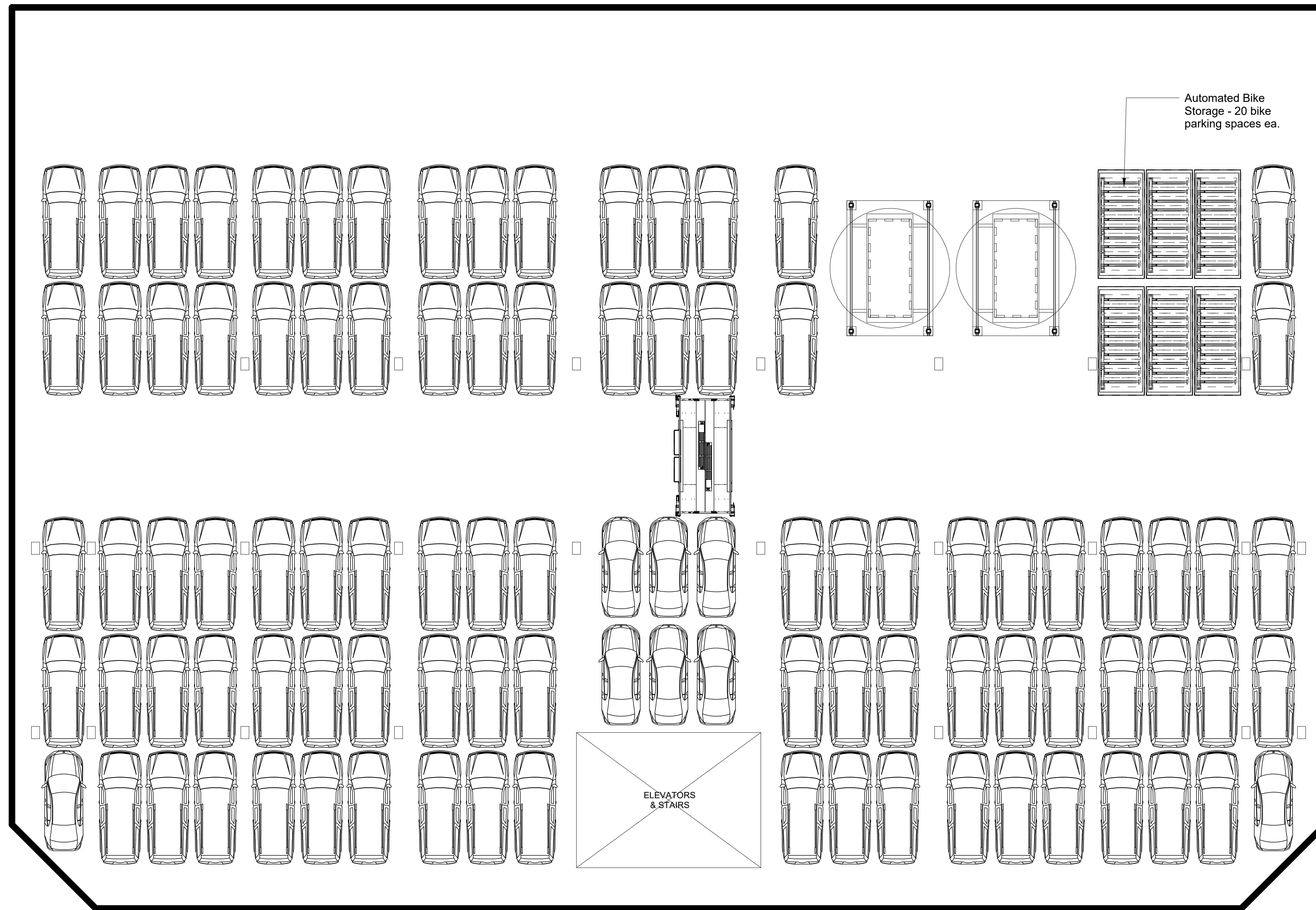
*Calculated per SRC 112.030 (b)



#	REVISION	DATE
2	Unit Changes	06/19/2024

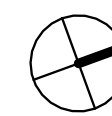
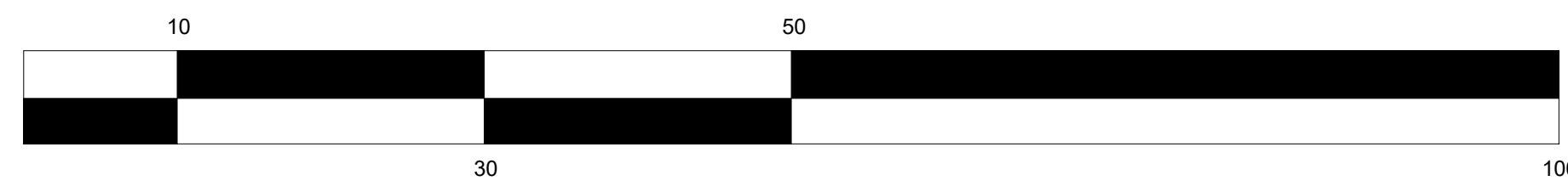


#	REVISION	DATE
2	Unit Changes	06/19/2024



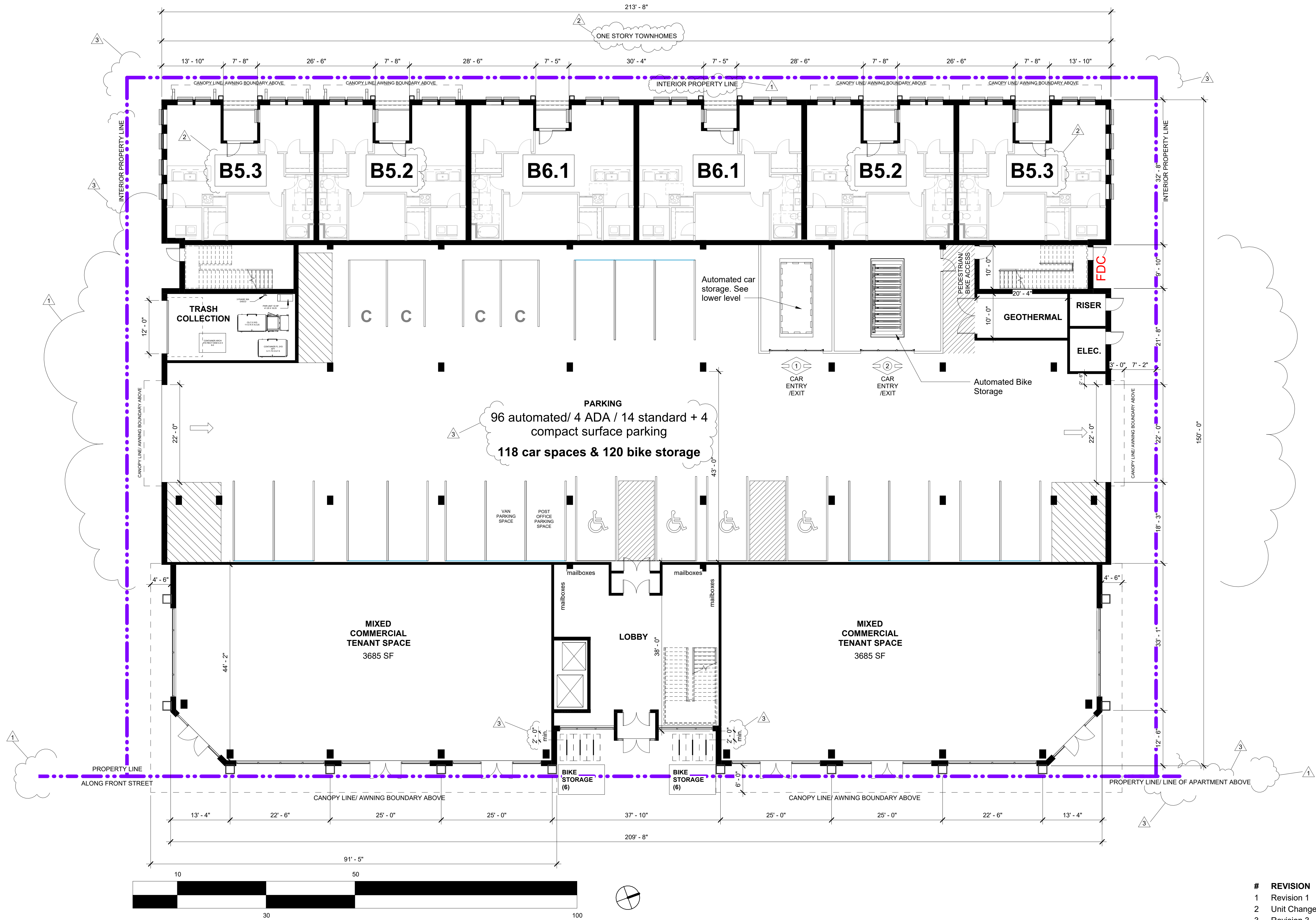
Automated Bike Storage - 20 bike parking spaces ea.

ELEVATORS & STAIRS

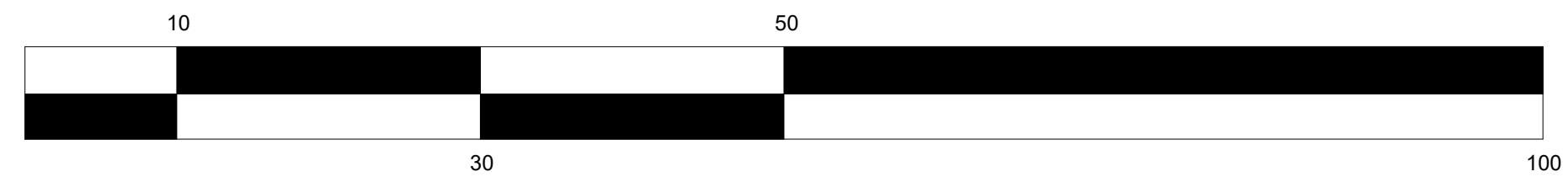
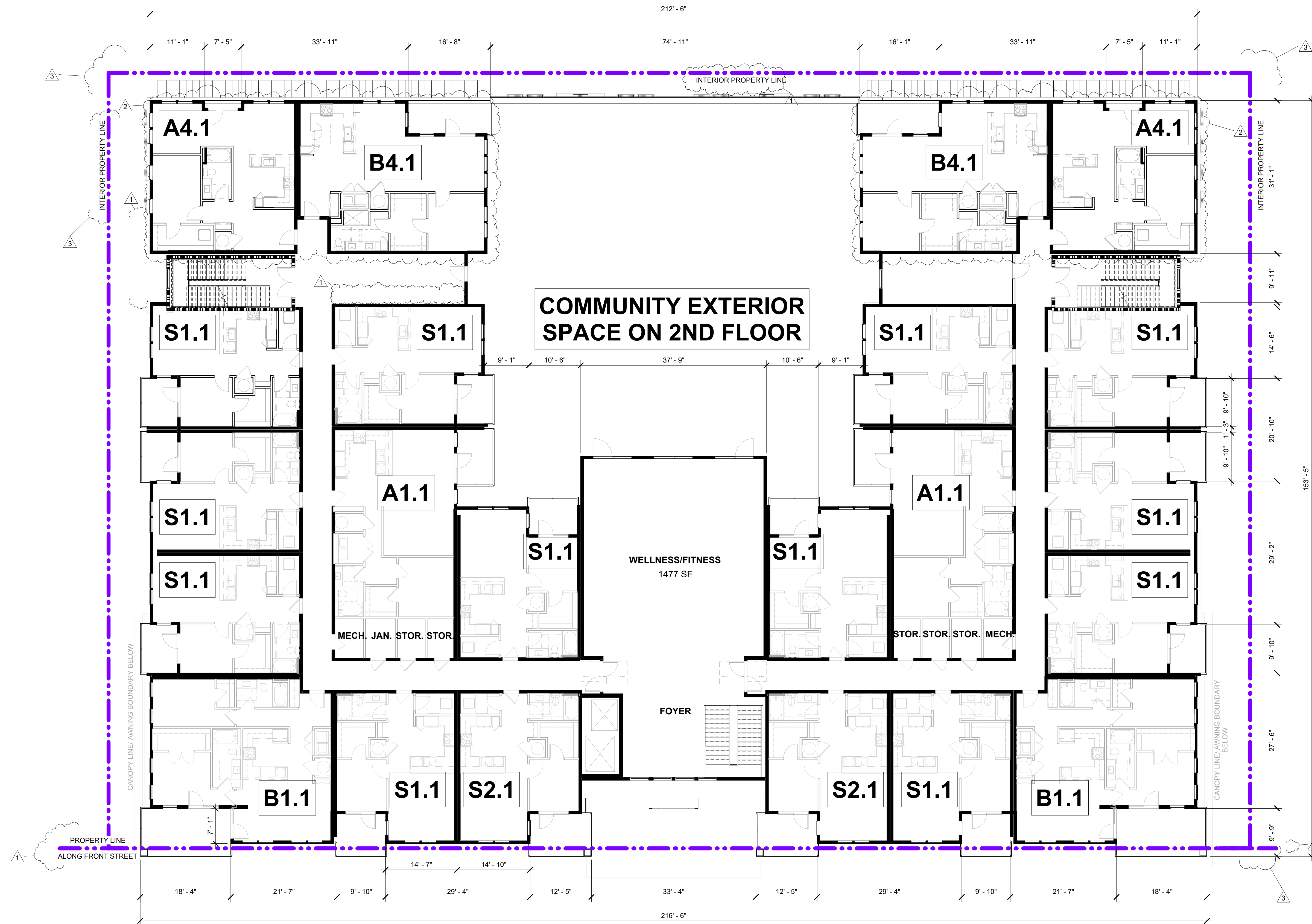


INSIGHT ARCHITECTS

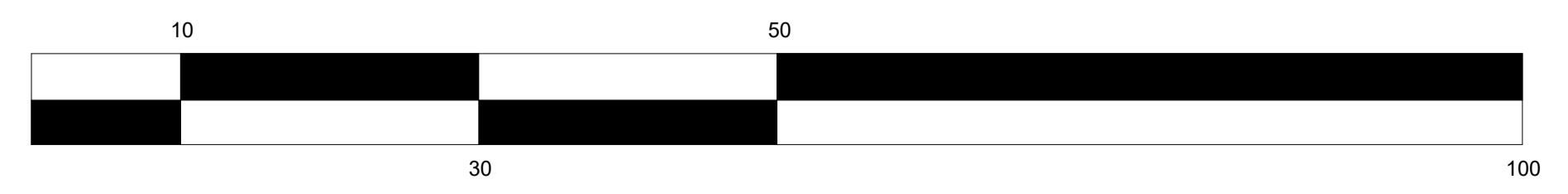
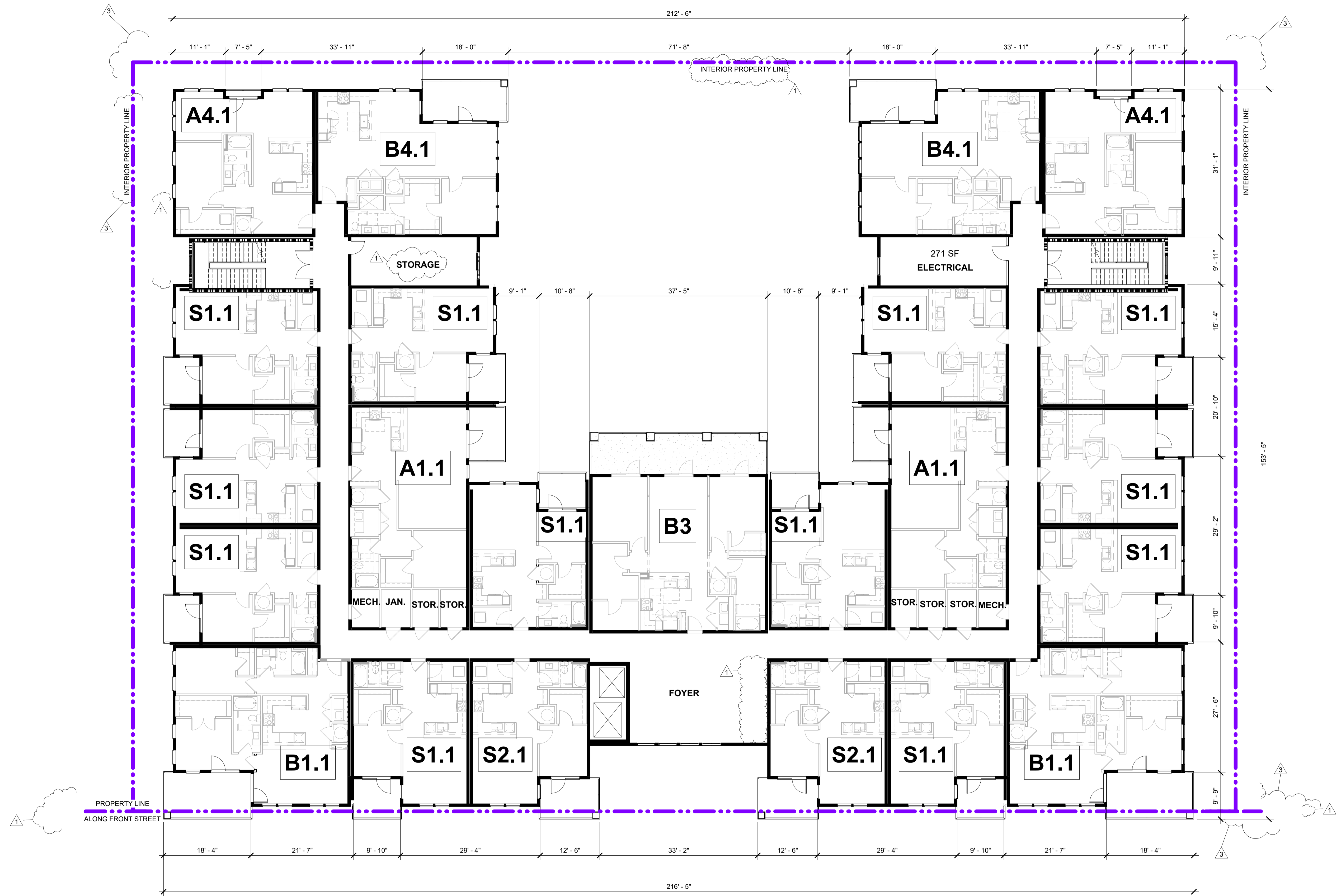
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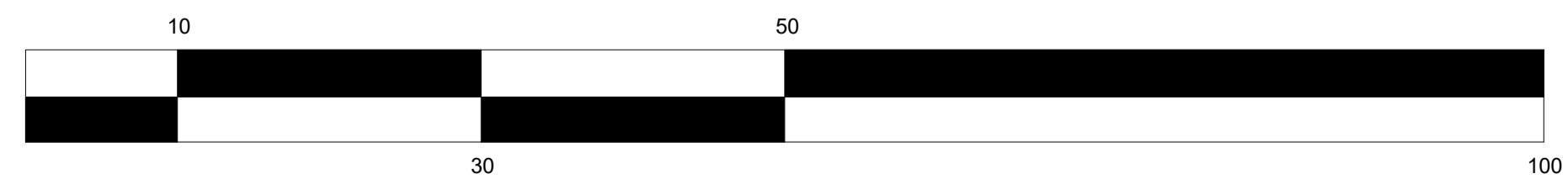
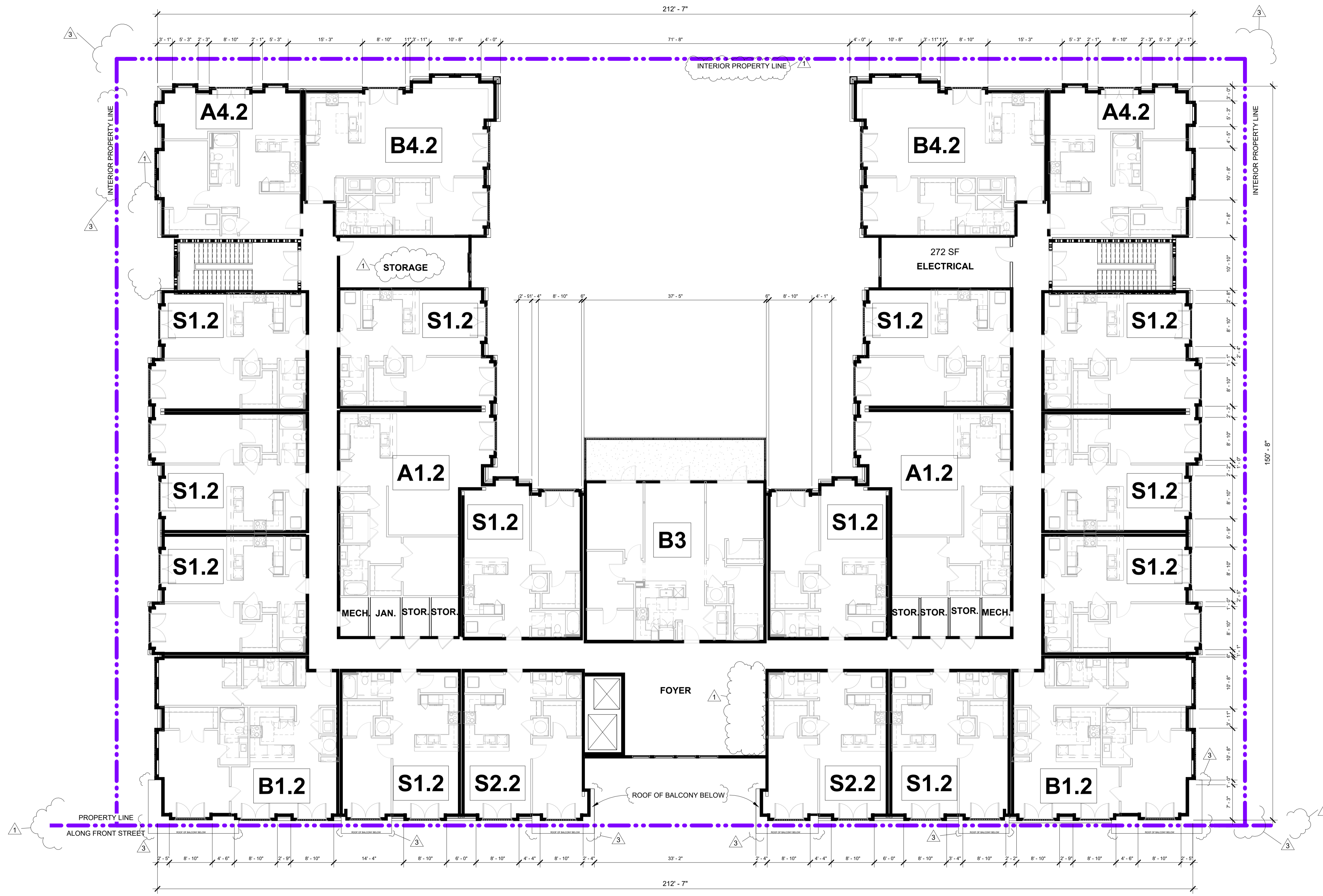
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3	Revision 3	07/23/2024



#	REVISION	DATE
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2	Unit Changes	06/19/2024
3	Revision 3	07/23/2024



#	REVISION	DATE
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3	Revision 3	07/23/2024

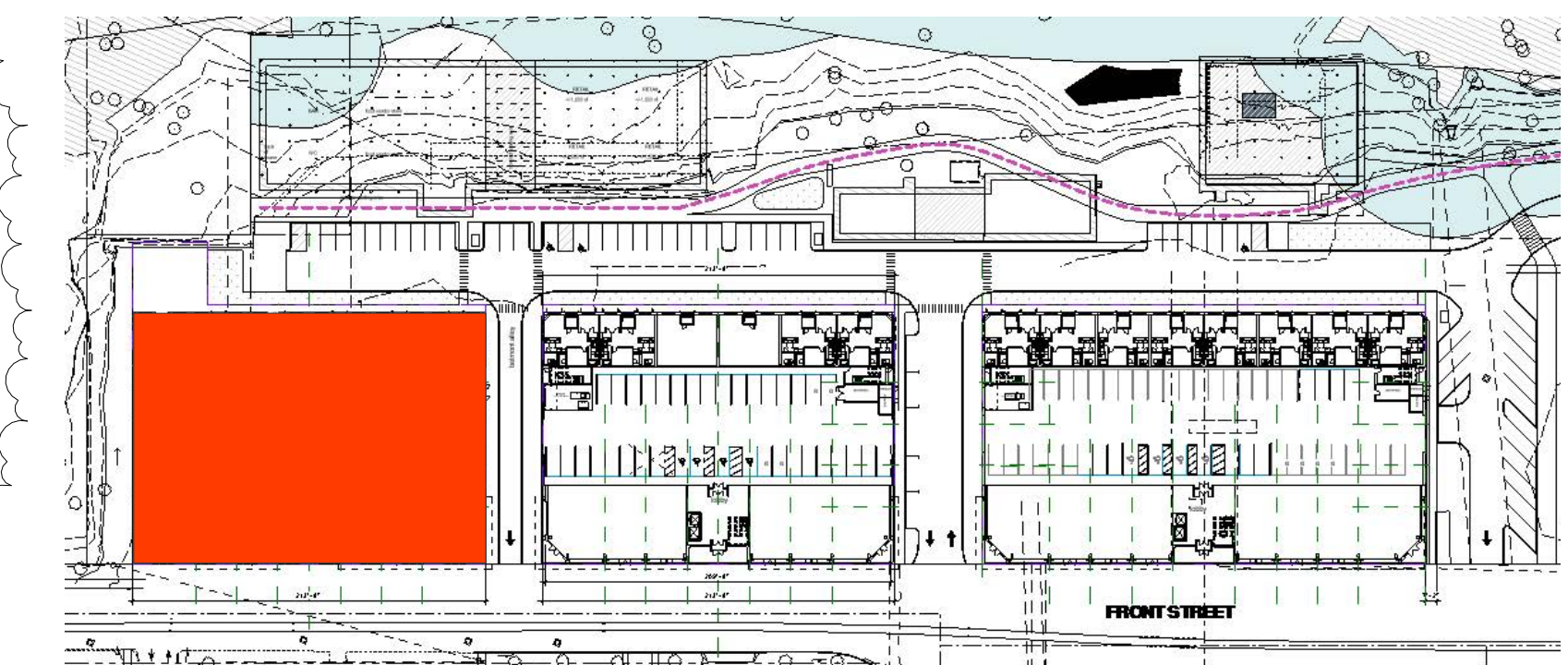


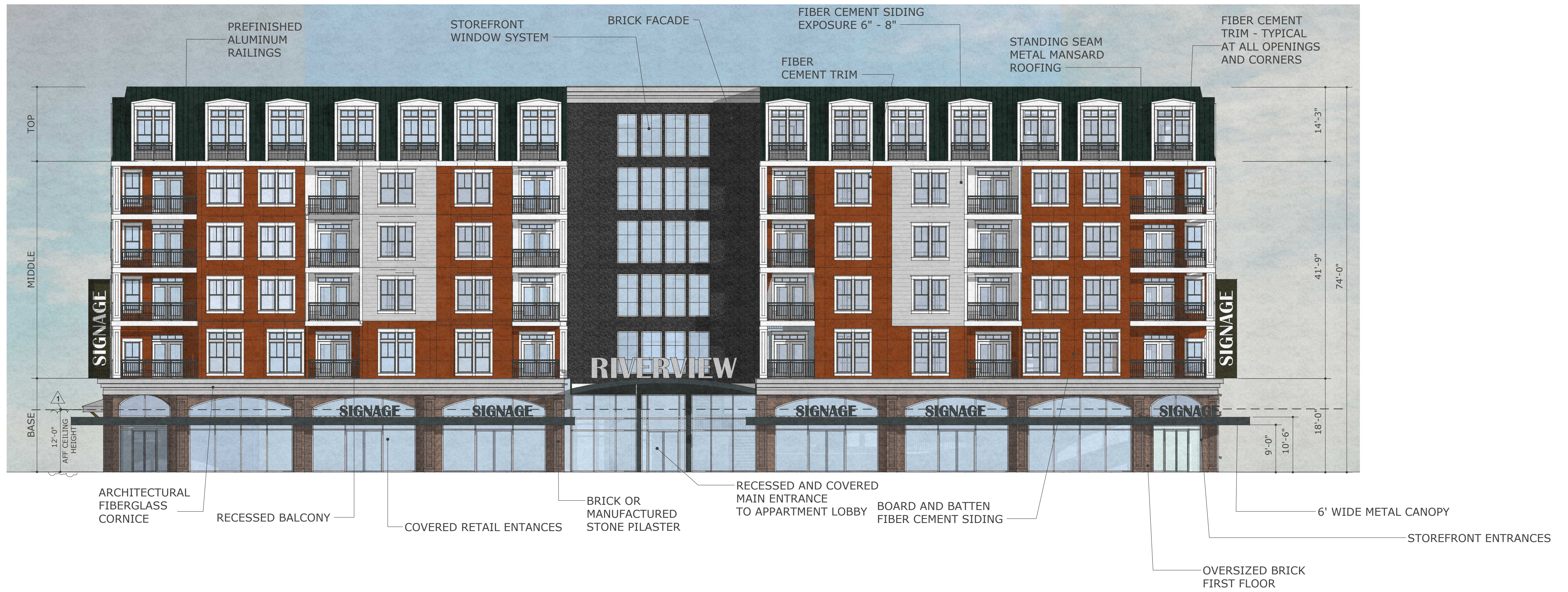
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3	Revision 3	07/23/2024



FLOOR	BLDG I	BLDG II	BLDG III
1ST	8	6	6
2ND	26	22	22
3RD	27	23	23
4TH	27	23	23
5TH	27	23	23
6TH	27	23	23
TOTAL	142	120	120

TOTAL NO. OF UNITS: 382





#	REVISION	DATE
1	Revision 1	05/07/2024

83% GLAZING*
89% WEATHERPROTECTION

*Calculated per SRC 112.030 (b)



#	REVISION	DATE
2	Unit Changes	06/19/2024

52% GLAZING*
59% WEATHERPROTECTION

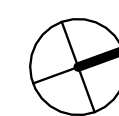
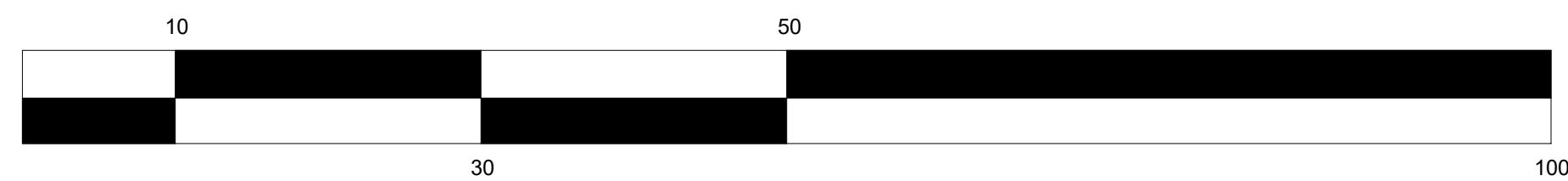
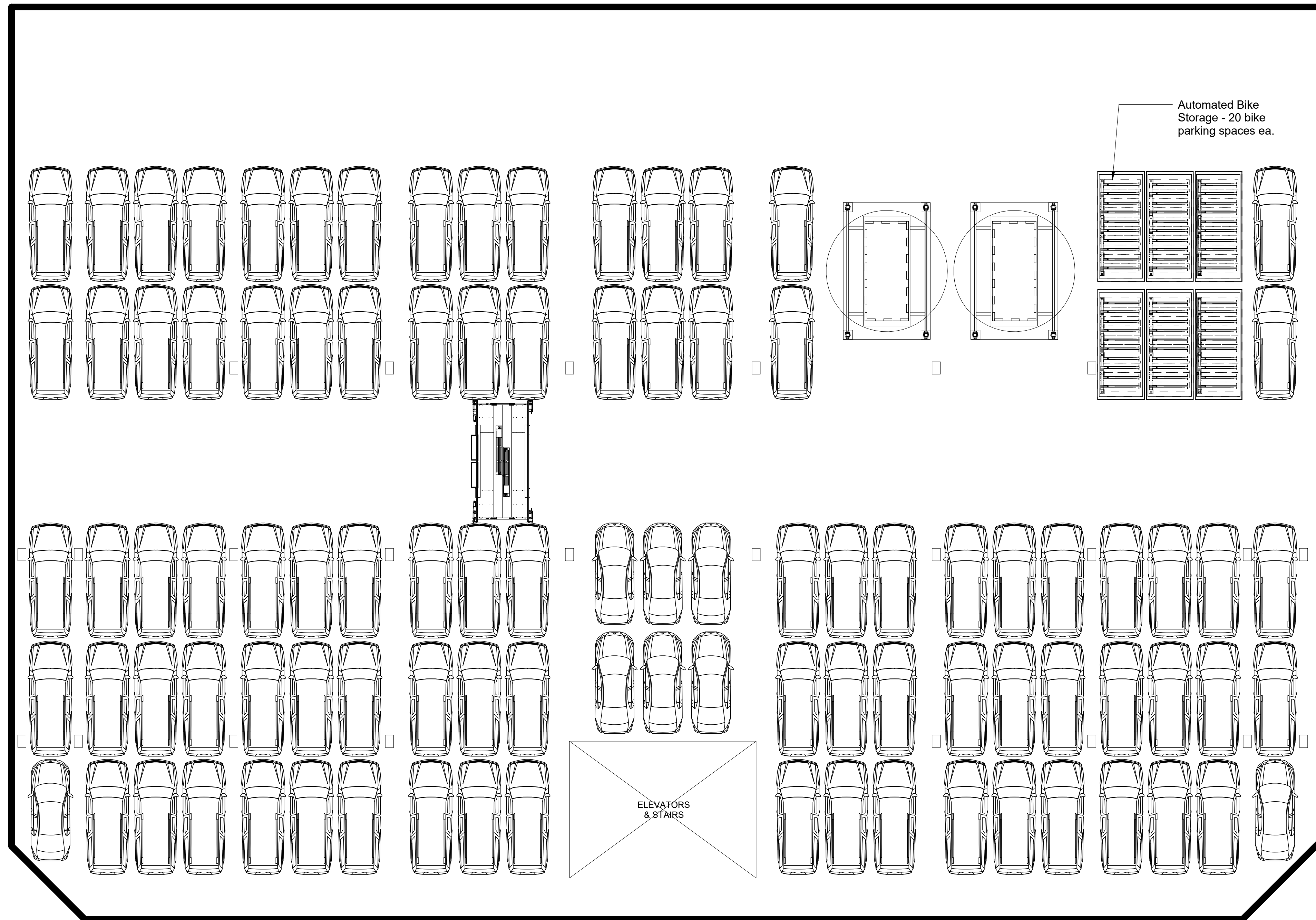
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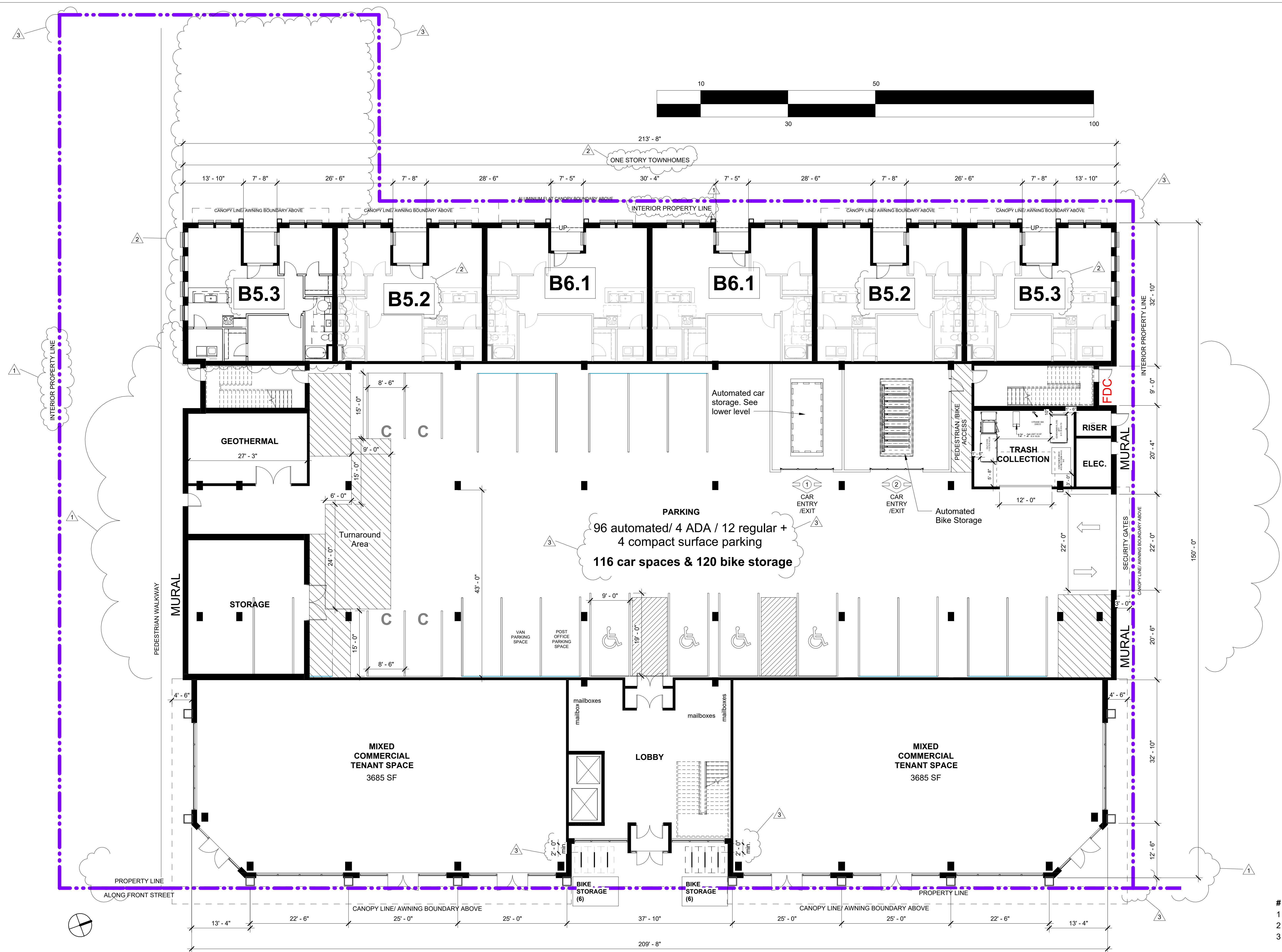
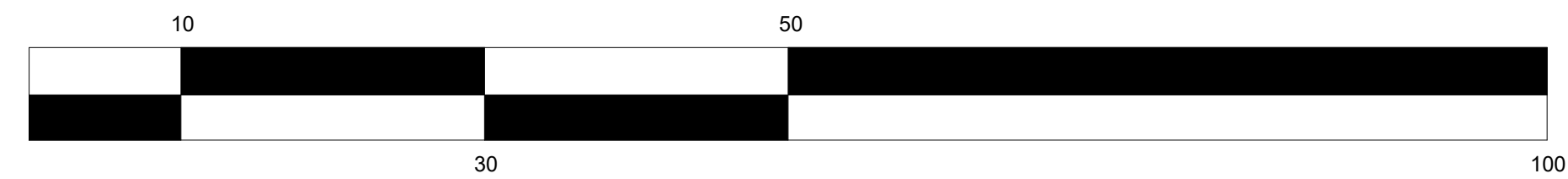
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2	Unit Changes	06/19/2024



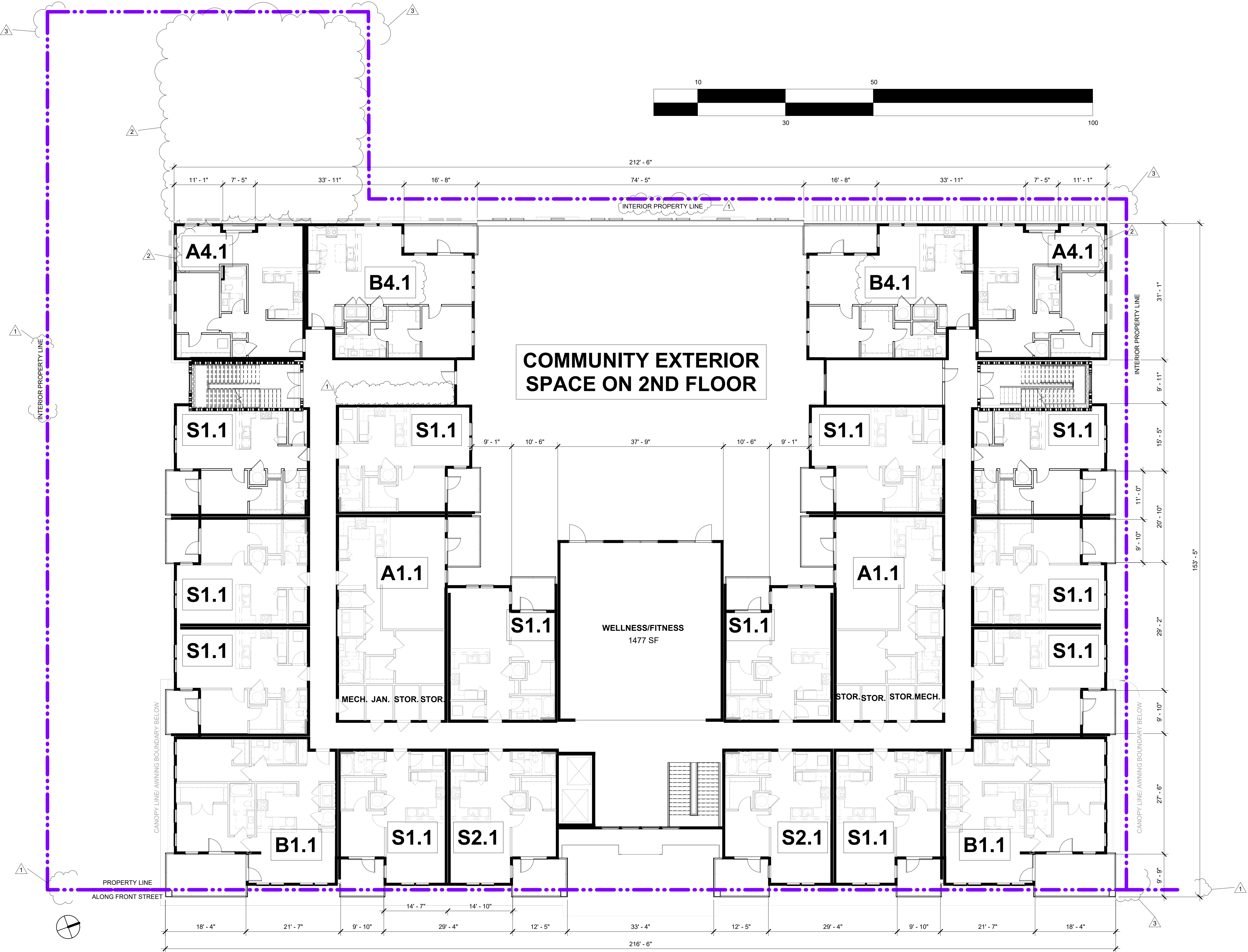
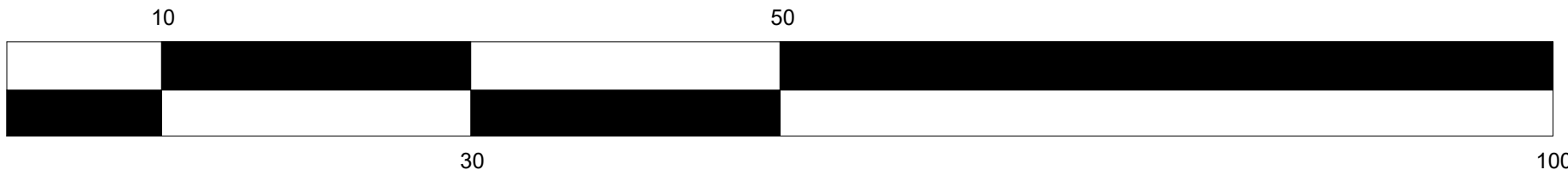
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2	Unit Changes	06/19/2024



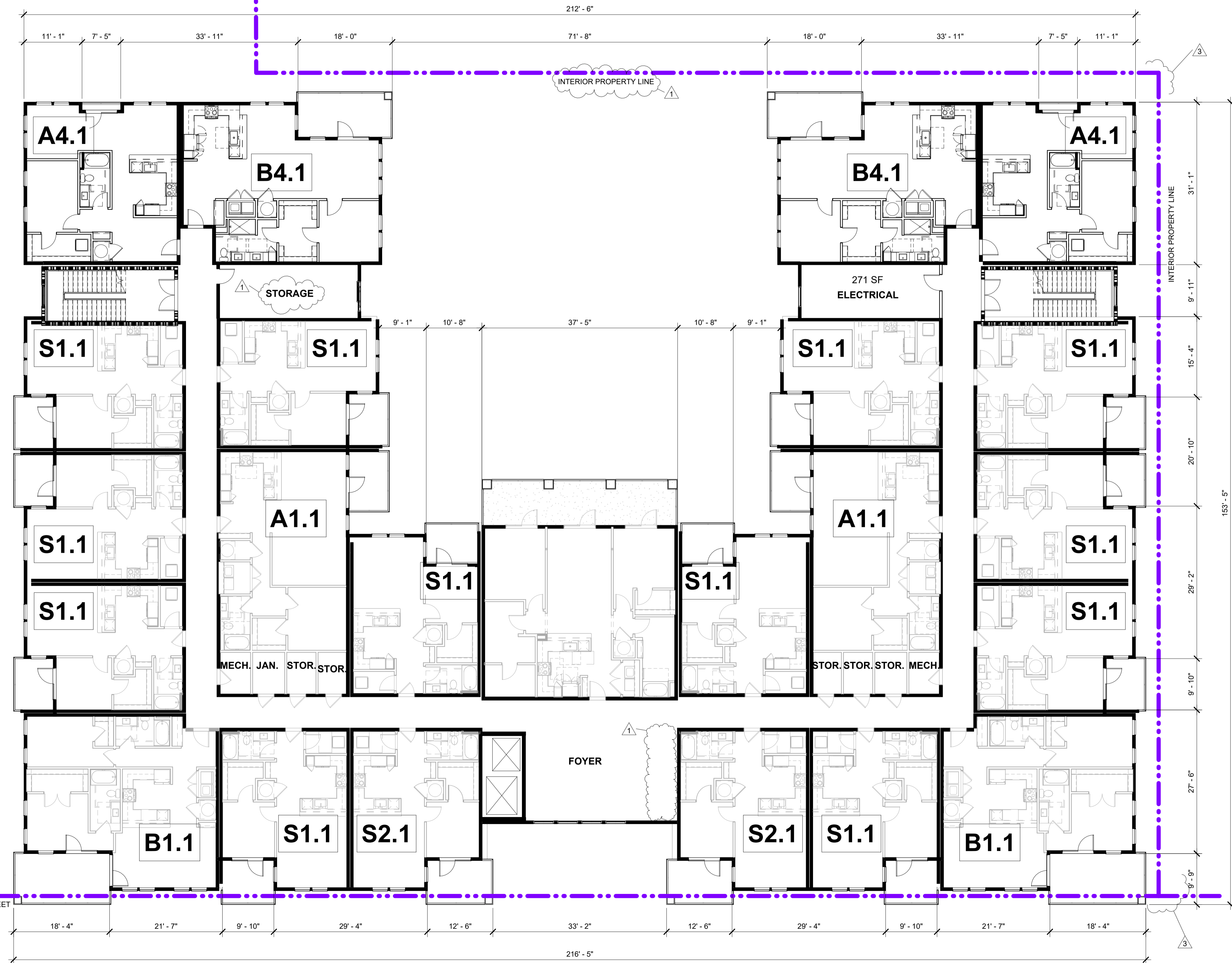
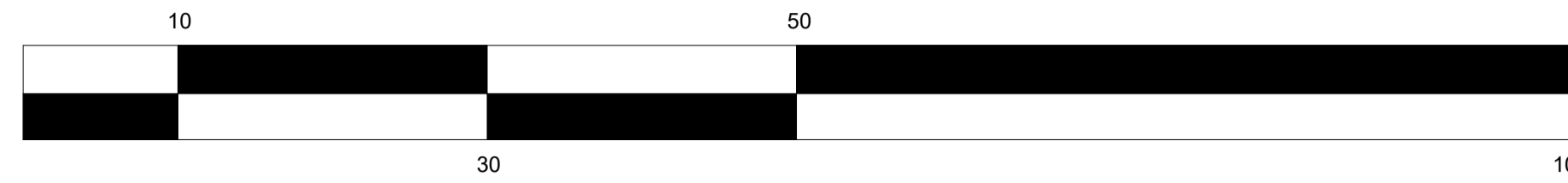
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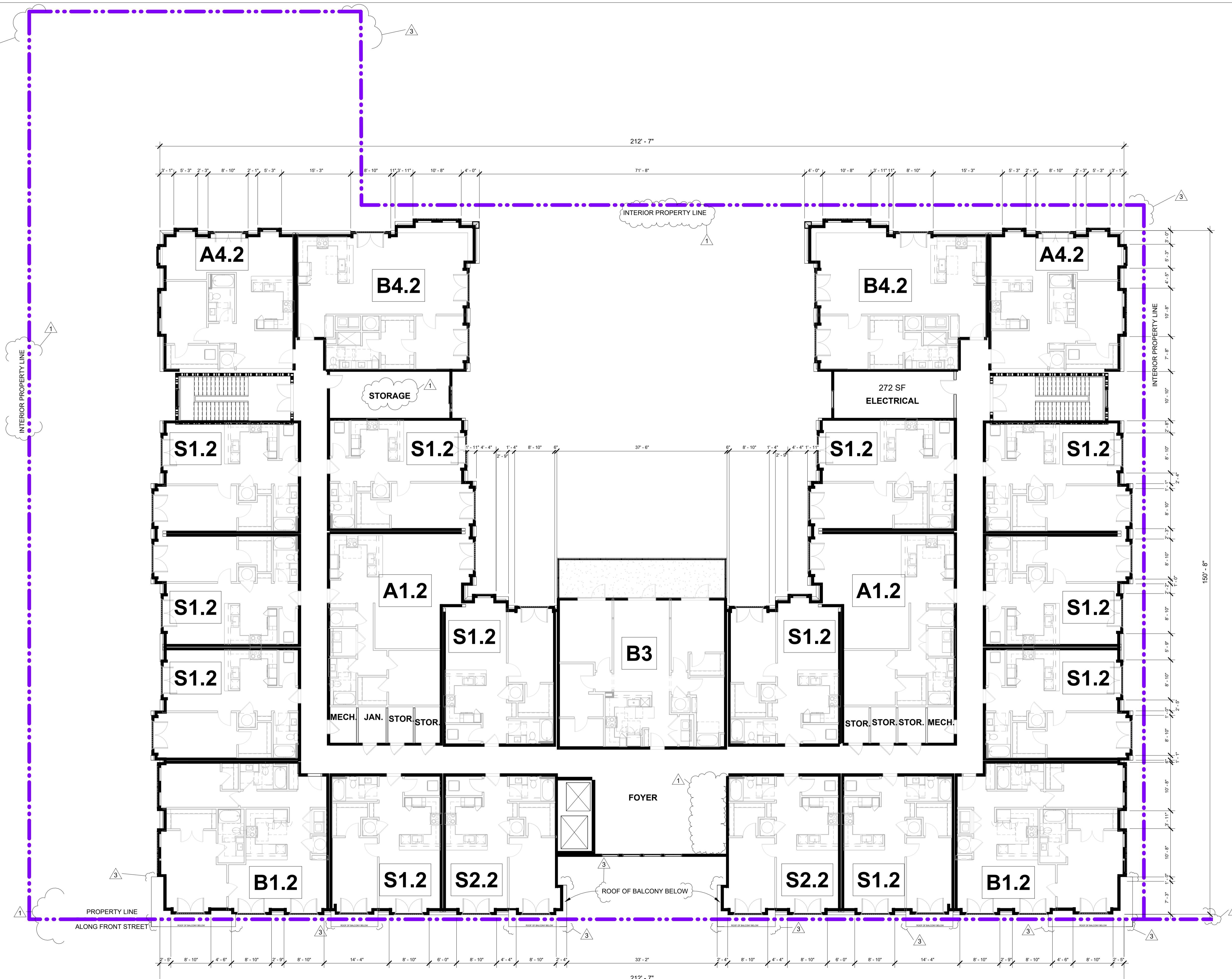
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3	Revision 3	07/23/2024



#	REVISION	DATE
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3	Revision 3	07/23/2024

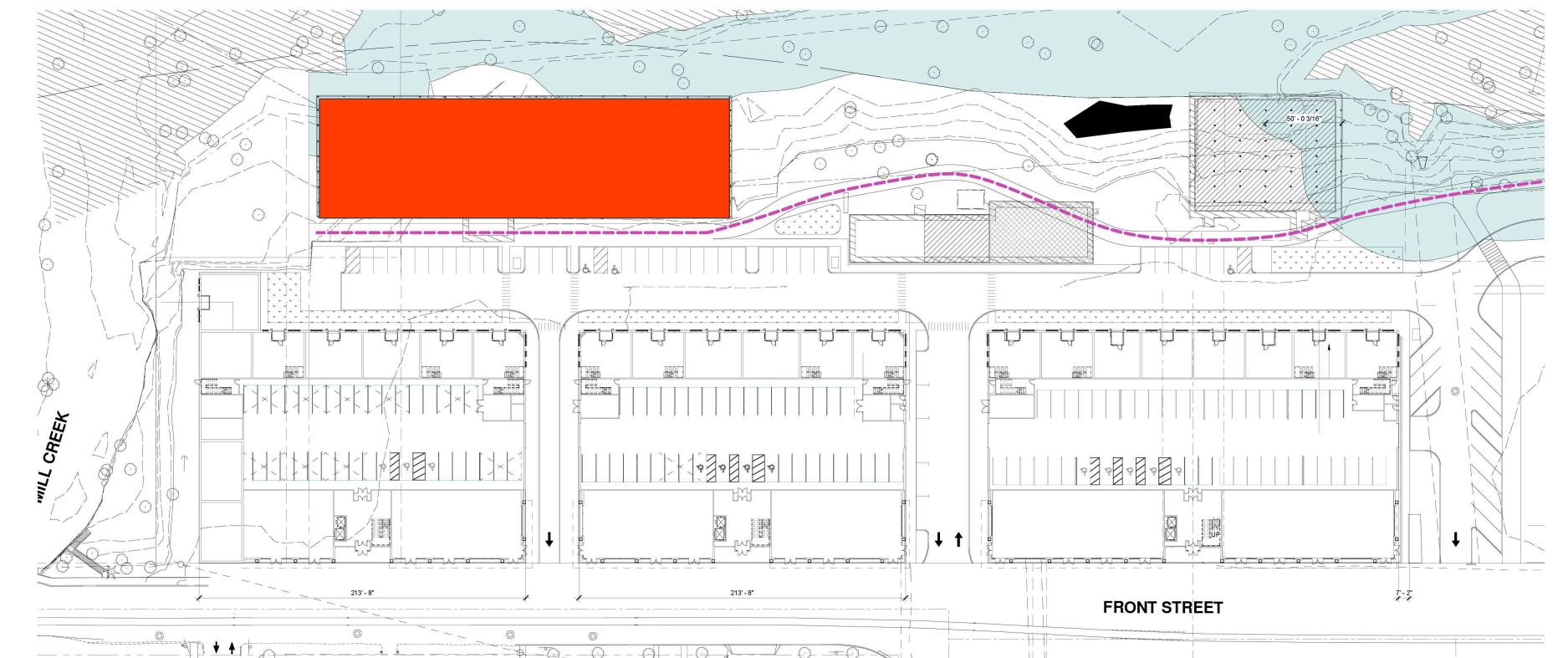


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3	Revision 3	07/23/2024



#	REVISION
1	Revision 1
3	Revision 3

DATE
05/07/2024
07/23/2024



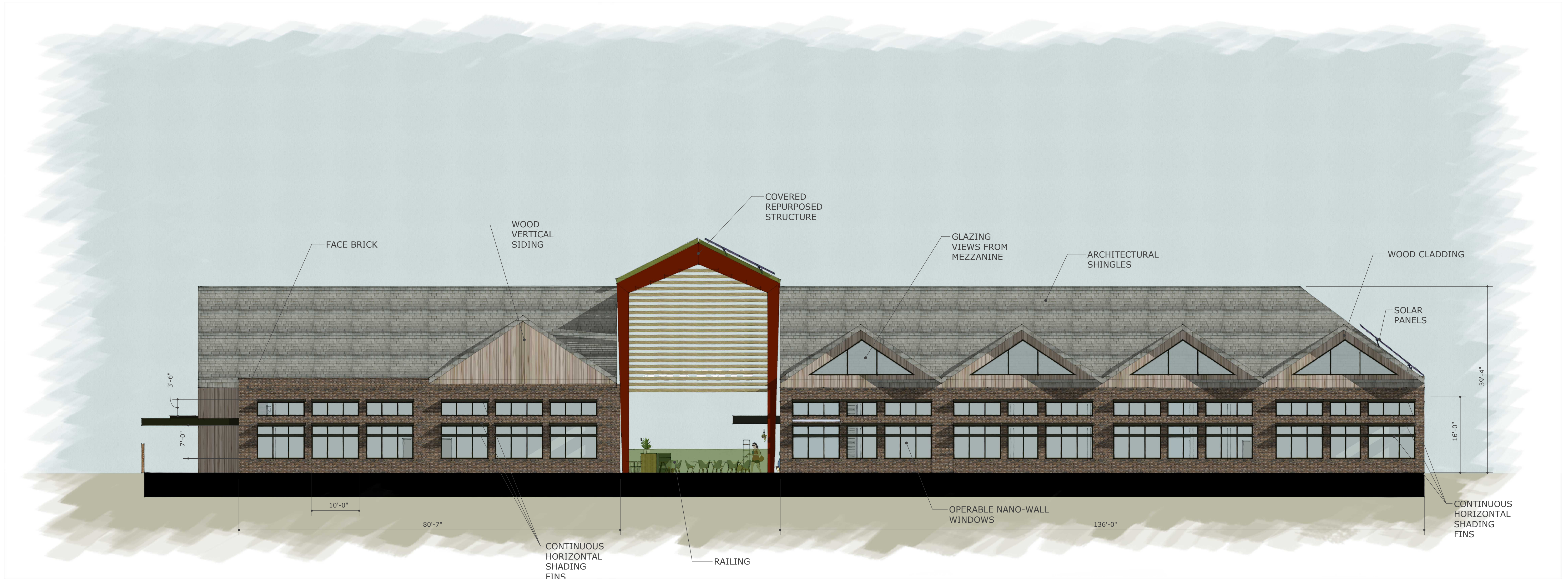


SOUTH SECTION

58% GLAZING*
 82% WEATHERPOTECTION.
 Pedestrian traffic between the two building sections.
 *Calculated per SRC 112.030 (b)

NORTH SECTION

68% GLAZING*
 91% WEATHERPOTECTION.
 Pedestrian traffic between the two building sections.
 *Calculated per SRC 112.030 (b)



NORTH SECTION

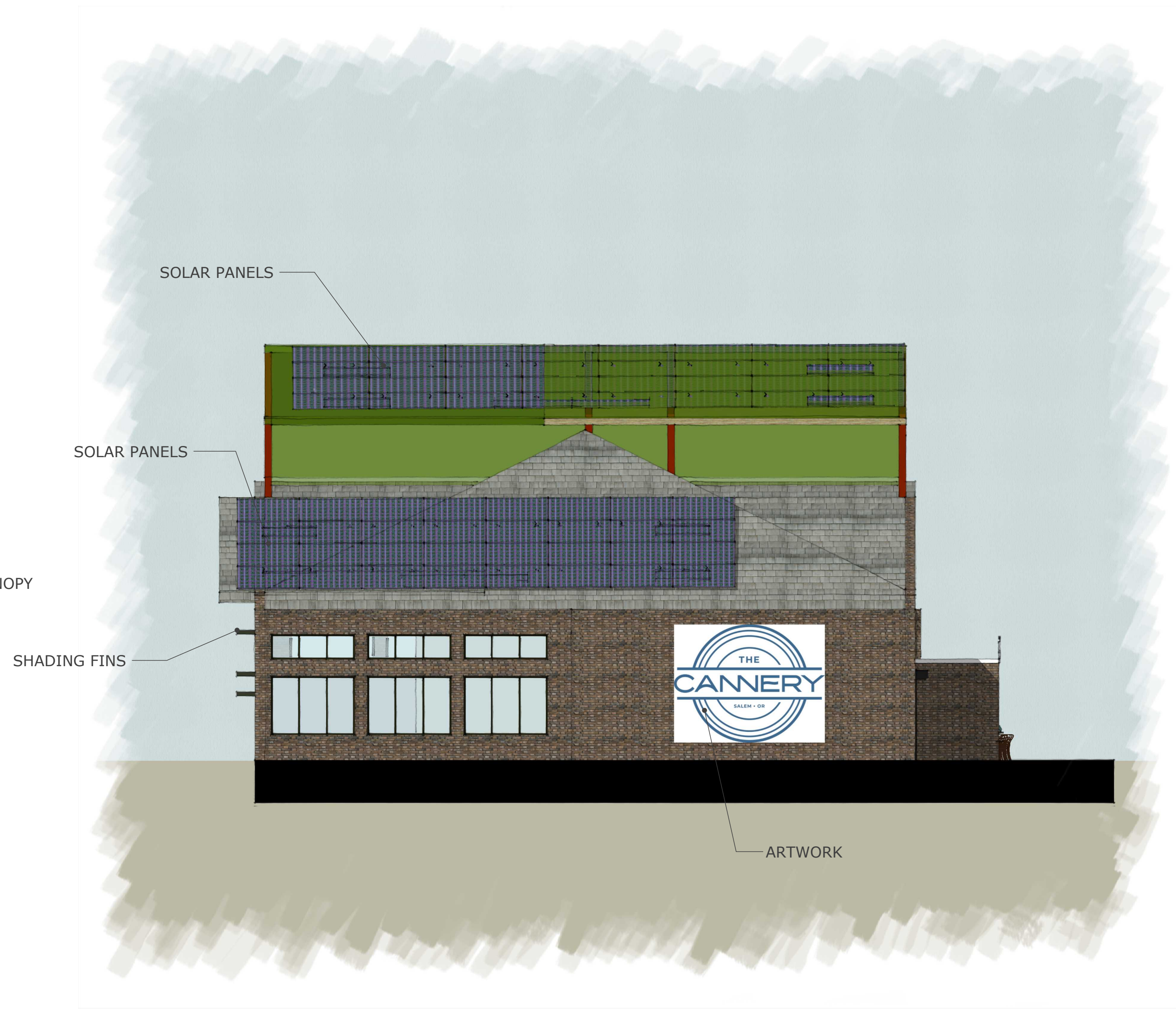
75% GLAZING*
 NO WEATHERPOTECTION.
 Pedestrian traffic between the two building sections.
 *Calculated per SRC 112.030 (b)

SOUTH SECTION

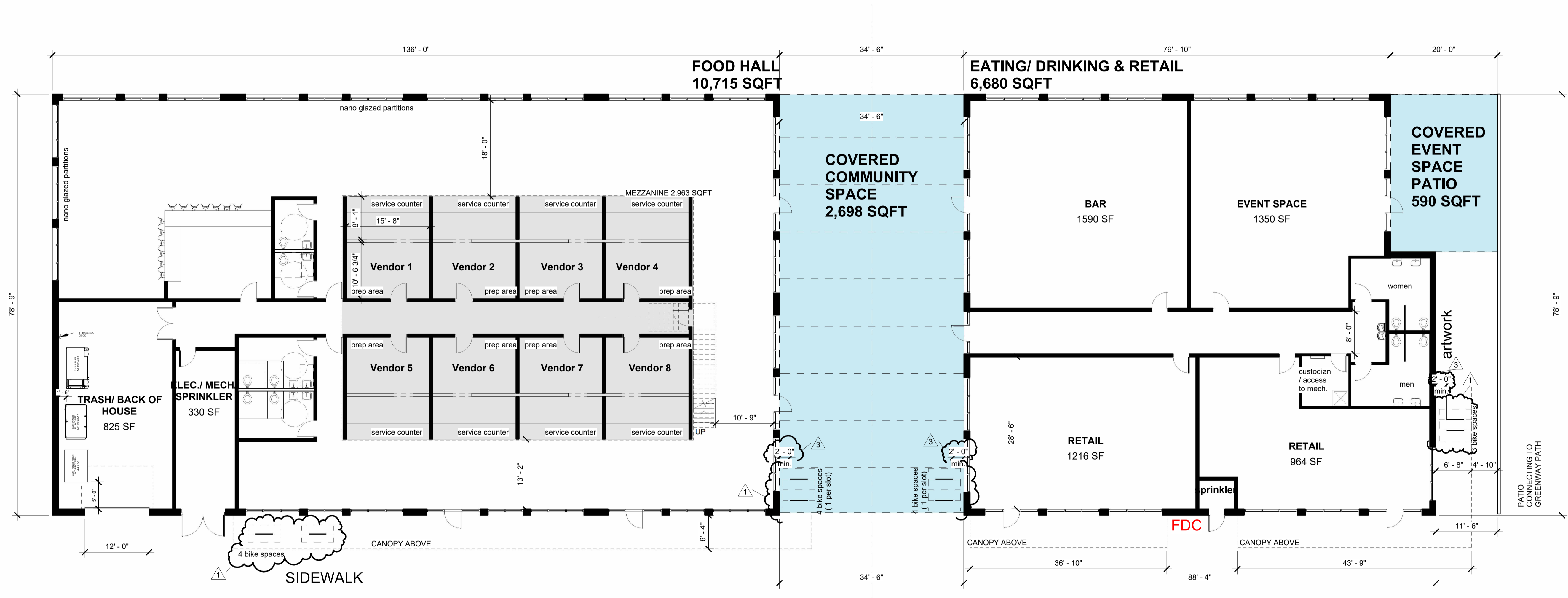
78% GLAZING*
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 Pedestrian traffic between the two building sections.
 *Calculated per SRC 112.030 (b)



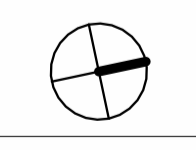
NORTH ELEVATION



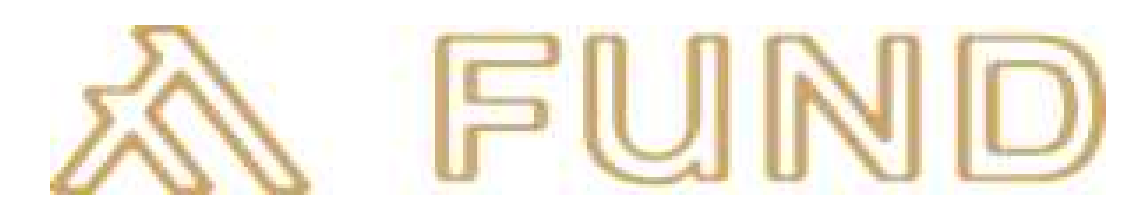
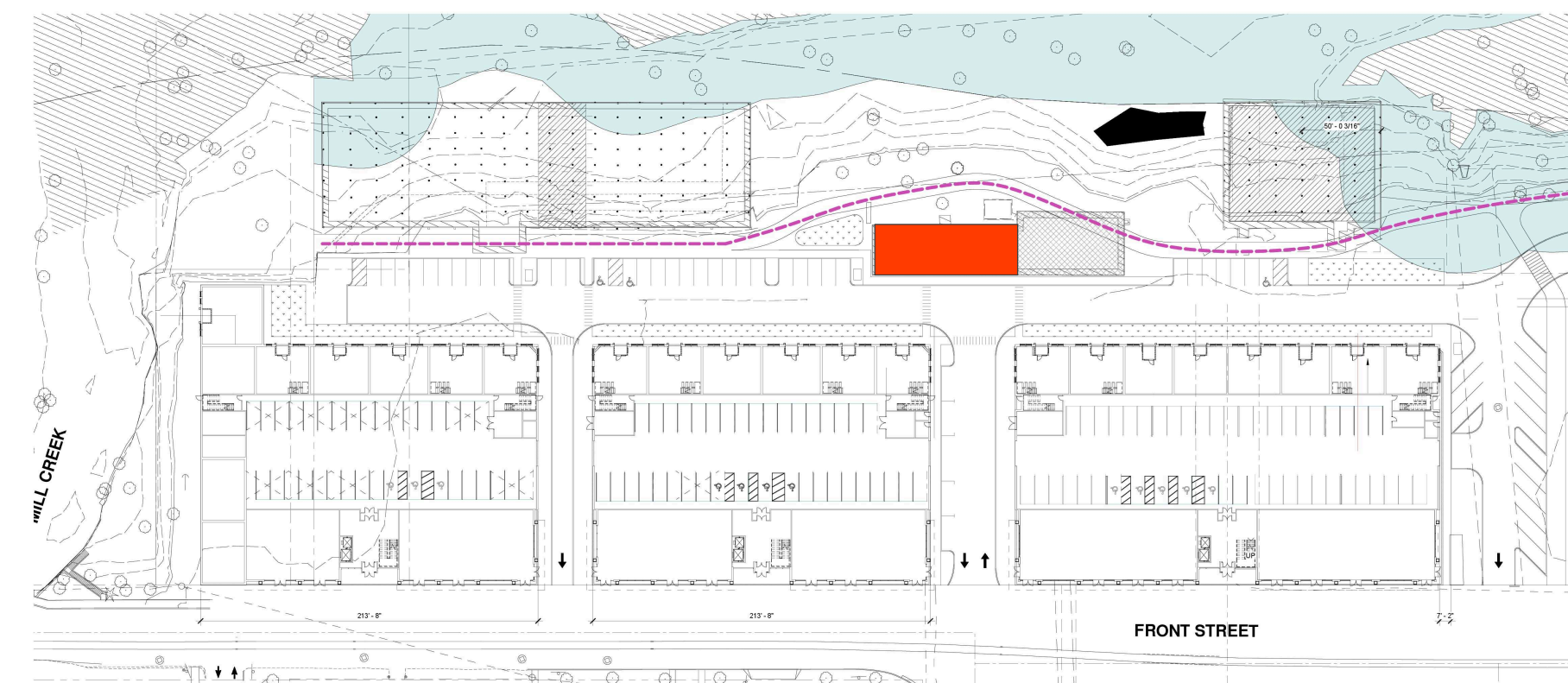
SOUTH ELEVATION



1 FOOD HALL
 3/32" = 1'-0"



#	REVISION	DATE
1	Revision 1	05/07/2024
3	Revision 3	07/23/2024





+/- 92'-0"
EXISTING

ARCHITECTURAL SHINGLES

SIGNAGE

+/- 26'-0"
EXISTING

EXISTING BRICK
DETAILING
TO REMAIN (TYP.)

Trés Jolie

ALL EXISTING WINDOWS TO REMAIN
FOR SOUTH, EAST AND WEST FACADE

EXISTING POURED IN PLACE CONCRETE
STRUCTURE PAINTED WHITE

ALL EXISTING WINDOWS
TO REMAIN

NEW WINDOW TO MATCH
EXISTING WINDOWS AND SILL DETAILING

MAINTAIN ALL EXISTING DIMENSIONS

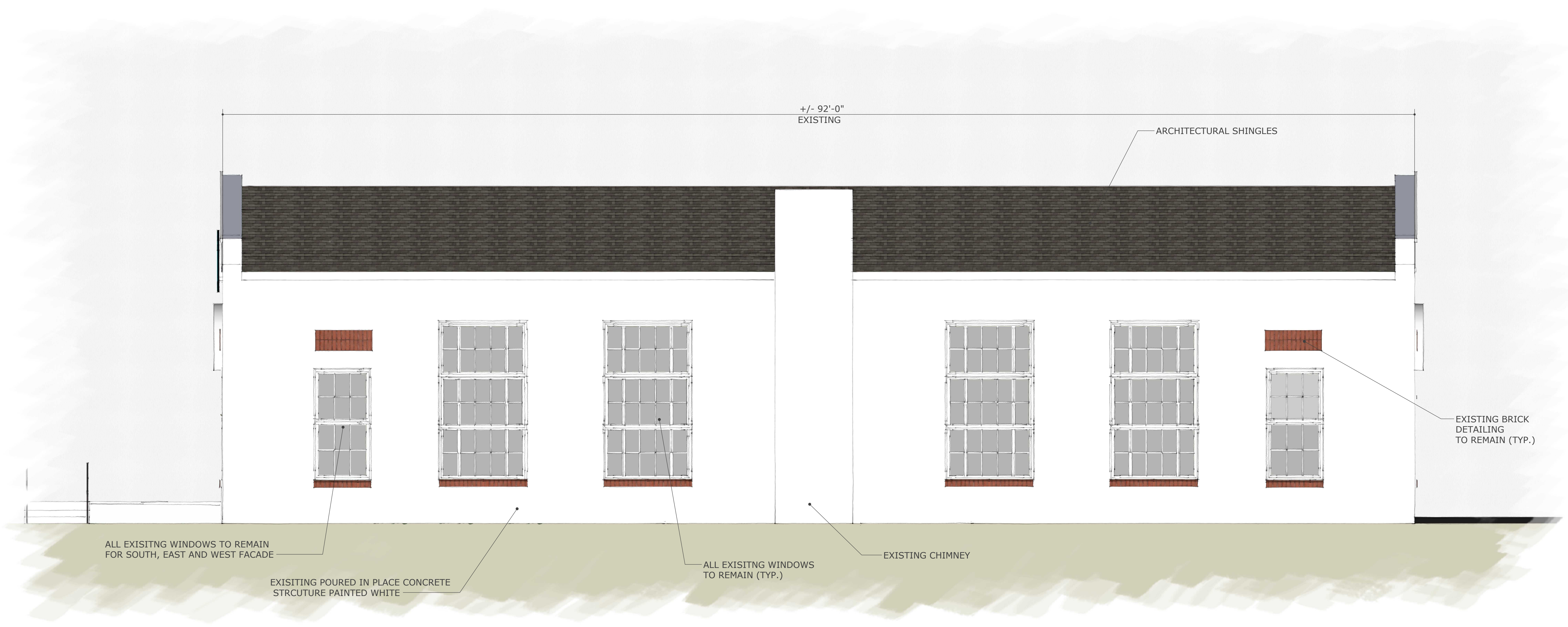


MAINTAIN ALL EXISTING DIMENSIONS



NOTE: CURRENTLY, THIS FACADE IS COVERED BY A PREVIOUS ADDITION, WHICH IS TO BE DEMOLISHED. ALTERED PORTION OF FACADE IS LESS THAN 10% OF THE OVERALL FACADE.

MAINTAIN ALL EXISTING DIMENSIONS



+/- 92'-0"
EXISTING

ARCHITECTURAL SHINGLES

EXISTING BRICK
DETAILING
TO REMAIN (TYP.)

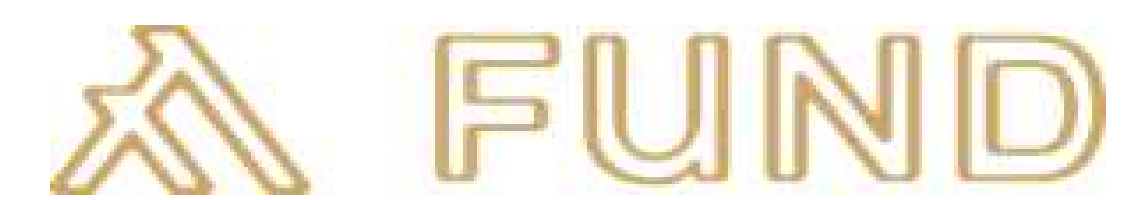
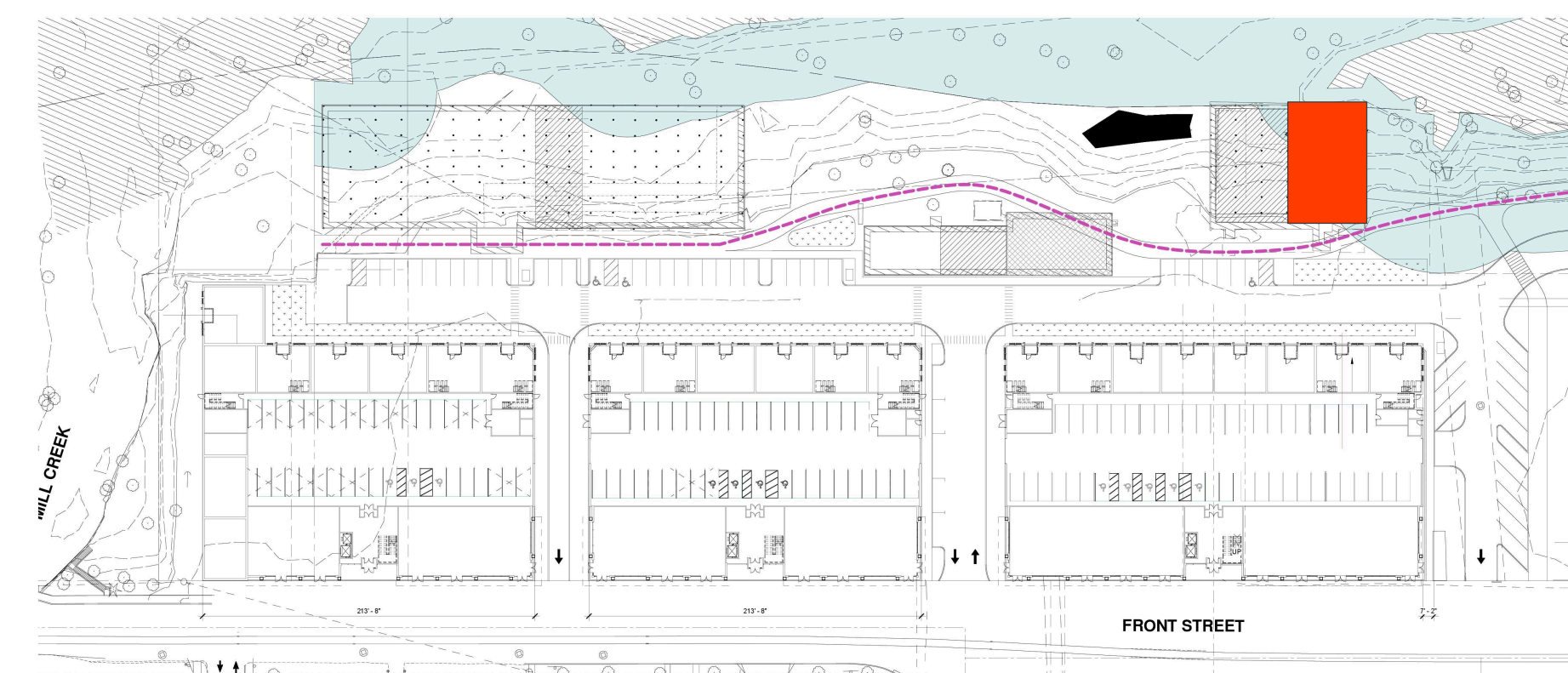
ALL EXISTING WINDOWS TO REMAIN
FOR SOUTH, EAST AND WEST FACADE

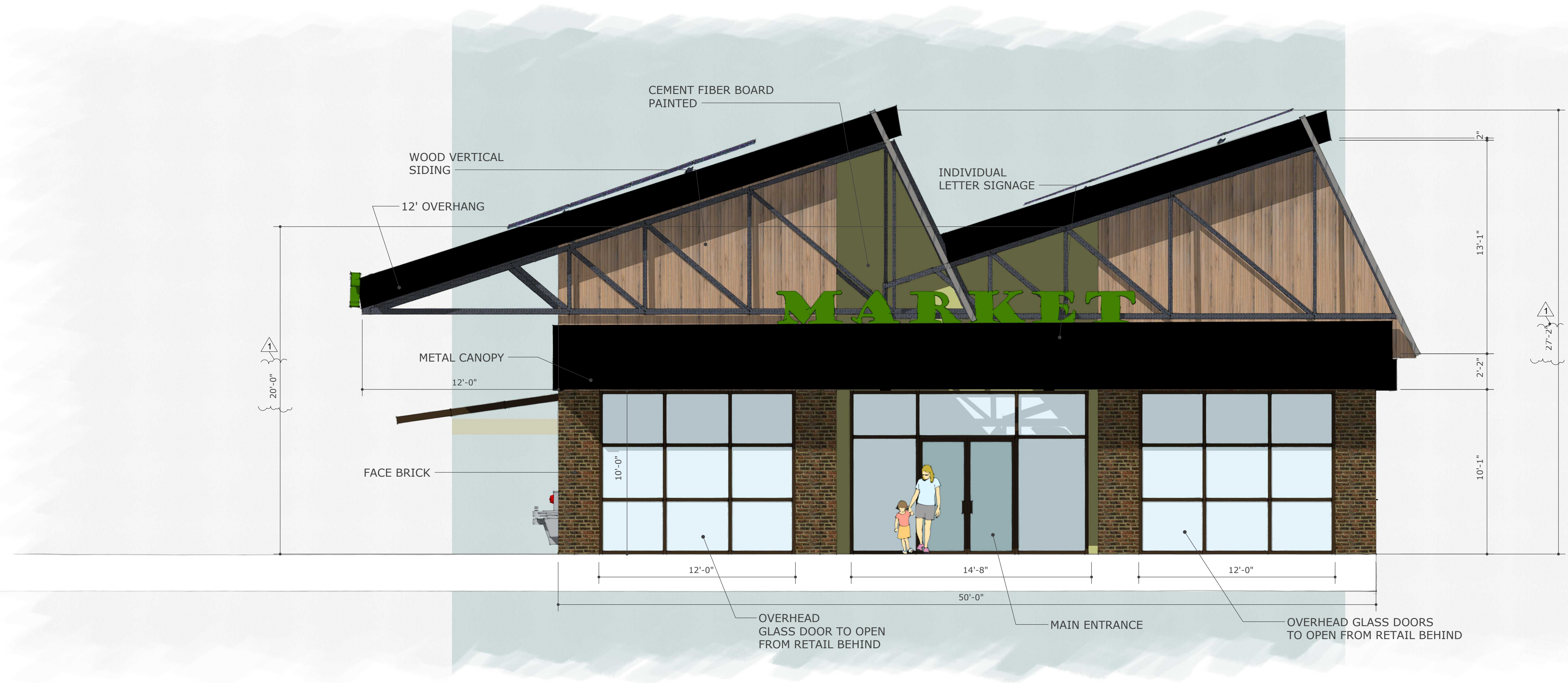
EXISTING POURED IN PLACE CONCRETE
STRUCTURE PAINTED WHITE

ALL EXISTING WINDOWS
TO REMAIN (TYP.)

EXISTING CHIMNEY

MAINTAIN ALL EXISTING DIMENSIONS





#	REVISION	DATE
1	Revision 1	05/07/2024

77% GLAZING*
100% WEATHERPROTECTION

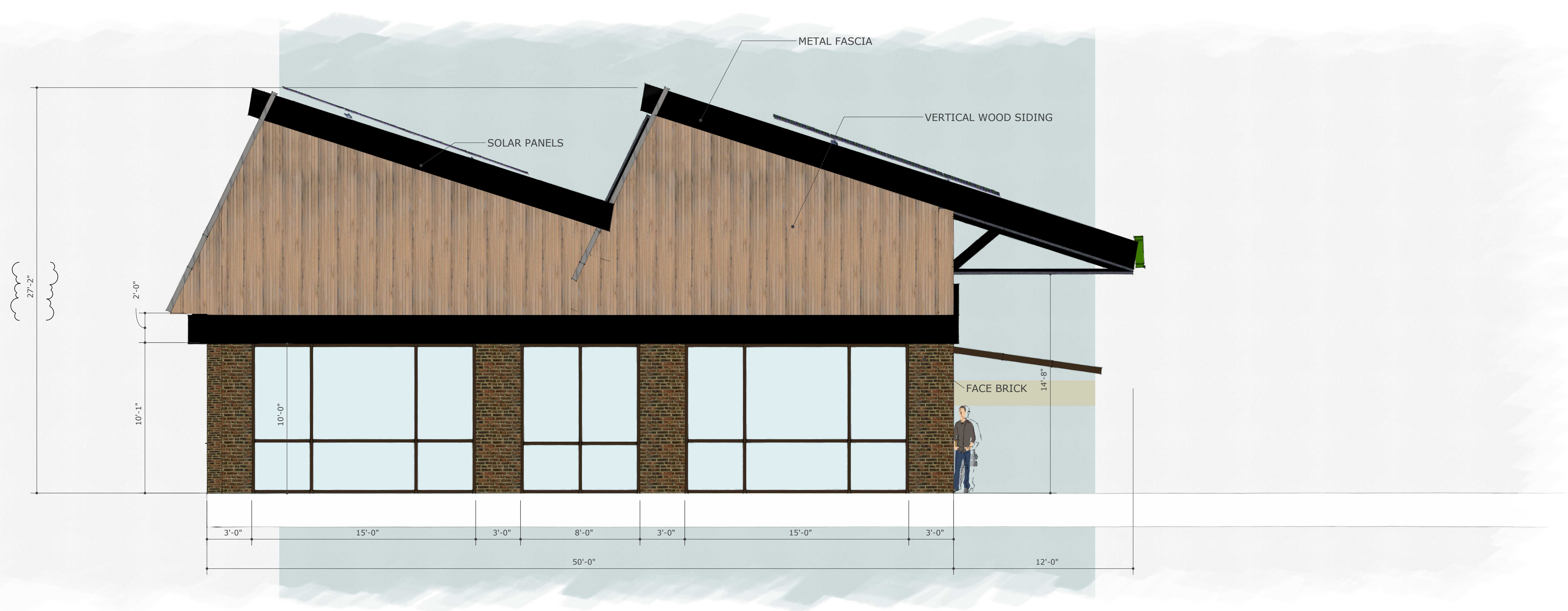
*Calculated per SRC 112.030 (b)



#	REVISION	DATE
1	Revision 1	05/07/2024



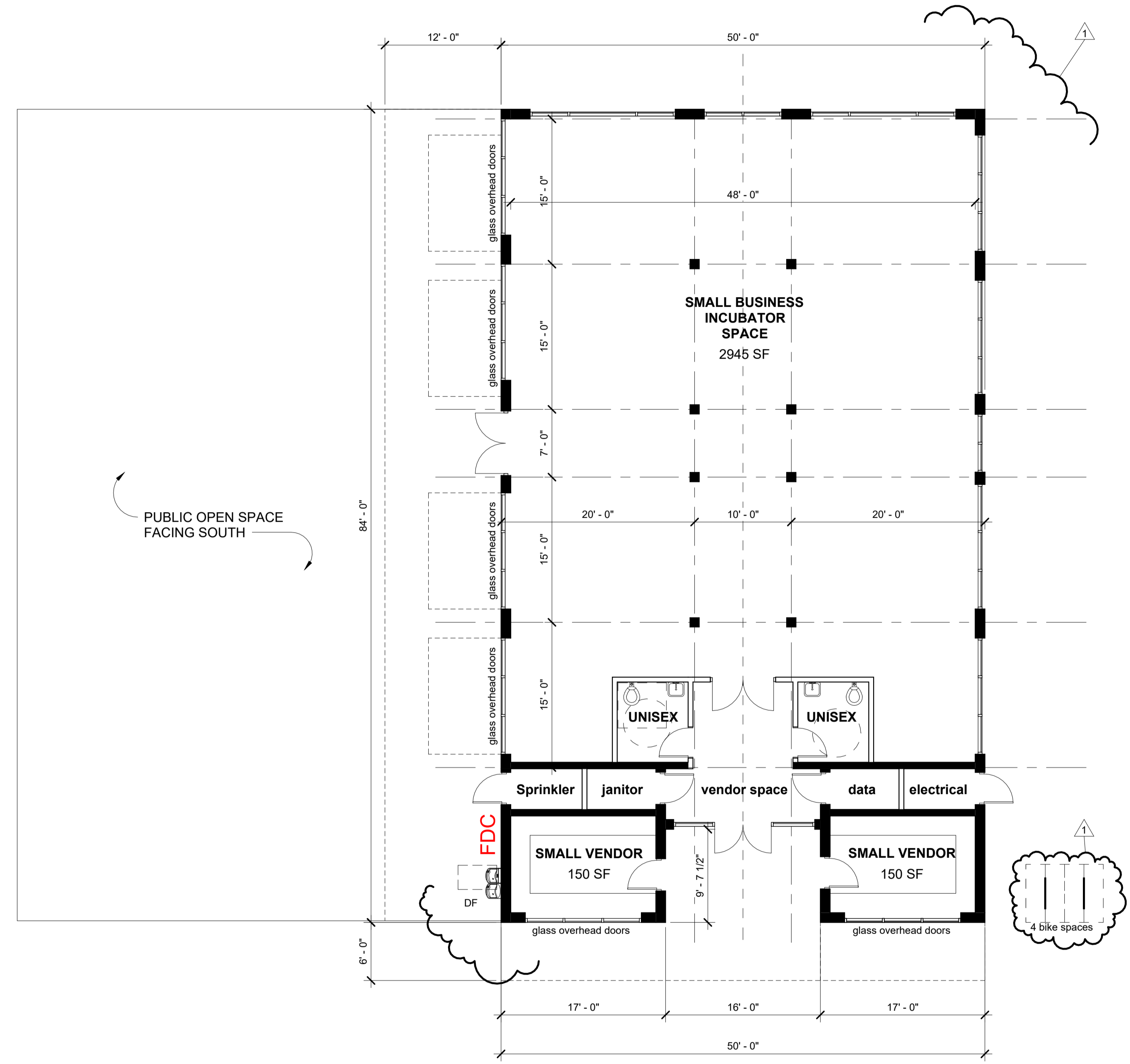
#	REVISION	DATE
1	Revision 1	05/07/2024



#	REVISION	DATE
1	Revision 1	05/07/2024

76% GLAZING*
NO WEATHERPROTECTION

*Calculated per SRC 112.030 (b)



1 1st FLOOR PLAN
 1/8" = 1'-0" 

TOTAL BUILDING SQFT 4,200 sf
 COVERED AREA SOUTH: 1,008 sf
 TOTAL: 5,208 sf

#	REVISION	DATE
1	Revision 1	05/07/2024

Attachment F: Geotechnical Engineering Services Memorandum

7662 SW Mohawk Street, Tualatin, Oregon 97062

www.centralgeotech.com

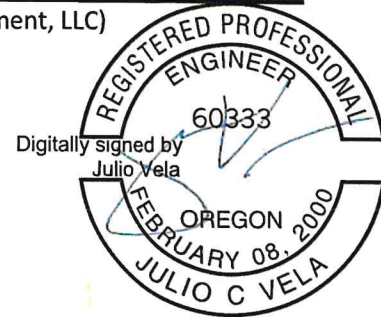
To: Trent Michels, FUND (Future of Neighborhood Development, LLC)
Grace Wolff, AKS Engineering & Forestry, LLC

From: Julio Vela, PhD, PE, GE

CGS Project Number: FUND-1-01

Date: July 19, 2024

Subject: Geotechnical Engineering Services
Response to City of Salem Second Review Comments (July 19, 2024)
Salem Cannery Project
Salem, Oregon



EXPIRES: 06/30/26

Central Geotechnical Services, LLC (CGS) has prepared this memorandum in response to a submittal requirements comment with respect to geotechnical requirements in City of Salem's, "Land Use Application Completeness Review, Second Review Comments", dated July 19, 2024. The comment references the geotechnical report for the project dated March 24, 2023 prepared under my direction as geotechnical engineer of record (GER). Julio Vela will remain GER for the project under contract with FUND through design and construction phases of the project.

The intent of geotechnical design and construction recommendations provided in the geotechnical report and continuing geotechnical engineering services being provided through the design phase are intended to meet the requirements of Chapter 18 of the International Building Code (IBC) as well as Salem Revised Code (SRC) Sec. 600.025 Subsection (a)(2). Geotechnical design and construction recommendations for proposed development are being provided in consideration of not adversely affecting the stability of land area.

Please let us know if we can provide additional information.

Attachment G: Flood Carrying Capacity Memorandum



July 23, 2024

Community Planning and Development Staff
City of Salem
555 Liberty St SE, Room 305, Salem, OR 97301

RE: The Cannery Project – Flood Capacity Memo

Dear City Staff:

This letter is to address the impact to the flood capacity remark in the City of Salem’s “*Land Use Application Completeness Review, Second Review Comments*”, reference number 24-106451-PLN, dated July 19, 2024. The Cannery site is located along the mouth of Mill Creek and the Willamette River with a regulatory floodplain extending within the site on each waterfront. As proposed, the project would not have an impact on the flood carrying capacity of either watercourse with the proposed improvements being above or outside the regulatory floodplain of each watercourse.

A portion of the project is to develop a new building on existing pier systems for Buildings 4 and 6. As such, structural improvements to the pier system may be necessary (but this analysis and determination has yet to be completed). If structural improvements are in fact required to meet current building code requirements within the floodplain boundary and below the floodplain elevation, then a flood capacity review can be provided at the time of building permit when the extent of the improvements is known.

In response to SRC 600.015(d)(6), and based on discussions with City staff, a detailed flood study based on the current known project improvements is not required as no impact to the full flood carrying capacity is currently anticipated.

The attached Exhibit A is a visual representation of the proposed development and associated improvements relative to the floodplain elevations. We request the city accept this letter as documentation to address SRC 600.015(d)(6).

Sincerely,

AKS ENGINEERING & FORESTRY, LLC

Tyler D. Roth, P.E.

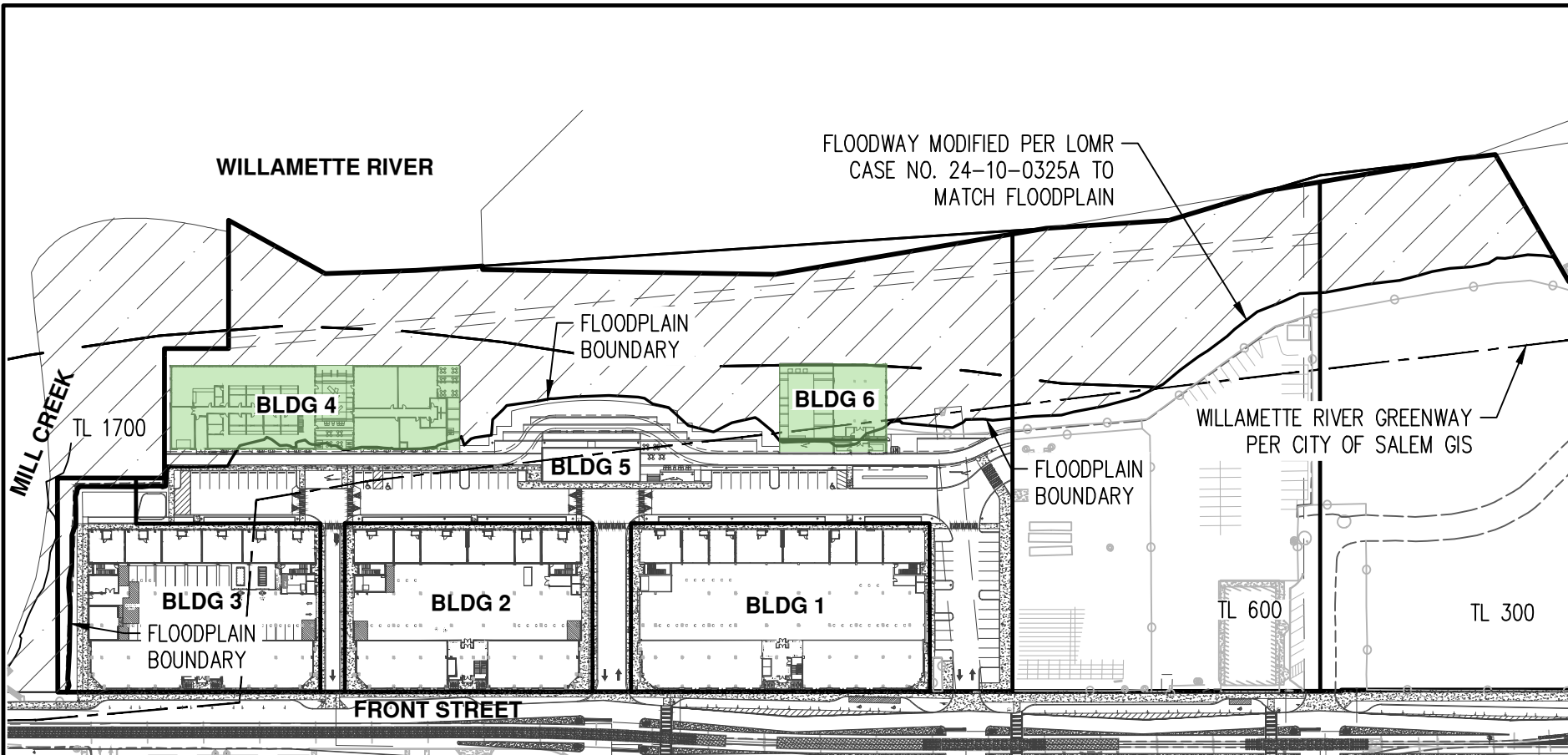
3700 River Rd N Suite 1
Keizer, OR, 97303
(503) 400-6028 | RothT@aks-eng.com



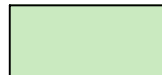
RENEWS: 12/31/24

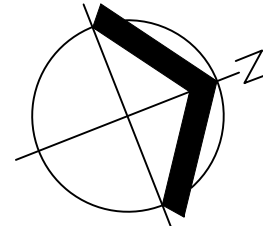
Attachments:

- Exhibit A: Floodplain Location Relative to Project Location
- Exhibit B: FEMA LOMR-FW Case No. 24-10-0325A

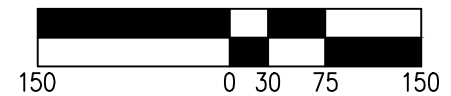


 WILLAMETTE RIVER OR MILL CREEK SIDE OF MAPPED FLOODPLAIN (ELEV= 141.10)

 DEVELOPMENT HORIZONTALLY WITHIN FLOODPLAIN, BUT VERTICALLY ABOVE FLOODPLAIN ELEVATION



SCALE: 1" = 150 FEET



DATE: 07/23/2024
 DRWN: MJM | CHKD: JDR
 AKS JOB: EXHIBIT
 5968-01

FLOODPLAIN LOCATION RELATIVE TO PROJECT SITE



AKS ENGINEERING & FORESTRY, LLC
 3700 RIVER RD N, STE 1
 KEIZER, OR 97303
 503.400.6028 WWW.AKS-ENG.COM
 DWG: 5968-01 FLOOD CAPACITY | LETTER-L



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP REVISION FLOODWAY DETERMINATION DOCUMENT (REMOVAL)

COMMUNITY AND MAP PANEL INFORMATION		LEGAL PROPERTY DESCRIPTION
COMMUNITY	CITY OF SALEM, MARION COUNTY, OREGON	Lots 1 and 2, Willamette Landing; Lots 1 through 8, Block 24, Lots 1 through 8, Block 25, North Salem; Lots 1 through 8, Block 1, Mill Addition; as shown on the Plats recorded in Book 35, Page 27, in Book 1, Page 34, and in Book 1, Page 90, all in the Office of the County Clerk, Marion County, Oregon The portion of property is more particularly described by the following metes and bounds:
	COMMUNITY NO.: 410167	
AFFECTED MAP PANEL	NUMBER: 41047C0333H DATE: 1/2/2003	
FLOODING SOURCE: WILLAMETTE RIVER		APPROXIMATE LATITUDE & LONGITUDE OF PROPERTY: 44.953284, -123.036610 SOURCE OF LAT & LONG: LOMA LOGIC DATUM: NAD 83

DETERMINATION

LOT	BLOCK/ SECTION	SUBDIVISION	STREET	OUTCOME WHAT IS REMOVED FROM THE SFHA	FLOOD ZONE	1% ANNUAL CHANCE FLOOD ELEVATION (NGVD 29)	LOWEST ADJACENT GRADE ELEVATION (NGVD 29)	LOWEST LOT ELEVATION (NGVD 29)
--	--	--	0, 1375, 1105 Front Street NE	Portion of Property	X (shaded)	--	--	141.1 feet

Special Flood Hazard Area (SFHA) - The SFHA is an area that would be inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood).

ADDITIONAL CONSIDERATIONS (Please refer to the appropriate section on Attachment 1 for the additional considerations listed below.)

LEGAL PROPERTY DESCRIPTION	STATE LOCAL CONSIDERATIONS
INADVERTENT INCLUSION FLOODWAY 1	
PORTIONS REMAIN IN THE SFHA	

This document provides the Federal Emergency Management Agency's determination regarding a request for a Letter of Map Revision for the property described above. Using the information submitted and the effective National Flood Insurance Program (NFIP) map, we have determined that the described portion(s) of the property(ies) is/are not located in the NFIP regulatory floodway or the SFHA, an area inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood). This document revises the effective NFIP map to remove the subject property from the NFIP regulatory floodway and the SFHA located on the effective NFIP map; therefore, the Federal mandatory flood insurance requirement does not apply. However, the lender has the option to continue the flood insurance requirement to protect its financial risk on the loan.

This determination is based on the flood data presently available. The enclosed documents provide additional information regarding this determination. If you have any questions about this document, please contact the FEMA Mapping and Insurance eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, LOMC Clearinghouse, 3601 Eisenhower Avenue, Suite 500, Alexandria, VA 22304-6426.

Patrick "Rick" F. Sacibit, P.E., Branch Chief
Engineering Services Branch
Federal Insurance and Mitigation Administration



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP REVISION FLOODWAY DETERMINATION DOCUMENT (REMOVAL)

ATTACHMENT 1 (ADDITIONAL CONSIDERATIONS)

LEGAL PROPERTY DESCRIPTION (CONTINUED)

Beginning at the Northeasterly corner of Lot 2 of the plat of "Willamette Landing", also being on the westerly extension of the northerly line of Lot 1 of said plat; thence along said westerly extension and continuing along said northerly line, South 68°39'53" East 268.50 feet, to the northeasterly corner thereof; thence along the easterly line thereof, South 21°22'07" West 278.73 feet; thence South 68°37'53" East 1.00 feet, to the northwesterly right-of-way line of Front Street NE (49.50 feet from the centerline); thence along said northwesterly line, (continuing along said northwesterly rightof- way line as the right-of-way width decreases to 30.00 feet from the centerline) South 21°22'07" West 1193.01 feet; thence leaving said northwesterly right-of-way line, North 45°59'45" West 6.11 feet; thence North 79°28'11" West 6.51 feet; thence North 64°49'00" West 26.55 feet; thence North 67°33'52" West 40.32 feet; thence North 73°53'11" West 18.75 feet; thence North 64°07'32" West 8.91 feet; thence North 57°56'56" West 12.32 feet; thence North 67°44'32" West 14.46 feet; thence North 68°32'32" West 5.14 feet; thence North 68°10'22" West 8.37 feet; thence North 51°21'39" West 6.05 feet; thence North 34°24'49" West 3.42 feet; thence North 69°00'29" West 5.28 feet; thence North 68°25'00" West 11.20 feet; thence North 71°07'13" West 11.56 feet; thence North 46°55'38" West 3.01 feet; thence North 56°38'29" West 1.72 feet; thence North 60°05'27" West 2.69 feet; thence North 13°44'52" West 3.50 feet; thence North 21°56'20" East 57.91 feet; thence North 14°36'22" East 6.40 feet; thence North 22°11'31" East 11.52 feet; thence North 52°45'48" East 1.04 feet; thence North 05°46'33" East 3.13 feet; thence North 64°32'11" West 17.88 feet; thence North 21°54'41" East 2.11 feet; thence South 68°57'53" East 51.80 feet; thence North 21°00'57" East 62.34 feet; thence North 69°24'18" West 49.95 feet; thence North 20°35'42" East 33.30 feet; thence North 21°17'23" East 35.62 feet; thence North 69°24'49" West 16.06 feet; thence North 21°23'26" East 142.39 feet; thence North 68°33'31" West 9.56 feet; thence North 20°50'40" East 30.64 feet; thence North 69°59'46" West 19.73 feet; thence North 22°00'31" West 12.78 feet; thence North 27°38'32" West 5.03 feet; thence North 13°19'21" East 29.35 feet; thence North 10°49'12" East 11.98 feet; thence North 14°51'39" East 2.90 feet; thence North 17°13'05" East 20.89 feet; thence North 15°20'01" East 1.55 feet; thence North 19°00'22" East 29.53 feet; thence North 24°59'49" East 27.58 feet; thence North 30°18'09" East 5.98 feet; thence North 56°22'26" East 9.07 feet; thence North 34°31'42" East 15.41 feet; thence North 63°44'34" East 24.91 feet; thence North 35°52'49" East 24.19 feet; thence North 05°03'52" West 19.47 feet; thence North 11°27'15" East 3.31 feet; thence North 08°08'38" East 18.81 feet; thence North 53°01'23" East 3.58 feet; thence North 61°18'50" East 17.57 feet; thence North 80°08'29" East 2.52 feet; thence North 71°13'49" East 6.66 feet; South 68°35'42" East 11.81 feet; thence North 21°25'06" East 10.09 feet; thence South 68°34'54" East 4.98 feet; thence North 21°45'58" East 7.90 feet; thence North 67°50'29" West 6.06 feet; thence North 21°26'08" East 48.02 feet; thence South 69°08'44" East 14.00 feet; thence North 21°29'59" East 13.25 feet; thence North 68°46'11" West 8.01 feet; thence North 20°57'30" East 5.25 feet; thence North 68°36'13" West 5.11 feet; thence North 21°23'47" East 17.13 feet; thence North 68°26'47" West 25.22 feet; thence North 21°31'51" West 3.35 feet; thence North 09°27'57" East 5.88 feet; thence North 21°50'03" East 2.52 feet; thence North 40°51'49" East 2.35 feet; thence North 80°45'47" East 1.23 feet; thence North 26°43'02" East 46.91 feet; thence North 15°36'24" East 0.82 feet; thence North 06°19'00" West 10.85 feet; thence North 17°44'15" West 0.87 feet; thence North 26°58'45" West 2.53 feet; thence North 19°18'30" East 16.40 feet; thence North 30°34'42" East 9.25 feet; thence North 15°18'09" East 38.26 feet; thence North 19°03'55" East 25.93 feet; thence North 07°10'36" East 11.80 feet; thence North 11°01'57" East 28.33 feet; thence North 06°18'05" East 33.13 feet; thence North 05°22'57" West 27.12 feet; thence North 18°54'31" West 20.37 feet; thence North 14°26'53" West 21.42 feet; thence North 11°25'08" West 10.67 feet; thence North 15°30'05" West 22.62 feet; thence North 22°44'17" West 9.13 feet; thence North 15°43'20" West 27.20 feet; thence North 00°28'03" West 25.39 feet; thence North 03°38'22" West 17.56 feet; thence North 19°02'28" East 24.07 feet; thence North 10°42'53" East 20.68 feet; thence North 09°50'25" East 18.73 feet; thence North 14°34'04" East 14.68 feet; thence North 13°44'24" East 16.28 feet; thence North 07°16'13" East 19.42 feet; thence North 06°10'42" East 12.08 feet; thence North 10°42'11" East 16.67 feet; thence North 16°48'49" East

This attachment provides additional information regarding this request. If you have any questions about this attachment, please contact the FEMA Mapping and Insurance eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, LOMC Clearinghouse, 3601 Eisenhower Avenue, Suite 500, Alexandria, VA 22304-6426.

Patrick "Rick" F. Sacibit, P.E., Branch Chief
Engineering Services Branch
Federal Insurance and Mitigation Administration



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP REVISION FLOODWAY DETERMINATION DOCUMENT (REMOVAL)

ATTACHMENT 1 (ADDITIONAL CONSIDERATIONS)

20.68 feet; thence North 09°42'22" East 16.12 feet; thence North 09°58'16" West 9.36 feet; thence North 24°02'01" East 7.55 feet; thence North 12°28'05" East 21.55 feet; thence North 24°28'48" East 14.47 feet; thence North 31°39'21" East 11.25 to the northerly line of said Lot 2; thence along said northerly line, North 81°22'07" East 156.85 feet to the Point of Beginning

INADVERTENT INCLUSION IN THE FLOODWAY 1 (PORTIONS OF THE PROPERTY REMAIN IN THE FLOODWAY) (This Additional Consideration applies to the preceding 1 Property.)

A portion of this property is located within the National Flood Insurance Program (NFIP) regulatory floodway for the flooding source indicated on the Determination Document, while the subject of this determination is not. The NFIP regulatory floodway is the area that must remain unobstructed in order to prevent unacceptable increases in base flood elevations. Therefore, no construction may take place in an NFIP regulatory floodway that may cause an increase in the base flood elevation, and any future construction or substantial improvement on the property remains subject to Federal, State/Commonwealth, and local regulations for floodplain management. The NFIP regulatory floodway is provided to the community as a tool to regulate floodplain development. Therefore, the NFIP regulatory floodway modification described in the Determination Document, while acceptable to the Federal Emergency Management Agency (FEMA), must also be acceptable to the community and adopted by appropriate community action, as specified in Paragraph 60.3(d) of the NFIP regulations. Any proposed revision to the NFIP regulatory floodway must be submitted to FEMA by community officials. The community should contact either the Regional Director (for those communities in Regions I-IV, and VI-X), or the Regional Engineer (for those communities in Region V) for guidance on the data which must be submitted for a revision to the NFIP regulatory floodway. Contact information for each regional office can be obtained by calling the FEMA Mapping and Insurance eXchange toll free at (877) 336-2627 (877-FEMA MAP) or from our web site at <http://www.fema.gov/about/regoff.htm>.

PORTIONS OF THE PROPERTY REMAIN IN THE SFHA (This Additional Consideration applies to the preceding 1 Property.)

Portions of this property, but not the subject of the Determination/Comment document, may remain in the Special Flood Hazard Area. Therefore, any future construction or substantial improvement on the property remains subject to Federal, State/Commonwealth, and local regulations for floodplain management.

STATE AND LOCAL CONSIDERATIONS (This Additional Consideration applies to all properties in the LOMR-FW DETERMINATION DOCUMENT (REMOVAL))

Please note that this document does not override or supersede any State or local procedural or substantive provisions which may apply to floodplain management requirements associated with amendments to State or local floodplain zoning ordinances, maps, or State or local procedures adopted under the National Flood Insurance Program.

This attachment provides additional information regarding this request. If you have any questions about this attachment, please contact the FEMA Mapping and Insurance eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, LOMC Clearinghouse, 3601 Eisenhower Avenue, Suite 500, Alexandria, VA 22304-6426.

Patrick "Rick" F. Sacbibit, P.E., Branch Chief
Engineering Services Branch
Federal Insurance and Mitigation Administration

Attachment H: Traffic Impact Analysis Memorandum

Memorandum

To: City of Salem

Copy: Trent Michels, FUND
AKS Engineering & Forestry, LLC

From: Jennifer Danziger, PE

Date: July 24, 2024

Subject: The Cannery Transportation Impact Analysis – Addendum 1
Trip Generation Update



RENEWS: 12/31/2025

Introduction

A Transportation Impact Analysis (TIA)¹ was prepared for The Cannery, a proposed mixed-use development located at 1105 Front Street NE in Salem, Oregon. The project consists of three new 6-story residential buildings with ground floor retail and three repurposed buildings. Since the TIA was finalized, some minor changes to the plans have increased the multifamily housing proposed on the site from 371 units to 382 units; the proposal for the three repurposed buildings on the site will remain unchanged.

This memorandum presents an update of the trip generation calculations and concludes that the change in the number of proposed apartments will not affect the conclusions of the TIA.

Trip Generation

Under the current proposal, 382 apartments are proposed in three new 6-story buildings with ground floor commercial space. The three repurposed buildings will house a mix of commercial uses that include a food hall, eating/drinking establishments, event space, a winery, and small business incubator and vendor spaces. These buildings will also include covered outdoor spaces and a flexible plaza space.

The trip generation estimates have been updated using the same assumptions and procedures described in the TIA. The updated estimates are summarized in Table 1; detailed calculations are attached to this memorandum. Note that because of the greater number residential trips, the potential for internal trips between residential and commercial development on the site will go up, which results in some small change to the number of external trips for other site uses as well.

¹ Lancaster Mobley, *The Cannery Transportation Impact Analysis*, June 3, 2024.

Table 1: Trip Generation Summary – Proposed Development

ITE Code	Intensity	Morning Peak Hour			Evening Peak Hour			Daily Trips
		In	Out	Total	In	Out	Total	
221 - Multifamily Housing (Mid-Rise)	382 DU	32	109	141	91	58	149	1,734
	<i>Internal Trips</i>	-3	-16	-19	-25	-18	-43	-368
712 - Small Office Building	5.885 KSF	8	2	10	4	9	13	84
	<i>Internal Trips</i>	-1	-1	-2	-4	-2	-6	-28
822 - Strip Retail Plaza (<40k)	12.160 KSF	17	12	29	40	40	80	662
	<i>Internal Trips</i>	-2	-3	-5	-26	-23	-49	-260
	<i>Pass-by Trips</i>	-1	-1	-2	-3	-3	-6	-60
926 - Food Cart Pods	8 Carts	5	5	10	25	24	49	492
	<i>Internal Trips</i>	-1	-1	-2	-5	-8	-13	-109
	<i>Pass-by Trips</i>	0	0	0	-4	-4	-8	-76
932 - High-Turnover (Sit-Down) Restaurant	12.926 KSF	68	56	124	71	46	117	1,386
	<i>Internal Trips</i>	-17	-3	-20	-13	-20	-33	-307
	<i>Pass-by Trips</i>	-5	-5	-10	-8	-8	-16	-162
970 - Wine Tasting Room	2.925 KSF	4	2	6	11	10	21	134
	<i>Internal Trips</i>	0	0	0	0	0	0	0
975 - Drinking Place	4.309 KSF	0	0	0	32	17	49	490
	<i>Internal Trips</i>	0	0	0	-6	-8	-14	-108
Total Trips		134	186	320	274	204	478	4,982
<i>Internal Trips</i>		-24	-24	-48	-79	-79	-158	-1,180
Total External Trips		110	162	272	195	125	320	3,802
<i>Pass-by/Diverted Trips</i>		-6	-6	-12	-15	-15	-30	-298
Total Primary Trips		104	156	260	180	110	290	3,504

Notes:

1. *Internal trips calculated following the procedures in NCHRP 684.*
2. *Pass-by rates of 10% for morning, 20% for evening, and 15% for daily were applied only to external trips*
DU = dwelling units, KSF = 1,000 square feet of floor area

For the updated proposal, total external trip generation was estimated at 272 morning peak hour, 320 evening peak hour, and 3,802 daily trips. After deducting pass-by traffic, the proposed development is anticipated to generate 260 primary trips during the morning peak hour, 290 primary trips during the evening peak hour, and 3,504 primary trips each weekday.

Table 2 compares the updated trip generation estimates with the estimates presented in Table 5 of the TIA.



Table 2: Trip Generation Comparison

Trip Generation Scenario/Trip Type	Morning Peak Hour			Evening Peak Hour			Daily Trips
	In	Out	Total	In	Out	Total	
TIA Trip Generation with 371 Apartments							
Total Trips	134	182	316	271	203	474	4,932
<i>Internal Trips</i>	-24	-24	-48	-78	-78	-156	-1,168
Total External Trips	110	158	268	193	125	318	3,764
<i>Pass-by/Diverted Trips</i>	-6	-6	-12	-15	-15	-30	-298
Total Primary Trips	104	152	256	178	110	288	3,466
Updated Trip Generation with 382 Apartments							
Total Trips	134	186	320	274	204	478	4,982
<i>Internal Trips</i>	-24	-24	-48	-79	-79	-158	-1,180
Total External Trips	110	162	272	195	125	320	3,802
<i>Pass-by/Diverted Trips</i>	-6	-6	-12	-15	-15	-30	-298
Total Primary Trips	104	156	260	180	110	290	3,504
Net Difference							
Total Trips	0	4	4	3	1	4	50
<i>Internal Trips</i>	0	0	0	-1	-1	-2	-12
Total External Trips	0	4	4	2	0	2	38
<i>Pass-by/Diverted Trips</i>	0	0	0	0	0	0	0
Total Primary Trips	0	4	4	2	0	2	38

After accounting for internal trips and pass-by trips, the increase from 371 to 382 apartments is estimated to increase the primary trip generation for the proposed development by 4 morning peak hour trips, 2 evening peak hour trips, and 38 daily trips.

Operational Impacts

Table 9 of the TIA showed that all study intersections are projected to operate within standards under all analysis scenarios, except for Market Street NE/Center Access & Front Street NE. Operations on the westbound approach of Market Street NE are anticipated to exceed LOS E during the evening peak hour under 2029 buildout conditions although the approach is not expected to be over capacity.

The additional 11 apartments will have no measurable impact on the TIA conclusions. With an estimated increase of 4 morning peak hour trips, all intersections will continue to operate within standards. With a net increase of only 2 trips during the evening peak hour, no change in operations is anticipated.



Conclusions

The increase from 371 to 382 apartments will result in a nominal change in overall trip generation and will have no measurable impact on the conclusions presented in the TIA.

Attachments:

Trip Generation Estimates

Internal Trip Calculations





TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Land Use Subcategory: Not Close to Rail Transit
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **382**

AM PEAK HOUR

Trip Rate: 0.37

	Enter	Exit	Total
Directional Split	23%	77%	
Trip Ends	32	109	141

PM PEAK HOUR

Trip Rate: 0.39

	Enter	Exit	Total
Directional Split	61%	39%	
Trip Ends	91	58	149

WEEKDAY

Trip Rate: 4.54

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	867	867	1,734

SATURDAY

Trip Rate: 4.57

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	873	873	1,746

Source: Trip Generation Manual, 11th Edition



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Small Office Building
Land Use Code: 712
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **5.885**

AM PEAK HOUR

Trip Rate: 1.67

	Enter	Exit	Total
Directional Split	82%	18%	
Trip Ends	8	2	10

PM PEAK HOUR

Trip Rate: 2.16

	Enter	Exit	Total
Directional Split	34%	66%	
Trip Ends	4	9	13

WEEKDAY

Trip Rate: 14.39

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	42	42	84

SATURDAY

Trip Rate: 0

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	NA	NA	NA



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Strip Retail Plaza (<40k)
Land Use Code: 822
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: 12.160

AM PEAK HOUR

Trip Rate: 2.36

	Enter	Exit	Total
Directional Split	60%	40%	
Trip Ends	17	12	29

PM PEAK HOUR

Trip Rate: 6.59

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	40	40	80

WEEKDAY

Trip Rate: 54.45

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	331	331	662

SATURDAY

Trip Rate: 0

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	NA	NA	NA



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Food Cart Pods
Land Use Code: 926
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: Food Carts
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: 8

AM PEAK HOUR

Trip Rate: 1.232

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	5	5	10

* Assumes AM is 20% of PM.

PM PEAK HOUR

Trip Rate: 6.16

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	25	24	49

WEEKDAY

Trip Rate: 61.6

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	246	246	492

* Assumes Daily is 10 x PM.

SATURDAY

Trip Rate: 0

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	NA	NA	NA



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: High-Turnover (Sit-Down) Restaurant
Land Use Code: 932
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **12.926**

AM PEAK HOUR

Trip Rate: 9.57

	Enter	Exit	Total
Directional Split	55%	45%	
Trip Ends	68	56	124

PM PEAK HOUR

Trip Rate: 9.05

	Enter	Exit	Total
Directional Split	61%	39%	
Trip Ends	71	46	117

WEEKDAY

Trip Rate: 107.2

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	693	693	1,386

SATURDAY

Trip Rate: 122.4

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	791	791	1,582



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Wine Tasting Room
Land Use Code: 970
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: 2.925

AM PEAK HOUR

Trip Rate: 2.07

	Enter	Exit	Total
Directional Split	70%	30%	
Trip Ends	4	2	6

PM PEAK HOUR

Trip Rate: 7.31

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	11	10	21

WEEKDAY

Trip Rate: 45.96

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	67	67	134

SATURDAY

Trip Rate: 203.48

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	298	298	596



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Drinking Place
Land Use Code: 975
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **4.309**

AM PEAK HOUR

Trip Rate: 0

	Enter	Exit	Total
Directional Split	0%	0%	
Trip Ends	0	0	0

PM PEAK HOUR

Trip Rate: 11.36

	Enter	Exit	Total
Directional Split	66%	34%	
Trip Ends	32	17	49

WEEKDAY

Trip Rate: 113.6

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	245	245	490

SATURDAY

Trip Rate: 0

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	NA	NA	NA

* Assumes Daily is 10 x PM.

NCHRP 8-51 Internal Trip Capture Estimation Tool			
Project Name:	The Cannery	Organization:	Lancaster Mobley
Project Location:	Salem, OR	Performed By:	JED
Scenario Description:		Date:	
Analysis Year:		Checked By:	
Analysis Period:	AM Street Peak Hour	Date:	

Table 1-A: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				10	8	2
Retail				29	17	12
Restaurant				134	73	61
Cinema/Entertainment				0		
Residential				141	32	109
Hotel				0		
All Other Land Uses ²				0		
Total				314	130	184

Table 2-A: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-A: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-A: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	1	0	0	0
Retail	0		2	0	1	0
Restaurant	1	1		0	2	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	1	15	0		0
Hotel	0	0	0	0	0	

Table 5-A: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	314	130	184
Internal Capture Percentage	15%	18%	13%
External Vehicle-Trips ³	266	106	160
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-A: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	13%	50%
Retail	12%	25%
Restaurant	25%	7%
Cinema/Entertainment	N/A	N/A
Residential	9%	15%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	The Cannery
Analysis Period:	AM Street Peak Hour

Table 7-A: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-A (D): Entering Trips			Table 7-A (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	8	8	1.00	2	2
Retail	1.00	17	17	1.00	12	12
Restaurant	1.00	73	73	1.00	61	61
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	32	32	1.00	109	109
Hotel	1.00	0	0	1.00	0	0

Table 8-A (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		1	1	0	0	0
Retail	3		2	0	2	0
Restaurant	19	9		0	2	2
Cinema/Entertainment	0	0	0		0	0
Residential	2	1	22	0		0
Hotel	0	0	0	0	0	

Table 8-A (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		5	17	0	0	0
Retail	0		37	0	1	0
Restaurant	1	1		0	2	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	3	15	0		0
Hotel	0	1	4	0	0	

Table 9-A (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	1	7	8	7	0	0
Retail	2	15	17	15	0	0
Restaurant	18	55	73	55	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	3	29	32	29	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-A (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	1	1	2	1	0	0
Retail	3	9	12	9	0	0
Restaurant	4	57	61	57	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	16	93	109	93	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool			
Project Name:	The Cannery	Organization:	Lancaster Mobley
Project Location:	Salem, OR	Performed By:	JED
Scenario Description:		Date:	
Analysis Year:		Checked By:	
Analysis Period:	PM Street Peak Hour	Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				13	4	9
Retail				80	40	40
Restaurant				215	128	87
Cinema/Entertainment				0	0	0
Residential				149	91	58
Hotel				0	0	0
All Other Land Uses ²				0	0	0
Total				457	263	194

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	0	0	0	0
Retail	1		12	0	10	0
Restaurant	1	20		0	15	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	4	12	0		0
Hotel	0	0	0	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	457	263	194
Internal Capture Percentage	35%	30%	41%
External Vehicle-Trips ³	299	184	115
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	100%	22%
Retail	65%	58%
Restaurant	19%	41%
Cinema/Entertainment	N/A	N/A
Residential	27%	31%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	The Cannery
Analysis Period:	PM Street Peak Hour

Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	4	4	1.00	9	9
Retail	1.00	40	40	1.00	40	40
Restaurant	1.00	128	128	1.00	87	87
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	91	91	1.00	58	58
Hotel	1.00	0	0	1.00	0	0

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	0	0	0	0
Retail	1		12	2	10	2
Restaurant	3	36		7	16	6
Cinema/Entertainment	0	0	0		0	0
Residential	2	24	12	0		2
Hotel	0	0	0	0	0	

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		3	3	0	4	0
Retail	1		37	0	42	0
Restaurant	1	20		0	15	0
Cinema/Entertainment	0	2	4		4	0
Residential	2	4	18	0		0
Hotel	0	1	6	0	0	

Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	4	0	4	0	0	0
Retail	26	14	40	14	0	0
Restaurant	24	104	128	104	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	25	66	91	66	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	2	7	9	7	0	0
Retail	23	17	40	17	0	0
Restaurant	36	51	87	51	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	18	40	58	40	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.