



Community Planning and Development

555 Liberty St. SE / Room 305 • Salem, OR 97301-3503 • 503-588-6173 • www.cityofsalem.net

March 9, 2024

LAND USE APPLICATION COMPLETENESS REVIEW

Subject Property: 2110 Strong Road SE

Reference Nos.: 24-103231-PLN (Class 3 Site Plan Review; Class 2 Adjustment)

Applicant: Inder Singh Dhaliwal
Gurkirpa LLC
3997 Carson Drive SE
Salem, OR 97321

Phone:
E-Mail:

Agent: Britany Randall
BRAND Land Use
12150 Jefferson Hwy 99E SE
Jefferson, OR 97352


Phone: 503-680-0949
E-Mail: britany@brandlanduse.com

The Planning Division has conducted its completeness review of the proposed Class 3 Site Plan Review and Class 2 Adjustment for property located at 2110 Strong Road SE. In order to deem the application complete and to continue processing the application, modifications/and or additional information is needed to address the following item(s):

Item:	Description:
Application Form	SRC 300.210(a)(1)(G) requires land use applications to be signed by the applicant, owner of the property, and/or the duly authorized representative. The land use application form has been submitted by it has not yet been signed by the property owner. The owner of the subject property, Inder Singh Dhaliwal, needs to sign the application form authorizing its submittal.
Homeowners' Association Name & Contact Info.	For properties subject to an active and duly incorporated Homeowners' Association (HOA), SRC 300.210(a)(10) requires submittal of the name and mailing address of the Homeowners' Association. The application materials submitted indicate that the HOA for this property was contacted but the name and mailing address of the HOA have not yet been provided. The name and mailing address of the HOA is therefore required.
Written Statement	SRC 300.210(a)(9) requires land use applications to be accompanied by a written statement addressing each applicable approval criterion and standard. No written statement has been provided with the application including findings addressing the proposal's conformance with the applicable Class 3 Site Plan Review and Class 2 Adjustment approval criteria.

Item:	Description:
	Findings addressing the proposal's conformance with these criteria are needed.
Geologic Assessment	According to the City's adopted landslide hazard susceptibility maps, the subject property contains an area of two mapped landslide hazard susceptibility points. Pursuant to the requirements of the City's landslide hazard ordinance (SRC Chapter 810), there are three activity points associated with building permit applications for commercial developments. The cumulative total of five points, between that which is associated with the land and that which is associated with the proposed development activity, indicates a moderate landslide hazard susceptibility risk. A geologic assessment is therefore required in conjunction with the proposed development. In review of the application materials submitted, no geologic assessment has currently been provided.
Retaining Wall Heights	Based on the grading plan provided, a retaining wall is proposed along the north and east sides of Village Center Loop. It appears that in places the retaining wall could reach a height of roughly nine feet. Beyond the retaining wall, the grade will slope up a few additional feet until it reaches the sidewalk along Village Center Loop. Elevations of the retaining walls are needed depicting them in relation to the proposed grading on the site. In addition, due to the substantial change in elevation between the sidewalk along Village Center Loop and the parking lot proposed to serve the development, what safety considerations are being considered to be included to ensure pedestrian safety along Village Center Loop. Are railings proposed along the top of the retaining wall to prevent potential falls?
Building Elevation Drawings	Because the elevation drawings of the building will be included with the notice that is sent to the public, the elevations should be labeled (<i>e.g. North, South, East, and West</i>) so as to more easily identify which portion of the building is being shown and reduce the potential for confusion from the public.
Trash Enclosure Details	The details of the proposed trash enclosure (<i>including but not limited to the size and layout of proposed trash containers, the type and height of enclosure, gate swings, etc.</i>) are need in order to determine whether the proposed trash enclosure area will comply with the requirements of SRC 800.055 . In addition, due to the compact nature of this site and the narrowness of the proposed entrance and exit driveways, confirmation is needed whether a garbage truck will be able to navigate the turns within the parking lot in order to service the trash area.
Additional Class 2 Adjustments Required & Additional Application Fees	<p>Based on review of the development as currently proposed, the following additional adjustments are required:</p> <ul style="list-style-type: none"> ▪ Min. 0.75 FAR: The refinement plan requires new developments within the Village Center (VC) are to have a minimum Floor-Area-Ratio (FAR) of 0.75. Based on the square footage of the proposed building and the total area of the site (<i>exclusive of the private street right-of-way of Village Center Loop</i>), the resulting FAR of the development is 0.264, which falls below the minimum required 0.75. A Class 2 Adjustment to this standard is required. ▪ Min. Required Building Frontage Adjacent to Strong Road SE & Lindburg Road SE: The refinement plan requires a minimum of 70 percent of the lot frontages of Strong Road SE and Lindburg Road SE to be occupied by buildings placed at the minimum setback line. As shown on the plans, none of the frontages of the lot abutting Strong Road or Lindburg Road are occupied by buildings places at the minimum setback line. The proposed development

Item:	Description:
	<p>therefore does not currently meet this standard. The application materials submitted indicate that a Class 2 Adjustment is being requested to reduce lot frontage standard below the minimum required 40 percent. The application materials will need to be updated to reflect that the lot frontage standard of the refinement plan is a minimum of 70 percent rather than a minimum of 40 percent.</p> <p>In addition, because this standard applies to two streets, both Lindburg Road and Strong Road, two adjustments will be needed to the standard – one to reduce the requirement abutting Lindburg Road and one to reduce the requirement abutting Strong Road.</p> <ul style="list-style-type: none"> ▪ <u>Side Street Setback:</u> The refinement plan requires a minimum building setback of 10 feet and a maximum building setback of 20 feet for side yards abutting streets. The proposed development meets the required side setbacks abutting Lindburg Road but does not, however, meet required side setbacks abutting Village Center Loop to the west. The maximum 20-foot building setback is exceeded and a Class 2 Adjustment to this standard is needed. ▪ <u>Parking Lot Setback from Streets:</u> The refinement requires parking lots to be setback a minimum of 20 feet from streets. The proposed parking lot is currently setback more than 20 feet from Lindburg Road SE and Strong Road SE but it is not, however, currently setback more than 20 feet from Village Center Loop. On the site plan a 20-foot setback distance is indicated between the parking lot and the paved travel lane of Village Center Loop. However, required setbacks adjacent the private street are measured from the inside edge of the sidewalk (<i>because the sidewalk is part of the private street right-of-way for the street</i>). A Class 2 Adjustment to this standard is needed. <p>Because additional adjustments will be required, there will be additional application fees applicable to the application. Once the final number of required adjustments has been identified the requisite additional application fees will be billed to the folder so they can be paid by the applicant.</p>
<p>Findings in Support of Adjustments</p>	<p>In order for the proposed development to be approved adjustments are required to several development standards of the code, including adjustments to standards that are intended to promote a more intensive development patten within the Village Center Area.</p> <p>Based on the information currently provided it's not clear how the proposed development otherwise equally or better meets the intent of some of the development standards that will be required to be adjusted.</p> <p>In order for the development to equally or better meet the underlying purpose of the refinement plan standards, staff recommends the building design be modified to include the following elements that are required for buildings in the City's MU-I (Mixed-Use-I) zone:</p> <ul style="list-style-type: none"> ▪ A minimum ground floor building height of 14 ft. ▪ Ground floor windows on a minimum of 65 percent of the ground floor facades of the building facing Strong Road and Lindburg Road. ▪ Weather protection in the form of a canopy or awning along a minimum of 75 percent of the ground floor façade of the building facing Strong Road and

Item:	Description:
	<p>Lindburg Road.</p> <ul style="list-style-type: none"> ▪ Building façade articulation that more clearly defines the base, middle, and top portions of the building’s facades. <p>The above recommended building elements would serve to offset the smaller building size and reduced lot frontage by ensuring that those portions of the building which are located in proximity to the public street are designed to visually reinforce and support an active and inviting pedestrian environment at the intersection of Strong Road and Lindburg which is the key most prominent and significant street intersection within the Fairview Training Site located in the core of the Village Center area.</p>
<p>Transit Improvements</p>	<p>The Salem-Keizer Transit District (Cherriots) reviewed the proposal at the time of pre-application conference and provided comments pertaining to the need for a transit stop abutting the property on Strong Road to correspond to the stop that is required to be installed on the other side of the street. As discussed at the pre-application conference, the required bus stop will need to be designed to be able to bridge the existing stormwater swale facility within Strong Road and Cherriots indicates that the stop will need to be located approximately 10 feet from the storm drain closest to the corner as indicated by the blue dot in the below screenshot in order to ensure the bus will be able to serve the stop without running over the drain and potentially damaging it.</p>  <p>Included for reference are photos provided by Cherriots showing different examples of stormwater swale bridge designs that could be incorporated into the design of the required transit stop. Cherriots’ standard bus stop designs are also included for reference. Additional questions about the required transit stop can be addressed by Jolynn Franke with Cherriots. Jolynn’s contact information is as follows:</p> <ul style="list-style-type: none"> ▪ Jolynn Franke (Cherriots): jolynn.franke@cherriots.org / 503-361-7505
<p>Aeronautical Evaluation Required</p>	<p>At the time of pre-application conference the Oregon Department of Aviation (ODAV) provided comments indicating that the proposed development is required to undergo aeronautical evaluations by the Federal Aviation</p>

Item:	Description:
	Administration (FAA) and the ODAV. Confirmation of whether this process has started is needed.
Additional Comments on Plans	Please see the additional staff comments included on the attached plans.

Your application, which is incomplete, will be deemed complete upon receipt of one of the following:

- 1) All of the missing information;
- 2) Some of the missing information and written notice from you (the applicant) that no other information will be provided; or
- 3) Written notice from you (the applicant) that none of the missing information will be provided.

You have 180 days from the date the application was first submitted to respond in one of the three ways listed above, or the application will be deemed void.

For questions regarding any of the above requirements, please feel free to contact me directly by calling (503) 540-2399 or via e-mail at bbishop@cityofsalem.net.

The Salem Revised Code may be accessed online at the following location:

<https://www.cityofsalem.net/government/laws-rules/salem-revised-code>

Sincerely,

Bryce Bishop
Planner III

Floor-Area-Ratio (FAR):

- Min.: 0.75 FAR
- Lot Area (Exclusive of Private Street): 30,265.65 ft.²
- Total Building Floor Area: 8,000 ft.²
- Resulting FAR: 0.264 (Not OK - Min. FAR not met)

Building Frontage Abutting Strong Road:

- Min.: 70% of lot frontage occupied by buildings at minimum setback line
- Lot Frontage (Exclusive of Private Street): 192.38 ft.
- Total Building at Min. Setback Line: 0 ft.
- Resulting Building Frontage: 0% (Not OK - Adjustment requested to standard)

Building Frontage Abutting Lindburg Road:

- Min.: 70% of lot frontage occupied by buildings at minimum setback line
- Lot Frontage (Exclusive of Private Street): 87.54 ft.
- Total Building at Min. Setback Line: 0 ft.
- Resulting Building Frontage: 0% (Not OK - Adjustment requested to standard)

SITE PLAN GENERAL NOTES:

- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVES. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
- REFER TO CIVIL DRAWINGS FOR GRADING. SITE IS REQUIRED TO MEET THE LAWS OF FHA AND ADA. ACCESSIBLE ROUTES SHALL NOT EXCEED 5% (1 IN 20) OR CROSS SLOPES SHALL NOT EXCEED 2% (1 IN 50). ALL AT GRADE SIDEWALKS ARE ACCESSIBLE ROUTES.
- JOINTS IN CONCRETE WALKS NOTED AS E.J. ARE TO BE CONSTRUCTED AS EXPANSION JOINTS. ALL OTHER JOINTS SHOWN, TO BE TOOLED CONTROL JOINTS, SEE CIVIL.
- PROVIDE CONSTRUCTION FENCING AS REQUIRED TO SECURE SITE AND BUILDING DURING CONSTRUCTION.
- SEE LANDSCAPE DRAWINGS FOR LANDSCAPE AND IRRIGATION ELEMENTS.
- EXTREME CARE SHOULD BE TAKEN TO PRESERVE EXISTING ROOTS OF TREES TO REMAIN.
- SEE ELECTRICAL DRAWINGS FOR SITE LIGHTING.

SITE DEVELOPMENT CODE REVIEW:

- SITE AREA: 47,069.47 sf = 1.08 ac
NET SITE AREA: 35,242.10 sf = 0.809ac
- ZONING: FMU/VC Fairview Mixed Use - Village Center
- SURFACE AREA:
• LANDSCAPING: 11,917 SF
• BUILDING: 8,000 SF
• SIDEWALKS/PARKING/ON-SITE ROADWAY: 27,152 SF
- BUILDING AREAS:
• BLDG 1 LEVEL 01: 8,000 sf Retail
- PARKING:
• PARKING PROVIDED: 30 TOTAL (EXISTING 26 NEW)
• MAX PARKING FOR SHOPPING CENTER: 1 SPACE/200 SQ FT
40 MAX PARKING ALLOWED
30 TOTAL PROVIDED
- BIKE PARKING:
• BICYCLE PARKING FOR SHOPPING CENTER. THE GREATER OF 4 SPACES OR 1 SPACE PER 5,000 SQUARE FEET
4 SPACES REQUIRED
4 SPACES PROVIDED
- LOADING SPACES:
• MINIMUM LOADING FOR RETAIL SALES AND SERVICE
1 SPACE FOR BUILDINGS 5,000 TO 60,000 SQ FT
REQUIRED DIMENSIONS: 12' X 30'

The four existing spaces located within the private street right-of-way of Village Center Loop were counted as spaces for phase 2 of The Grove Apartments.

The minimum number of required bicycle parking spaces are established under the refinement plan rather than SRC 806. The refinement plan requires:

-Min. 1 space per 500 square feet (for non-residential).

The proposed 8,000 square-foot building therefore requires a minimum of 16 spaces (8 racks).

No bike spaces are currently shown on the site plan. The bike spaces are required to be located and developed in conformance with the bike parking standards included under SRC 806.060.

SITE PLAN NOTES:

- 1 PROPERTY LINE.

Refinement plan requires parking lots to be setback a minimum of 20 feet from streets. Parking lot is currently setback less than 20 feet from Village Center Loop.

Max. 20 ft. side setback exceeded abutting Village Center Loop.

Max. 20 ft. side setback exceeded abutting Village Center Loop.

Exterior Wall Length: Refinement plan limits exterior wall length to a maximum of 100 feet without a minimum 4-foot change in plane. Building off-set is required to be added in this section of the facade to meet the standard.

Is there a way to provide a pedestrian connection from the sidewalk on Village Center Drive to the entrances of the retail building? This would greatly benefit all the residents of the surrounding multiple family development.

It seems like this would make for a simple pedestrian connection to connect the building entrances at the rear of the building to Lindburg Road.

The parking lot drive-aisle for these standard size stalls projects into the entrance driveway.

Parking lot driveway aisle serving a combination of standard and compact size stalls is required to be a minimum of 24-feet in width.

Is there a way to provide a pedestrian connection across Village Center Loop in a convenient location in order to provide a way for residents in the multiple family development to the south to easily cross the street and get access to the proposed retail stores?

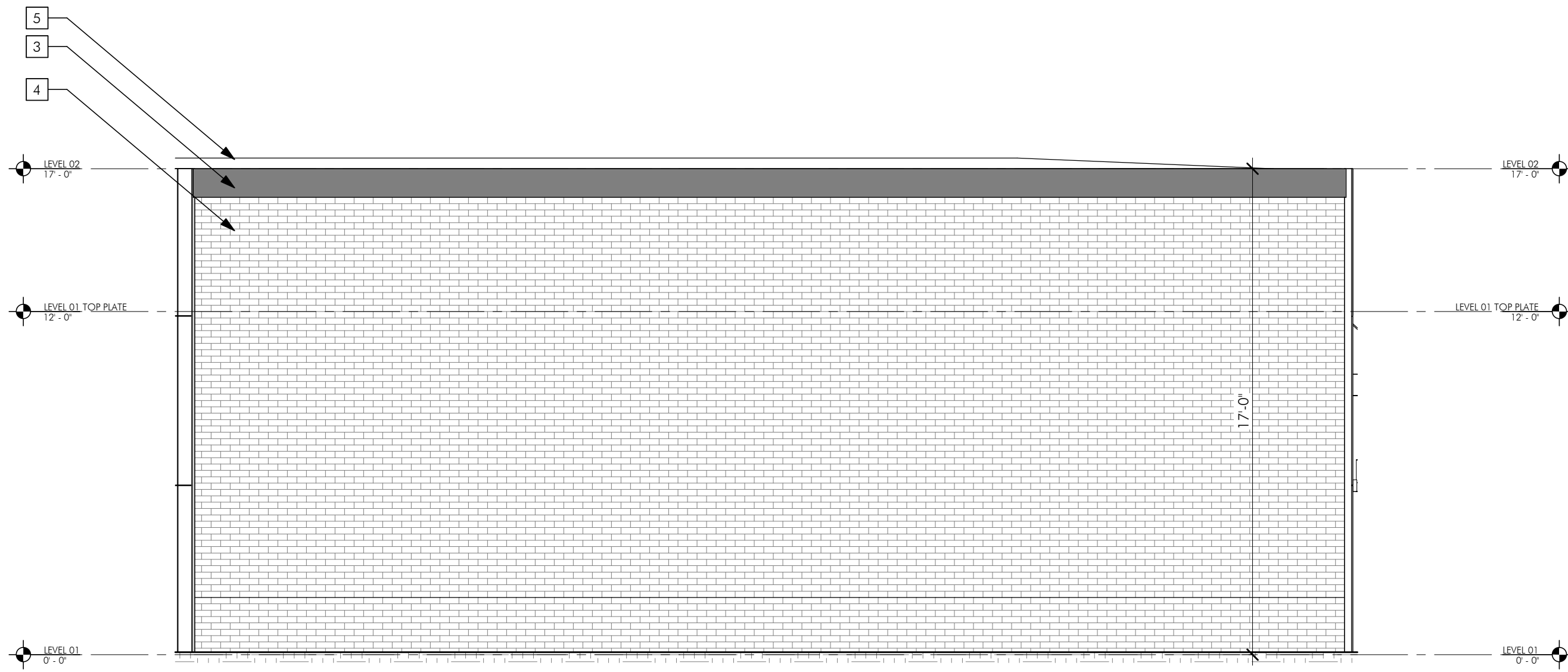


ELEVATION NOTES:

- 1 REDIDER OKO SKIN FIBER CEMENT BOARDS. COLOR: VARIATIONS FROM TIMBER LINE
- 2 FIBER CEMENT PANEL. COLOR:TBD
- 3 FIBER CEMENT PANEL. COLOR 2:TBD
- 4 STONE VENEER
- 5 PRE-FINISHED METAL PARAPET CAP.
- 6 STEEL CANOPY. PAINT FINISH. PREFINISHED METAL ROOF. LIGHTING PROVIDED UNDER ROOF.
- 7 ALUMINUM STOREFRONT. DOOR/WINDOW.
- 8 ALUMINUM FRAMED WINDOW.
- 9 EXTERIOR UP/DOWN SCONCE LIGHT.

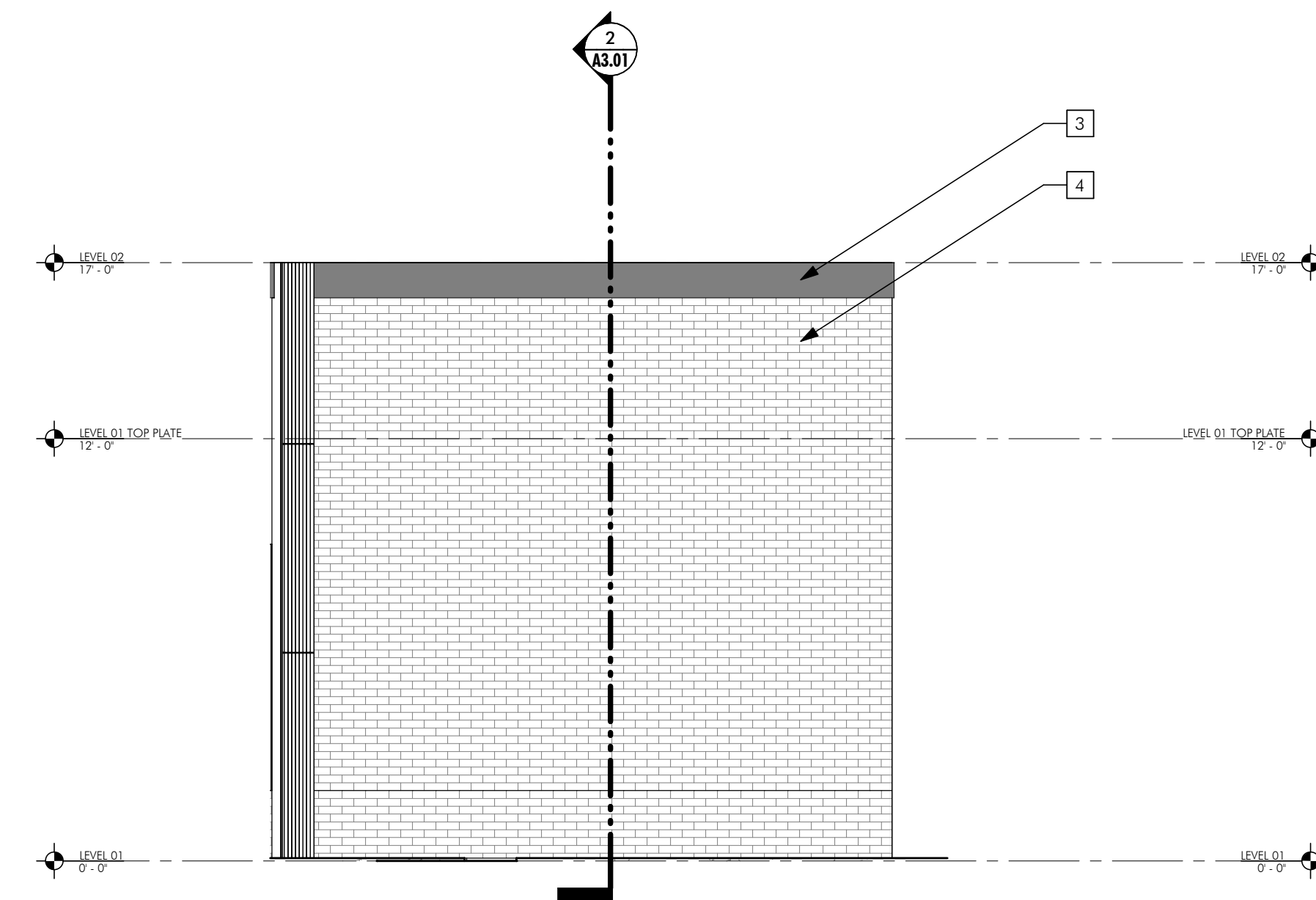
IN THE EVENT CONFLICTS ARE DISCOVERED BETWEEN THE ORIGINAL SIGNED AND SEALED DOCUMENTS PREPARED BY THE ARCHITECTS AND/OR THEIR CONSULTANTS, AND ANY COPY OF THE DOCUMENTS TRANSMITTED BY MAIL, FAX, ELECTRONICALLY OR OTHERWISE, THE ORIGINAL SIGNED AND SEALED DOCUMENTS SHALL GOVERN.

PROJECT # 2023-032
DATE: 2/2/2023
REVISIONS



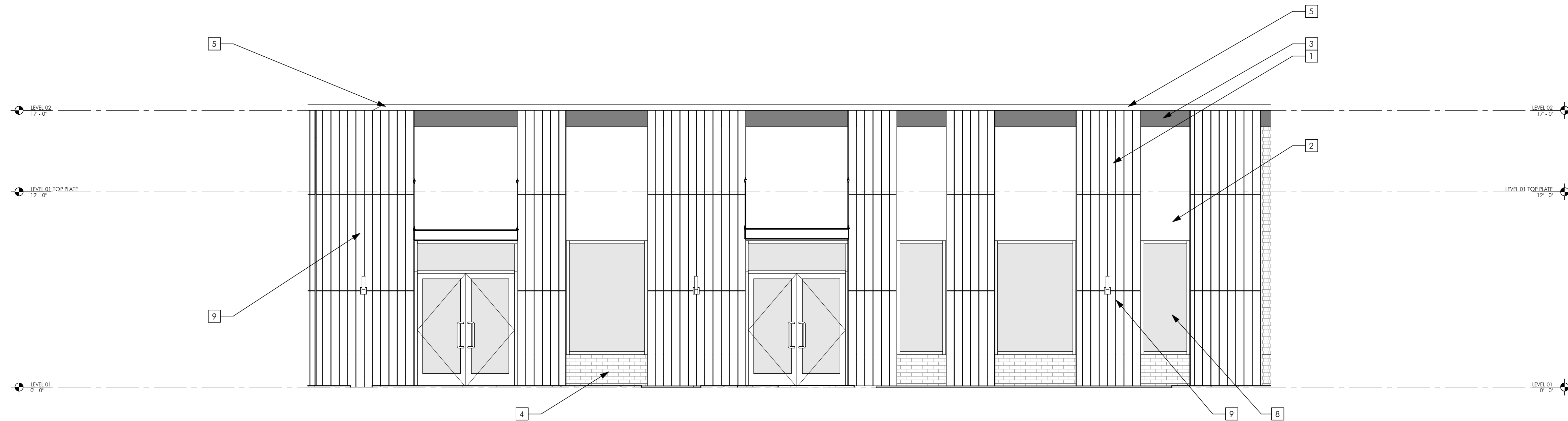
1 Building Elevation 1

Building Elevation Label Needed



2 Building Elevation 2

Building Elevation Label Needed



3 Building Elevation 3

Building Elevation Label Needed

New Retail Building

Fairview

2110 Strong Road SE - Salem, OR 97302

SHEET:

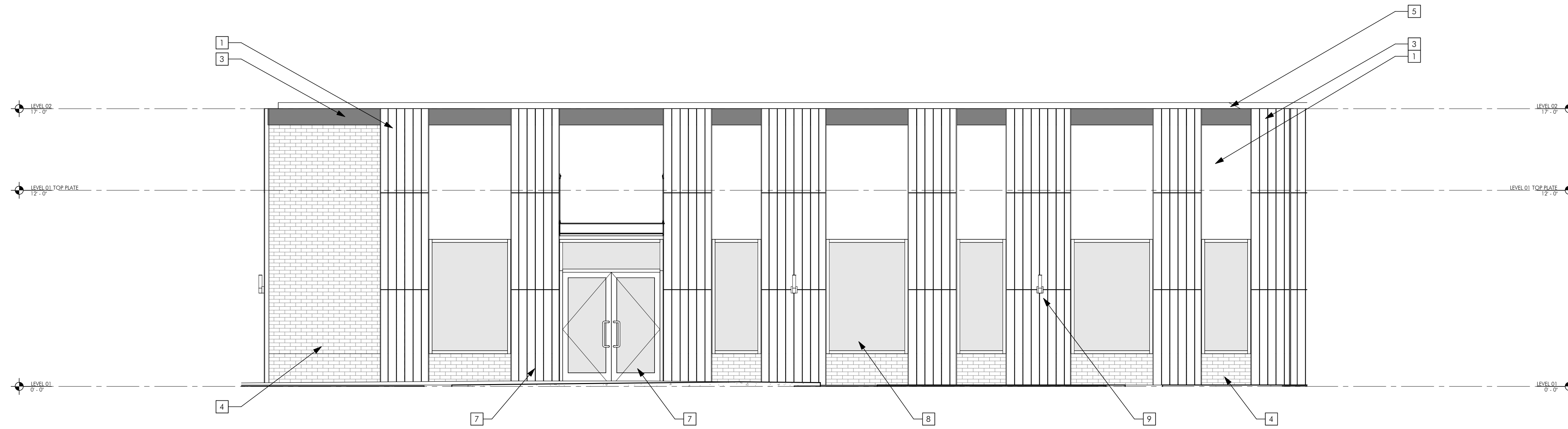
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AND/OR THEIR CONSULTANTS, AND ANY COPY OF
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PROJECT # 2023-032
DATE: 2/2/2023
REVISIONS

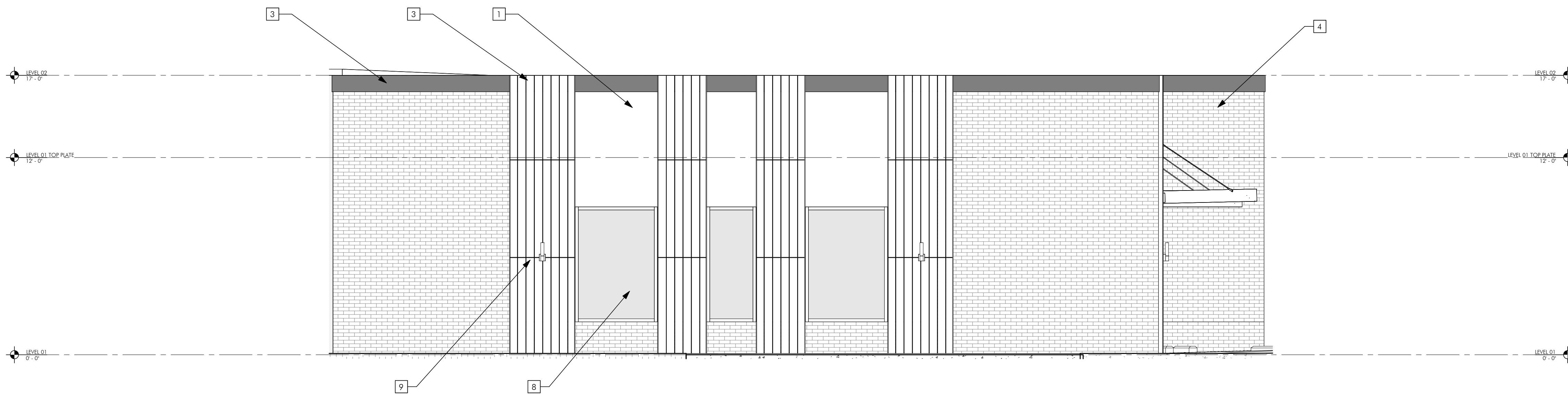
ELEVATION NOTES:

- 1 REDIDER OKO SKIN FIBER CEMENT BOARDS. COLOR: VARIATIONS FROM TIMBER LINE
- 2 FIBER CEMENT PANEL. COLOR:TBD
- 3 FIBER CEMENT PANEL. COLOR 2:TBD
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- 8 ALUMINUM FRAMED WINDOW.
- 9 EXTERIOR UP/DOWN SCONCE LIGHT.



6 Building Elevation 4
0' 4' 8' 16' 24' 48'
1/4" = 1'-0"

Building Elevation Label Needed



1 Building Elevation 5 West Elevation
0' 4' 8' 12' 16' 24' 3'

New Retail Building
Fairview
2110 Strong Road SE - Salem, OR 97302

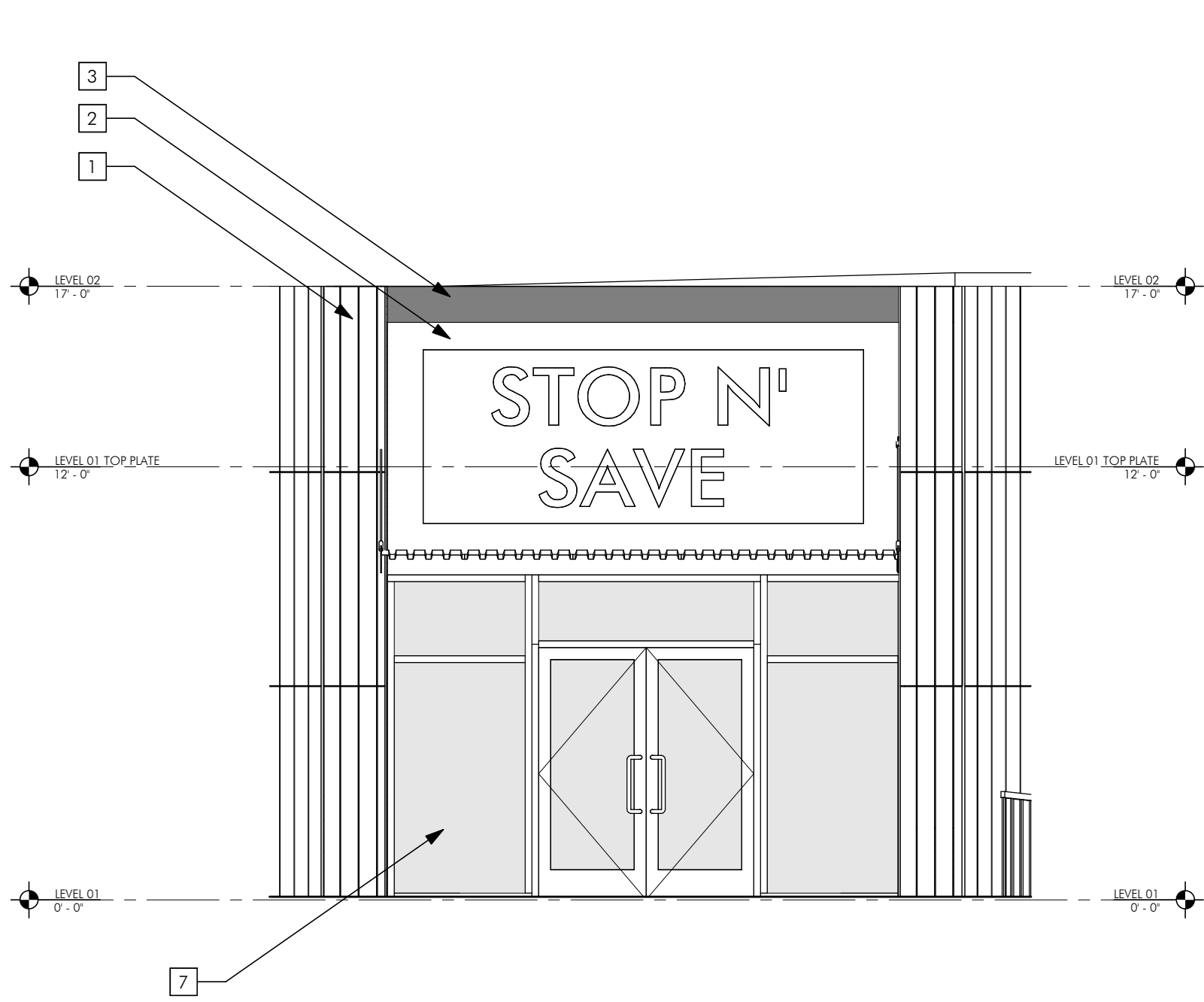
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REVISIONS

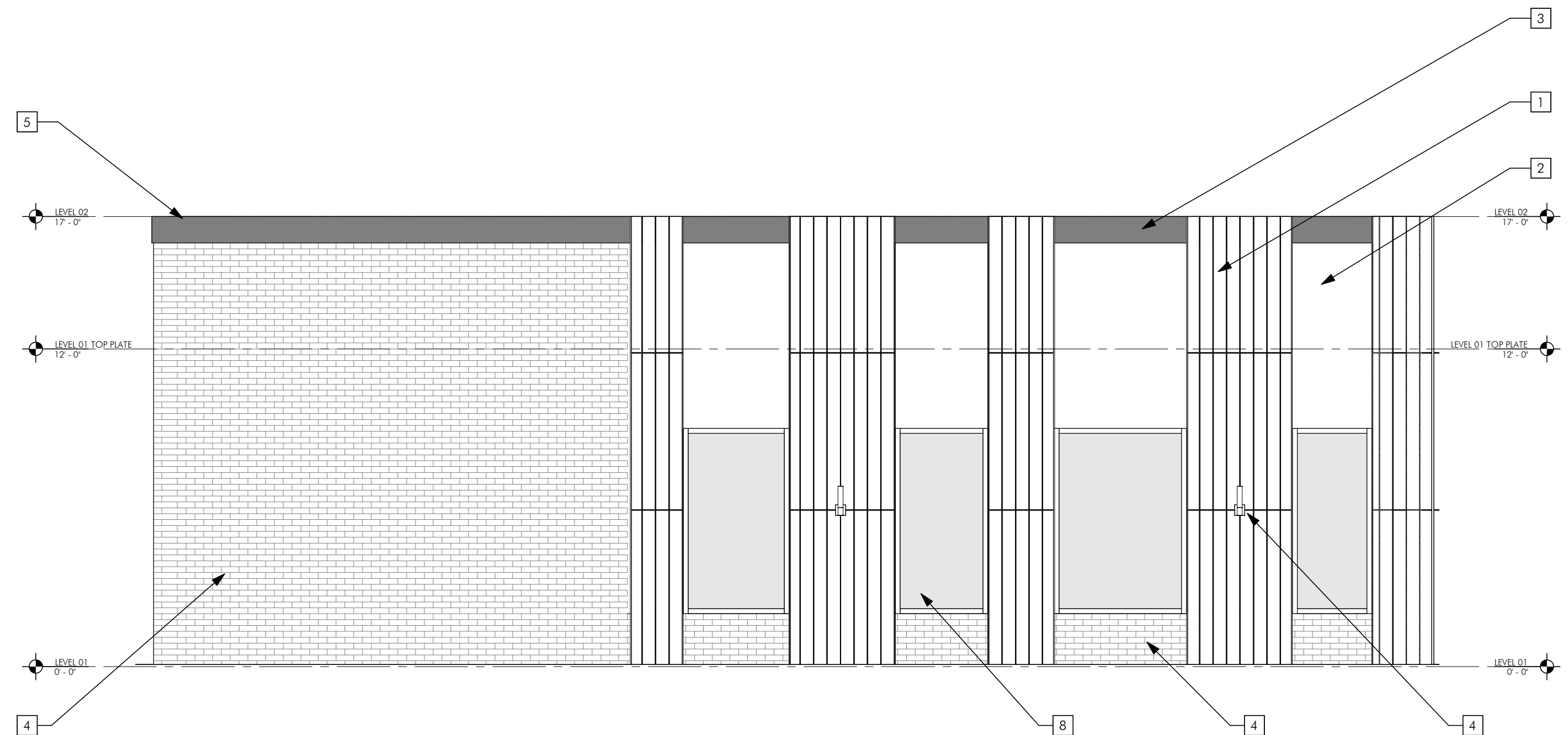
ELEVATION NOTES:

- 1 REDIDER OKO SKIN FIBER CEMENT BOARDS. COLOR: VARIATIONS FROM TIMBER LINE
- 2 FIBER CEMENT PANEL. COLOR:TBD
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- 8 ALUMINUM FRAMED WINDOW.
- 9 EXTERIOR UP/DOWN SCONCE LIGHT.



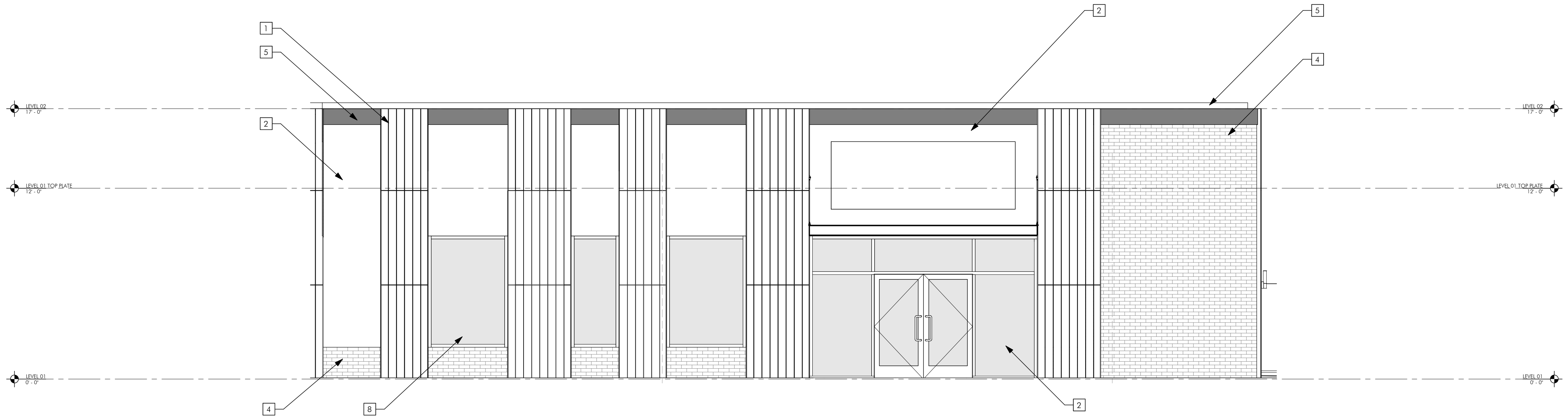
1 Building Elevation 6

Building Elevation Label Needed



2 Building Elevation 7

Building Elevation Label Needed



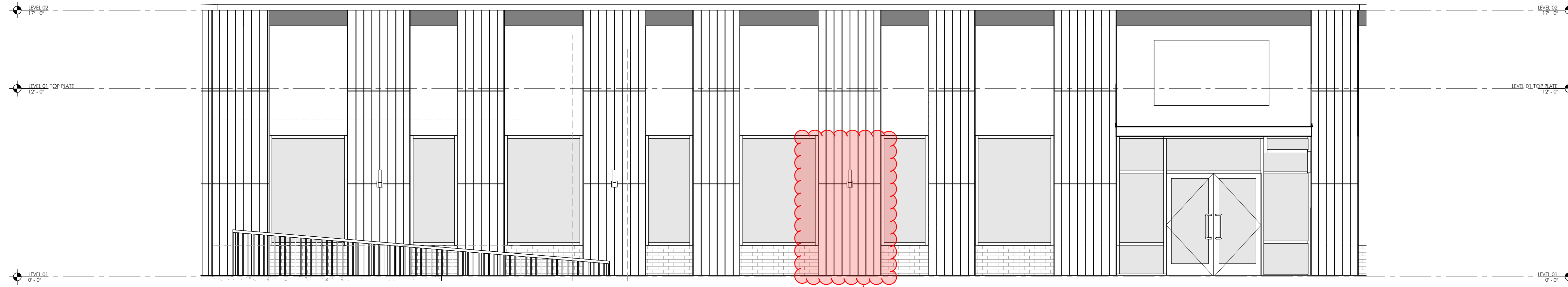
3 Building Elevation 8

Building Elevation Label Needed

New Retail Building
Fairview
2110 Strong Road SE - Salem, OR 97302

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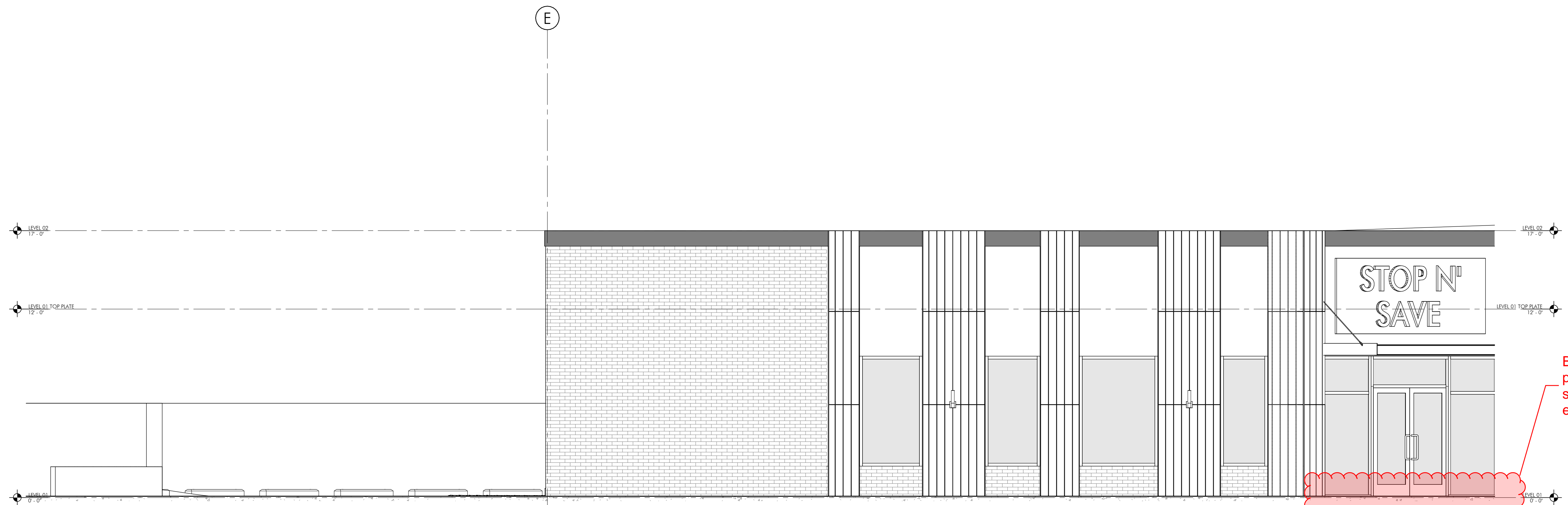
PROJECT # 2023-032
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REVISIONS



Building elevation does not match site plan. The site plan shows a proposed building entrance in this general location.

2 Building Elevation 9
0 1 2 4 8 12
1/4" = 1'-0"

North Elevation



Building elevation does not show proposed grade change from street with steps up to front corner entry.

1 Building Elevation 10
0 1 2 4 8 12
1/4" = 1'-0"

East Elevation

New Retail Building

Fairview

2110 Strong Road SE - Salem, OR 97302

SHEET:

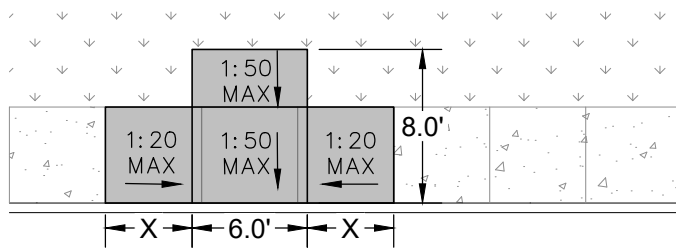
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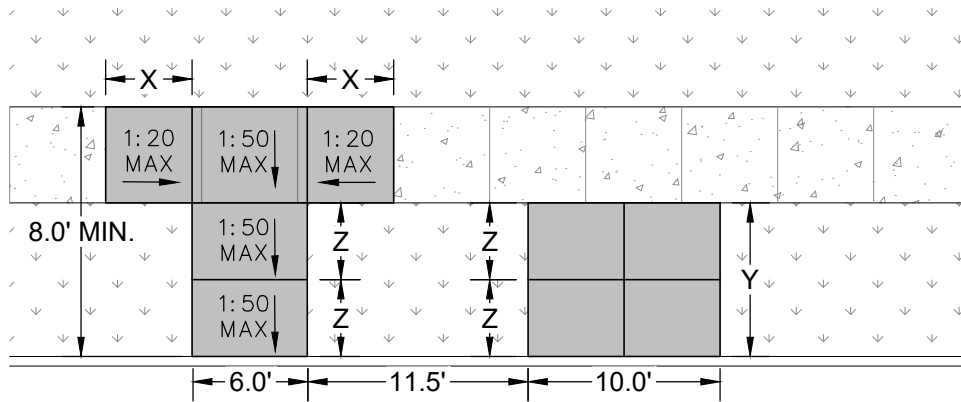
EXISTING ROADWAY

BUS STOP PADS - CURB LINE SIDEWALK

NOT TO SCALE

NOTES:

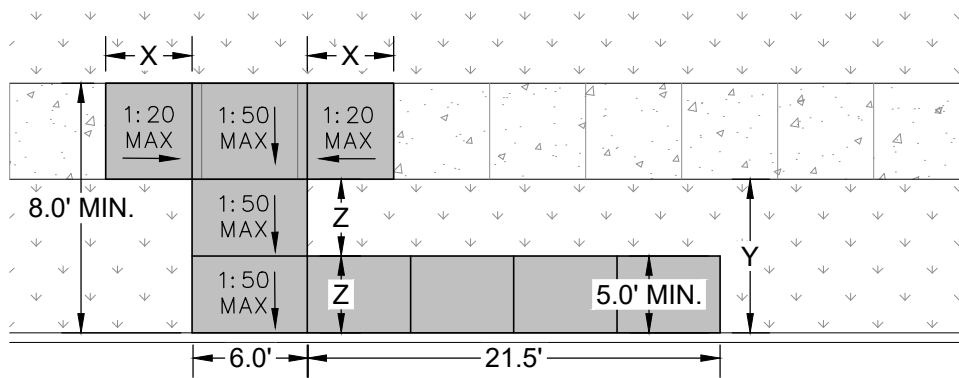
1. "X" WIDTH TO BE A MINIMUM OF 3.0' OR MATCH EXISTING SIDEWALK JOINT SPACING.
2. "Y" LENGTH TO BE EQUAL TO THE WIDTH OF THE PLANTER STRIP, FROM BACK OF CURB TO FRONT OF SIDEWALK.
3. JOINT SPACING, "Z" SHALL BE 3' MINIMUM, 6' MAXIMUM. PROVIDE SIDEWALK PANELS THAT ARE AS SQUARE AS POSSIBLE.
4. MATCH EXISTING WIDTH WHERE PROPOSED BUS STOP PANELS CONNECT TO THE EXISTING WALK.



EXISTING ROADWAY

BUS STOP PADS - PLANTER STRIP (12' WIDE OR LESS)

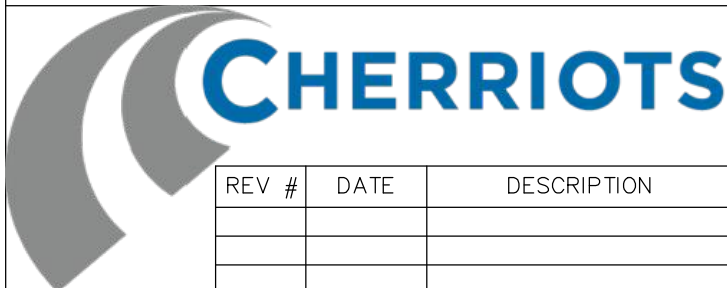
NOT TO SCALE



EXISTING ROADWAY

BUS STOP PADS - PLANTER STRIP (WIDER THAN 12')

NOT TO SCALE



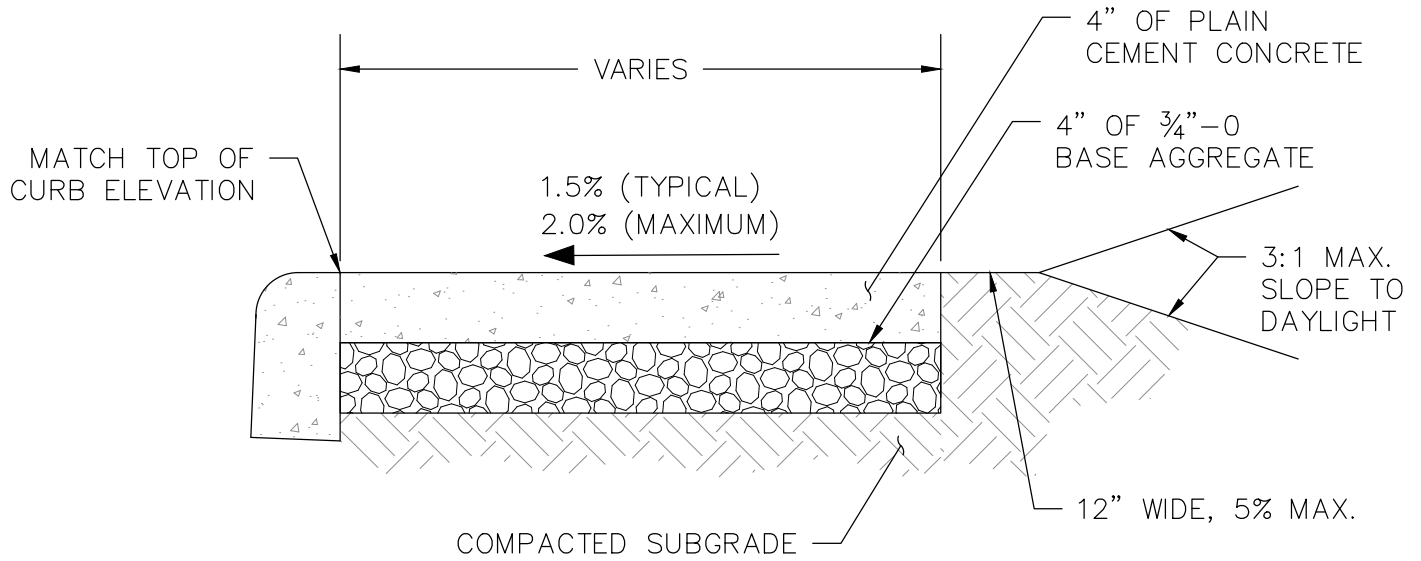
BUS STOP PADS LAYOUT

REV #	DATE	DESCRIPTION

BY EGW
DATE 03/03/22

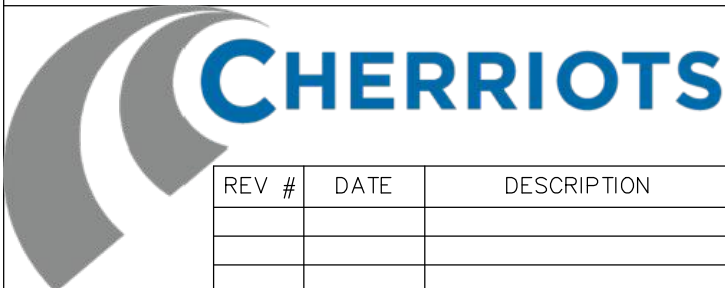
CHECKED RDV
DATE 03/03/22

C1



NOTES:

1. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 P.S.I. AT 28 DAYS.
2. CONTRACTION JOINTS OF THE WEAKENED PLANE TYPE SHALL BE 1-1/4" DEEP AND TOOL ROUNDED BEFORE BROOMING. MATCH EXISTING JOINT SPACING. 15' MAXIMUM.
3. EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, ADA CURB RAMPS, AND AT SPACING NOT TO EXCEED 45 FEET.
4. FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROWELED JOINT WITH A MIN. 1/2 INCH RADIUS.
5. FINISH WITH BROOM AND EDGE ALL JOINTS.
6. ALL EDGES SHALL BE TOOL ROUNDED AND SHINED PER JURISDICTIONAL REQUIREMENTS AFTER BROOMING. PROVIDE 3" SHINE IF NO OTHER REQUIREMENTS EXIST.
7. BASE AGGREGATE TO BE COMPACTED TO 95% OF AASHTO T-99.



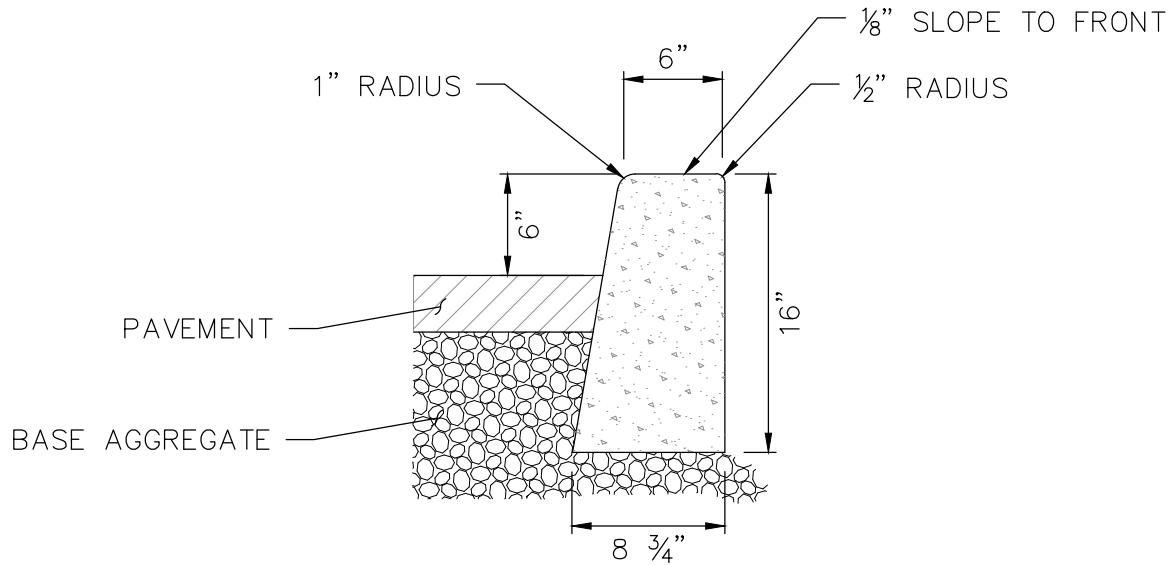
**STANDARD
SIDEWALK**

C2

REV #	DATE	DESCRIPTION

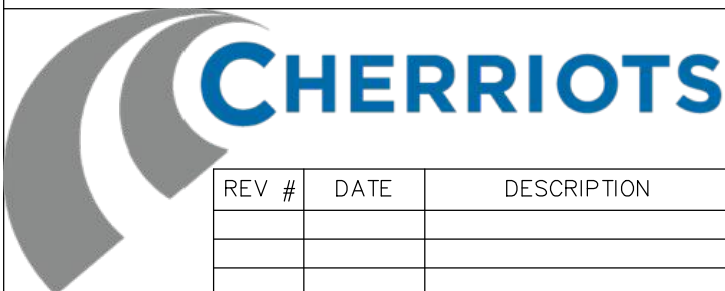
BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22



NOTES:

1. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 P.S.I. AT 28 DAYS.
2. EXPANSION JOINTS
 - 2.A. TO BE PROVIDED:
 - 2.A.1. AT EACH POINT OF TANGENCY OF THE CURB.
 - 2.A.2. AT EACH COLD JOINT.
 - 2.A.3. AT EACH SIDE OF THE INLET STRUCTURES.
 - 2.A.4. AT EACH END OF DRIVEWAYS.
 - 2.A.5. AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
3. CONTRACTION JOINTS:
 - 3.A. SPACING TO BE NOT MORE THAN 15 FEET.
 - 3.B. THE DEPTH OF THE JOINT SHALL BE AT LEAST 1-1/2 INCHES.
4. BASE AGGREGATE TO BE 1 1/2"-0" OR 3/4"-0" COMPACTED TO 95% OF AASHTO T-99 AND SHALL BE TO SUBGRADE, STREET STRUCTURE, OR 4" IN DEPTH, WHICHEVER IS GREATER.

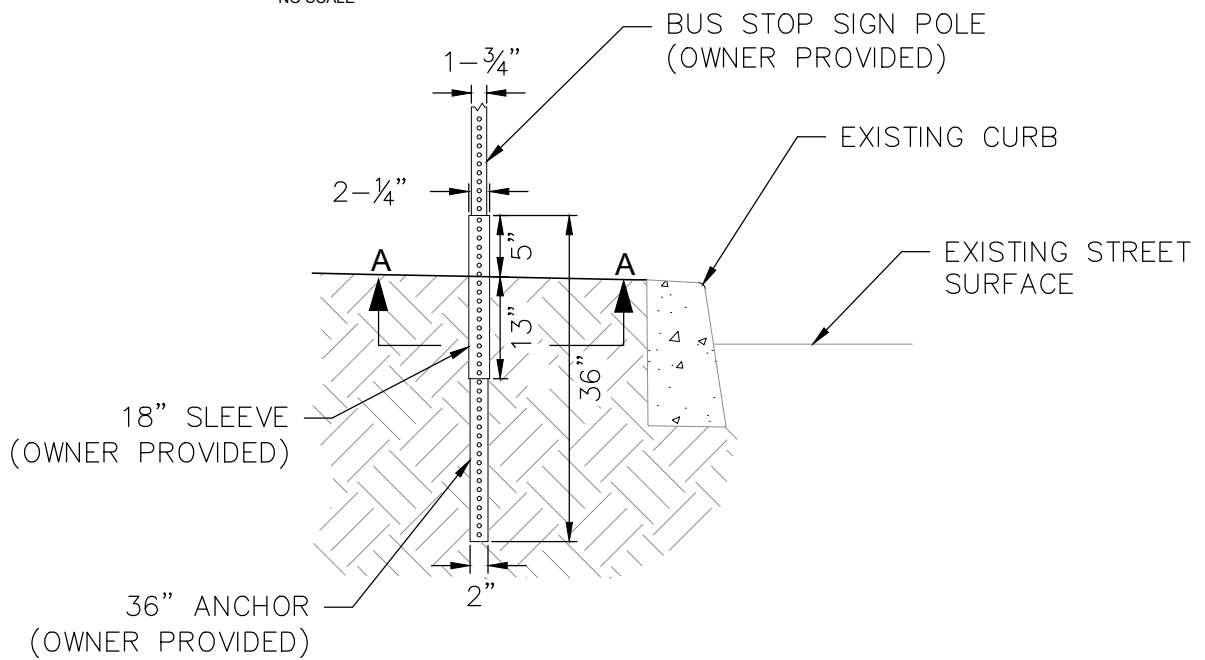
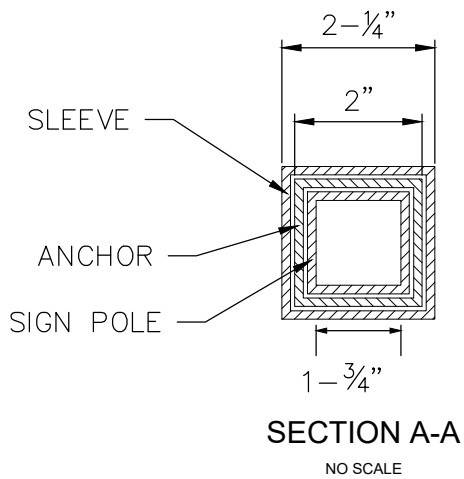


STANDARD CURB

C3

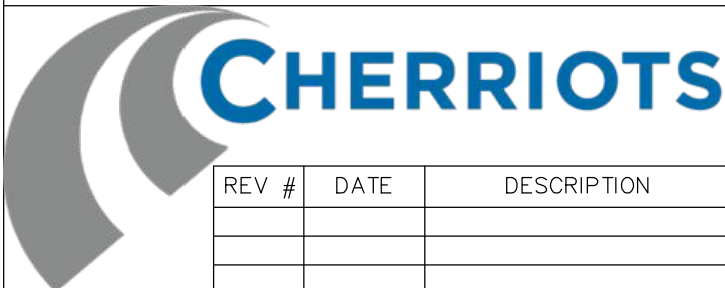
REV #	DATE	DESCRIPTION

BY EGW	CHECKED RDV
DATE 03/03/22	DATE 03/03/22



NOTES:

1. CONTACT CHERRIOTS PLANNING STAFF AT (503) 588-2424 TO OBTAIN SIGN MATERIALS.



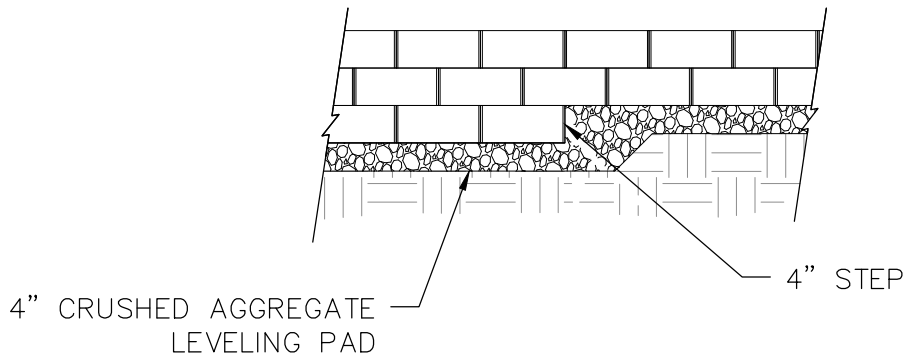
**BUS STOP SIGN POLE,
ANCHOR & SLEEVE
DETAIL**

REV #	DATE	DESCRIPTION

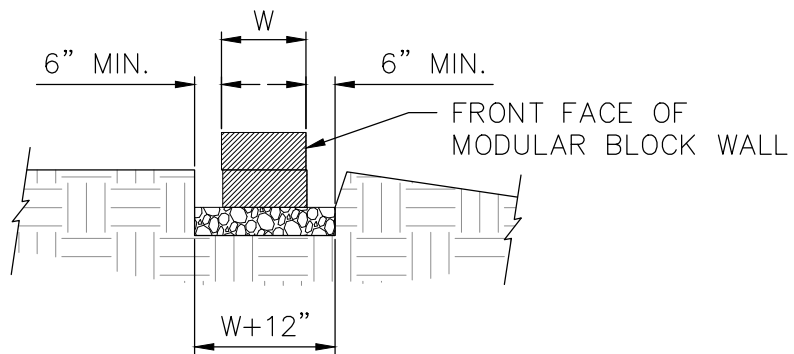
BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22

C4



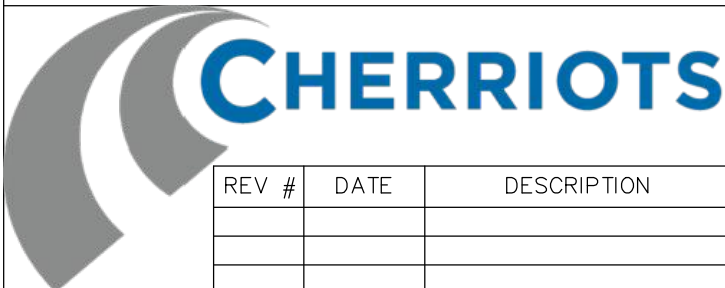
ELEVATION



SECTION

NOTE:

1. LEVELING PAD TO BE 3/4"-0 CRUSHED AGGREGATE COMPACTED TO 95% OF AASHTO T-99



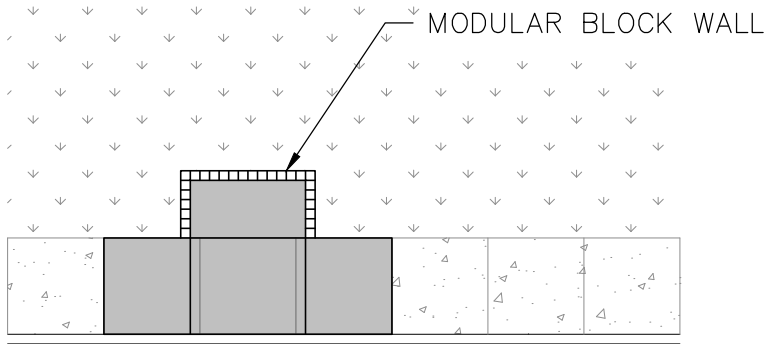
LEVELING PAD
DETAIL

REV #	DATE	DESCRIPTION

BY EGW
DATE 03/03/22

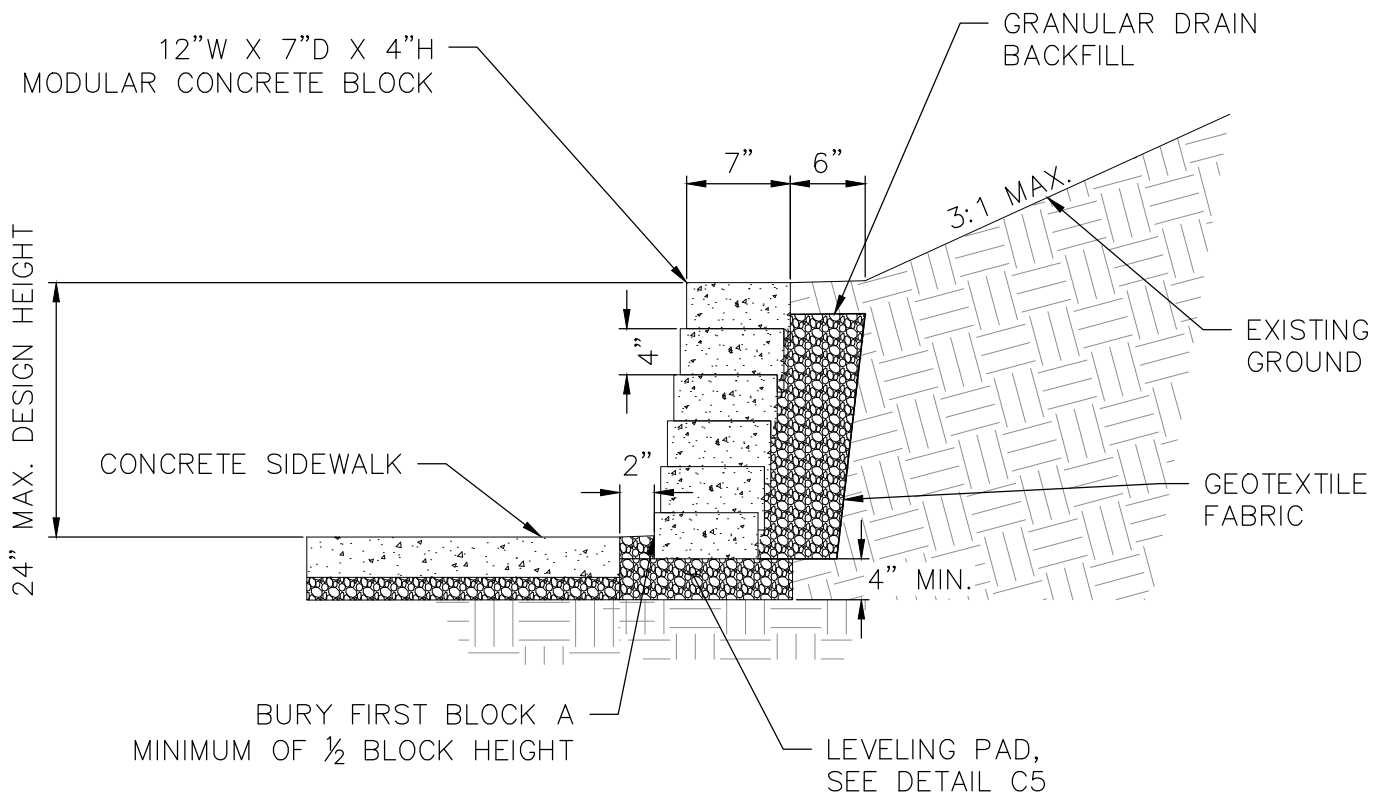
CHECKED RDV
DATE 03/03/22

C5



NOTES:

1. MODULAR BLOCK WALL SHALL BE REQUIRED WHEN SLOPE GRADING TO DAYLIGHT AT 3:1 MAX. CANNOT BE ACHIEVED.
2. THE MODULAR BLOCK WALL SHALL BE CONSTRUCTED WHEN WALL EXPOSURE IS TO BE GREATER THAN 6".



MODULAR BLOCK WALL DETAIL

C6

REV #	DATE	DESCRIPTION

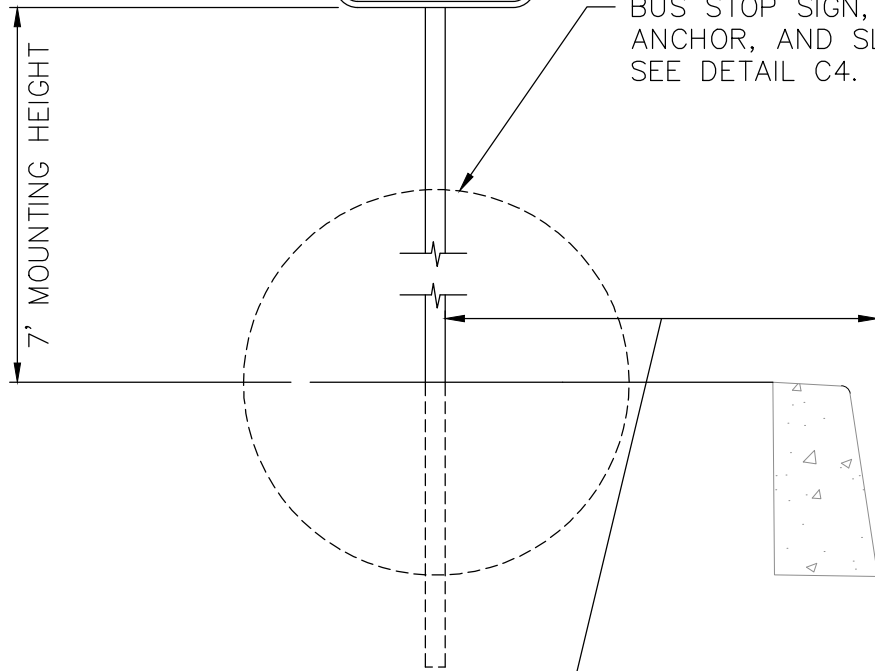
BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22

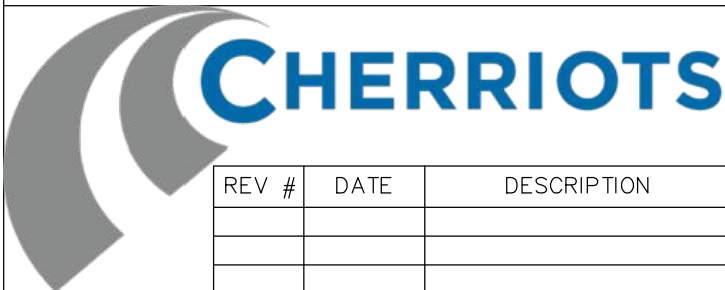
BUS STOP SIGN
(OWNER PROVIDED)



BUS STOP SIGN, POLE,
ANCHOR, AND SLEEVE,
SEE DETAIL C4.



2' FROM FACE OF CURB OR
1' FROM BACK OF SIDEWALK



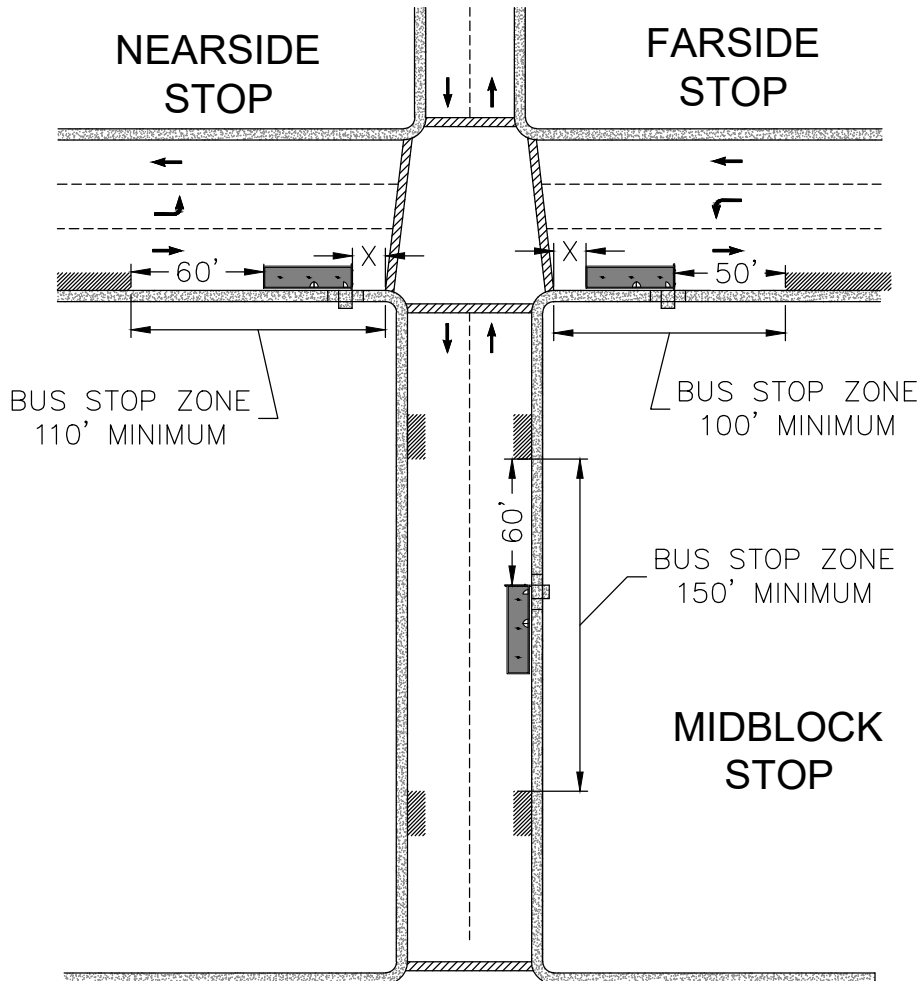
**TYPICAL BUS STOP
SIGN PLACEMENT
DETAIL**

C7

REV #	DATE	DESCRIPTION

BY EGW
DATE 03/03/22

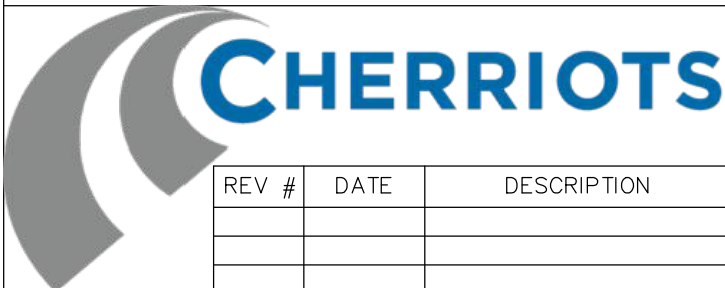
CHECKED RDV
DATE 03/03/22



NOTES:

1. CHERRIOTS TO PROVIDE INFORMATION REGARDING BUS TYPE, LENGTH, AND QUANTITY OF BUSES TO BE SERVICED BY BUS STOP.
2. FOR MULTIPLE BUSES BEING SERVED AT ONE STOP:
 - 2.A. ADD 50 FEET FOR EACH ADDITIONAL STANDARD 40-FOOT BUS.
 - 2.B. ADD 70 FEET FOR EACH ADDITIONAL 60-FOOT ARTICULATED BUS.
3. BUS STOP ZONE SHALL BE SIGNED AS A NO PARKING ZONE PER STANDARDS OF LOCAL JURISDICTION.
4. X = 10' MINIMUM FROM EDGE OF CROSSWALK OR END OF RADIUS, WHICHEVER IS FURTHER FROM THE INTERSECTION.

MINIMUM DISTANCE BETWEEN A BUS STOP AND LEFT TURN (FT)				
POSTED SPEED LIMIT	LANE CHANGES			
	1	2	3	4
30 MPH OR LESS	430	610	790	970
35 MPH	625	875	1125	1375
40 MPH	780	1080	1380	1680
45 MPH	1080	1430	1780	2130
50 MPH	1415	1865	2135	2765
55 MPH	1830	2380	2930	3480



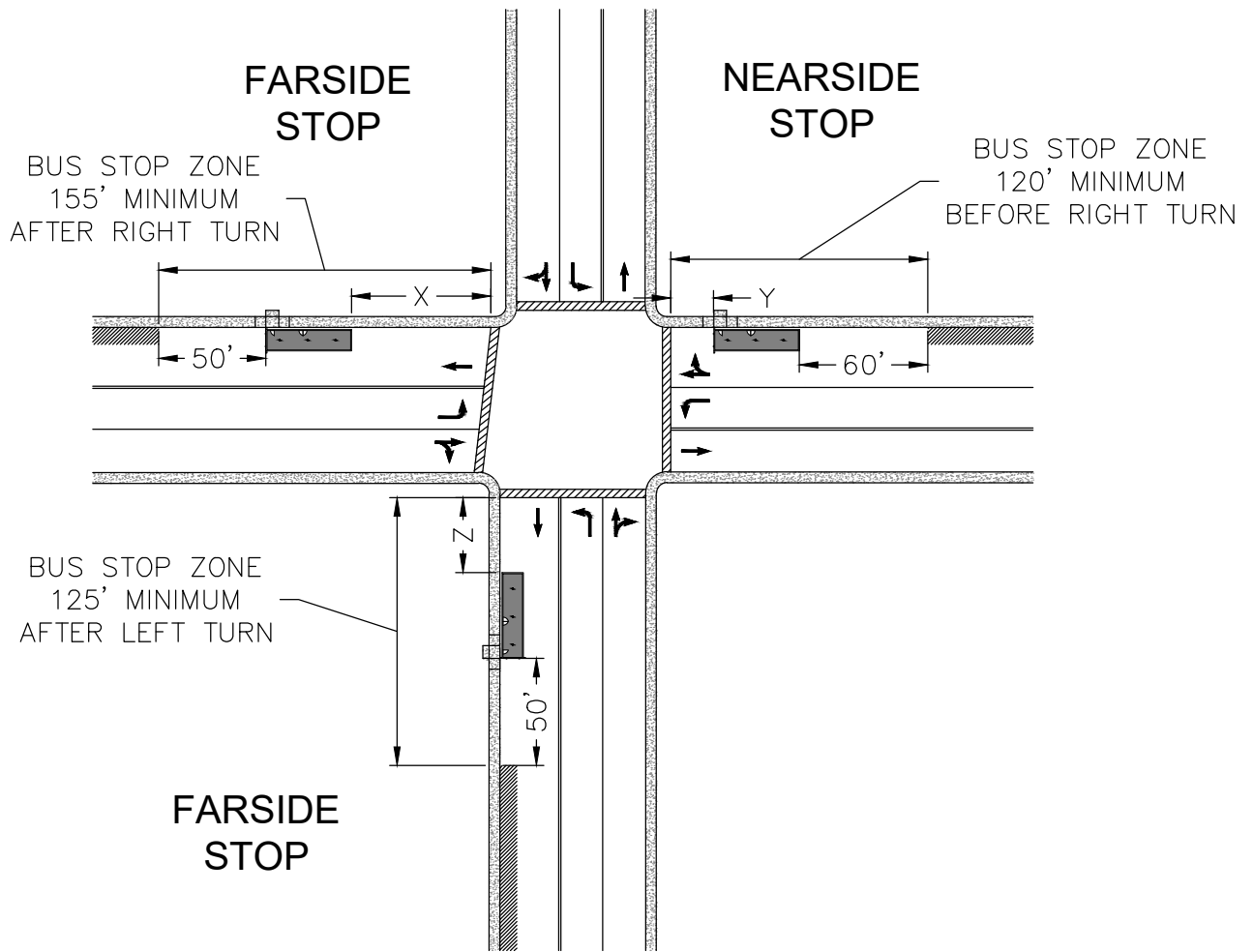
**NO PARKING ZONES AT INTERSECTIONS
DETAIL**

C8

REV #	DATE	DESCRIPTION

BY EGW
DATE 09/22/22

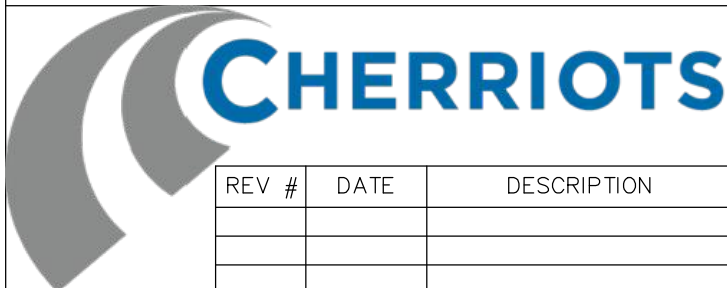
CHECKED RDV
DATE 09/22/22



NOTES:

1. CHERRIOTS TO PROVIDE INFORMATION REGARDING BUS TYPE, LENGTH, AND QUANTITY OF BUSES TO BE SERVICED BY BUS STOP.
2. FOR MULTIPLE BUSES BEING SERVED AT ONE STOP:
 - 2.A. ADD 50 FEET FOR EACH ADDITIONAL STANDARD 40-FOOT BUS.
 - 2.B. ADD 70 FEET FOR EACH ADDITIONAL 60-FOOT ARTICULATED BUS.
3. BUS STOP ZONE SHALL BE SIGNED AS A NO PARKING ZONE PER STANDARDS OF LOCAL JURISDICTION.
4. X = 65' MIN. FROM EDGE OF CROSSWALK OR END OF RADIUS, WHICHEVER IS GREATER.
5. Y = 20' MIN. FROM EDGE OF CROSSWALK OR END OF RADIUS, WHICHEVER IS GREATER.
6. Z = 35' MIN. FROM EDGE OF CROSSWALK OR END OF RADIUS, WHICHEVER IS GREATER.

MINIMUM DISTANCE BETWEEN A BUS STOP AND LEFT TURN (FT)				
POSTED SPEED LIMIT	LANE CHANGES			
	1	2	3	4
30 MPH OR LESS	430	610	790	970
35 MPH	625	875	1125	1375
40 MPH	780	1080	1380	1680
45 MPH	1080	1430	1780	2130
50 MPH	1415	1865	2135	2765
55 MPH	1830	2380	2930	3480



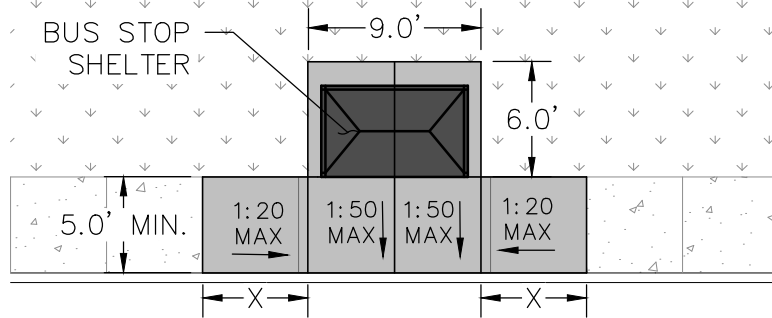
**NO PARKING ZONES WITH TURN LANES
DETAIL**

C9

REV #	DATE	DESCRIPTION

BY EGW
DATE 09/27/22

CHECKED RDV
DATE 09/27/22



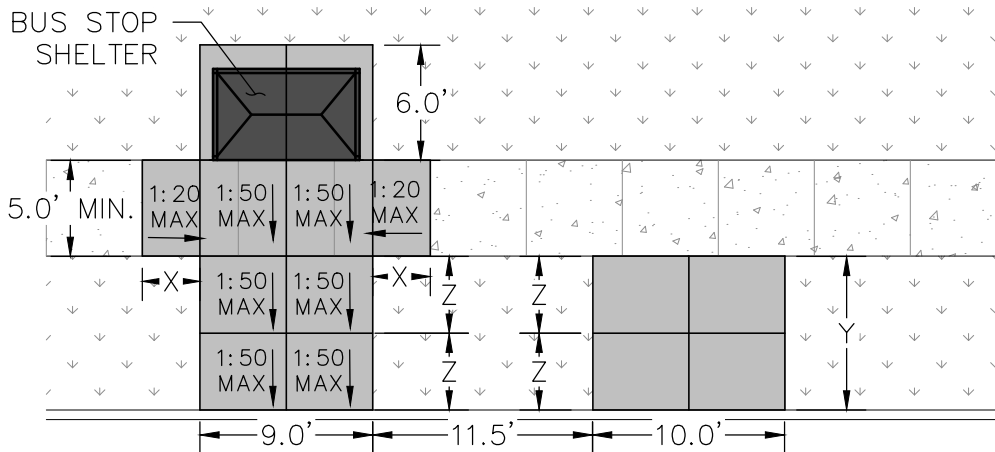
EXISTING ROADWAY

BUS SHELTER ON CURB-TIGHT SIDEWALK

NOT TO SCALE

NOTES:

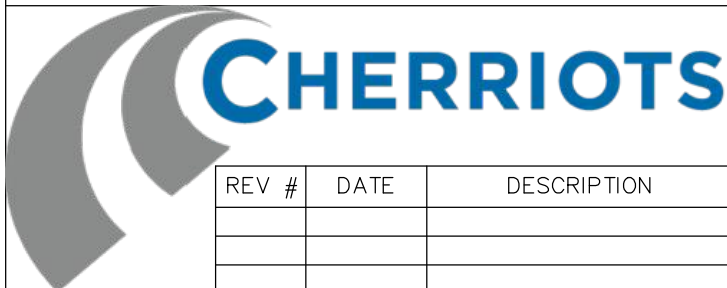
1. "X" WIDTH TO BE A MINIMUM OF 3.0' OR MATCH EXISTING SIDEWALK JOINT SPACING.
2. "Y" LENGTH TO BE EQUAL TO THE WIDTH OF THE PLANTER STRIP, FROM BACK OF CURB TO FRONT OF SIDEWALK.
3. JOINT SPACING, "Z" SHALL BE 3' MINIMUM, 6' MAXIMUM. PROVIDE SIDEWALK PANELS THAT ARE AS SQUARE AS POSSIBLE.
4. MATCH EXISTING WIDTH WHERE PROPOSED BUS STOP PANELS CONNECT TO THE EXISTING WALK.
5. BUS STOP SHELTER TO BE STANDARD BRASCO BUS STOP SHELTER, UNLESS OTHERWISE APPROVED BY CHERRIOTS.
6. BUS STOP SHELTER TO BE CONSTRUCTED AT BACK OF EXISTING WALK.



EXISTING ROADWAY

BUS SHELTER ON SIDEWALK WITH PLANTER STRIP

NOT TO SCALE



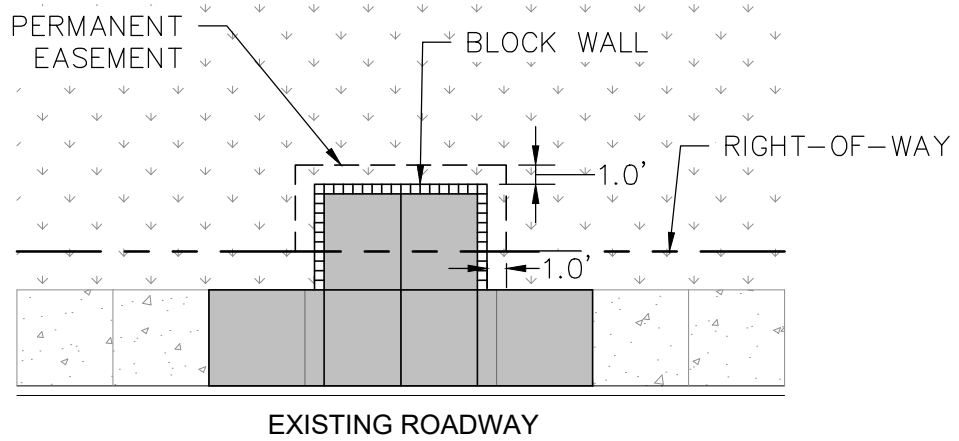
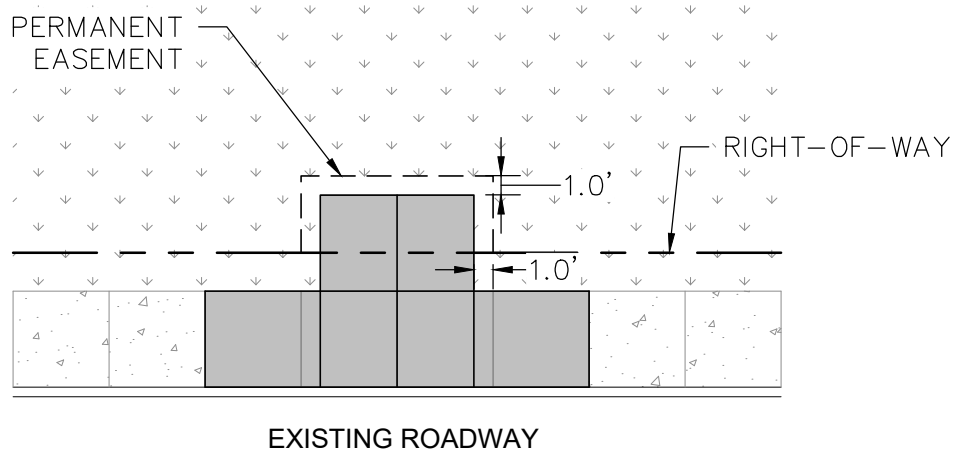
BUS STOP WITH SHELTER DETAIL

C10

REV #	DATE	DESCRIPTION

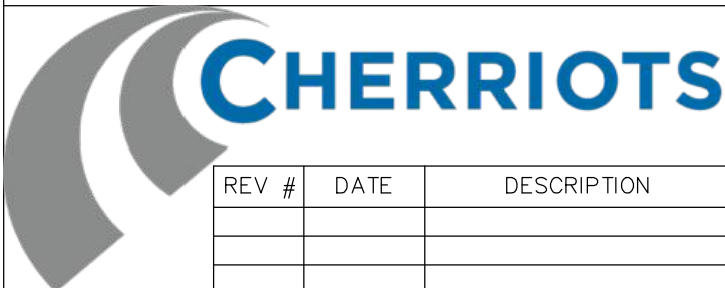
BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22



NOTES:

1. PERMANENT EASEMENT IS REQUIRED WHEN EXTENTS OF DESIGN ENCROACH UPON RIGHT-OF-WAY.
2. PERMANENT EASEMENTS ARE TO BE A MINIMUM OF 1' OFFSET FROM THE PROPOSED CONSTRUCTION EXTENTS THAT FALL OUTSIDE OF RIGHT-OF-WAY.



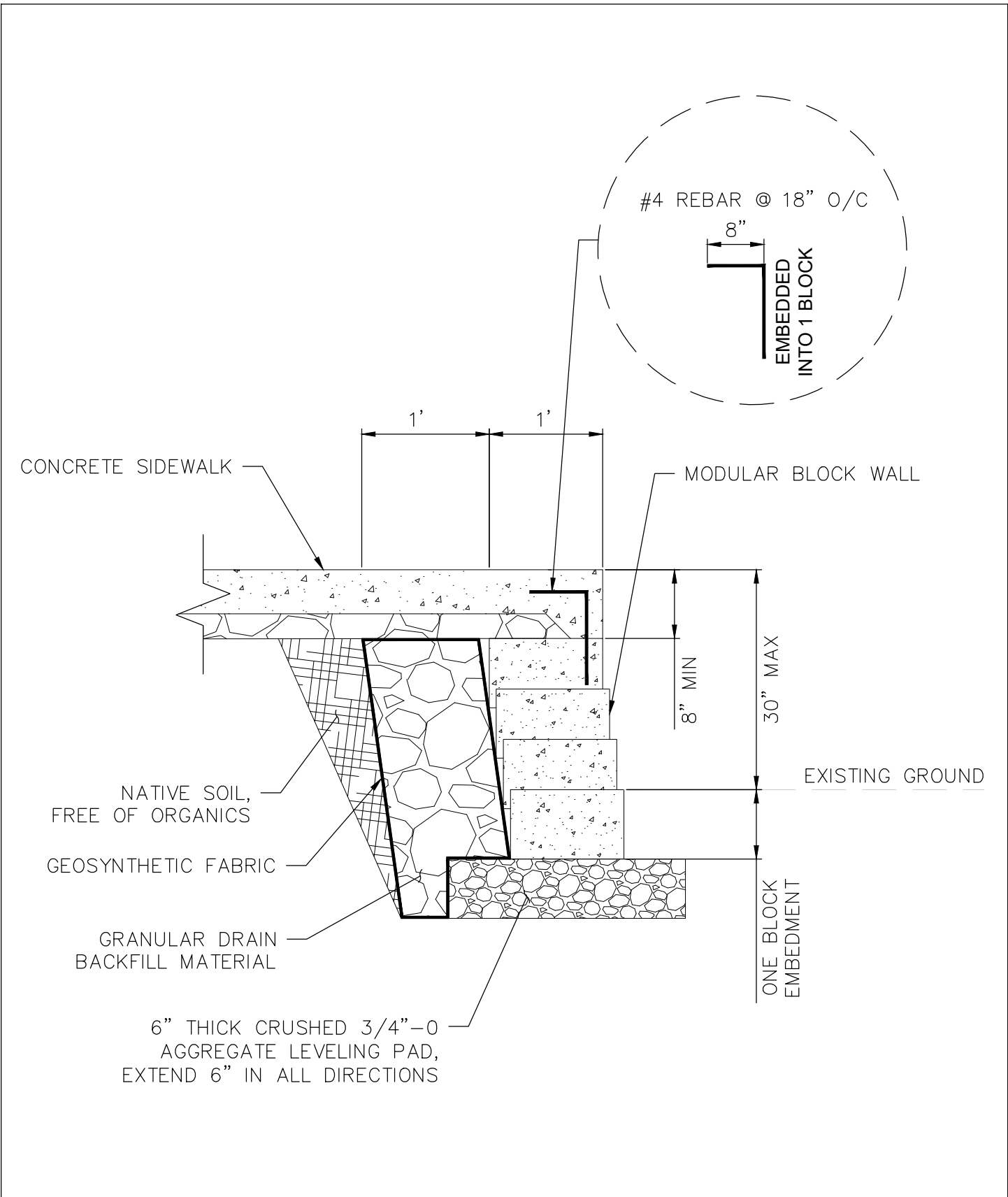
PERMANENT EASEMENT DETAIL

C11

REV #	DATE	DESCRIPTION

BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22

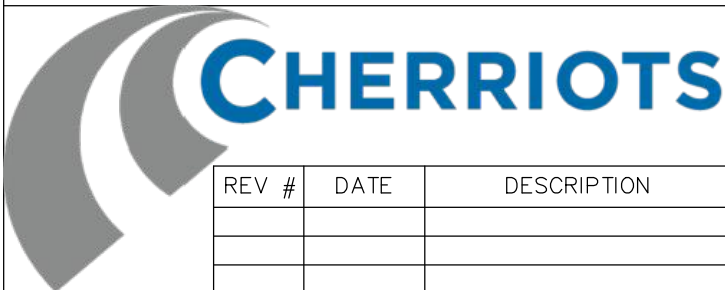
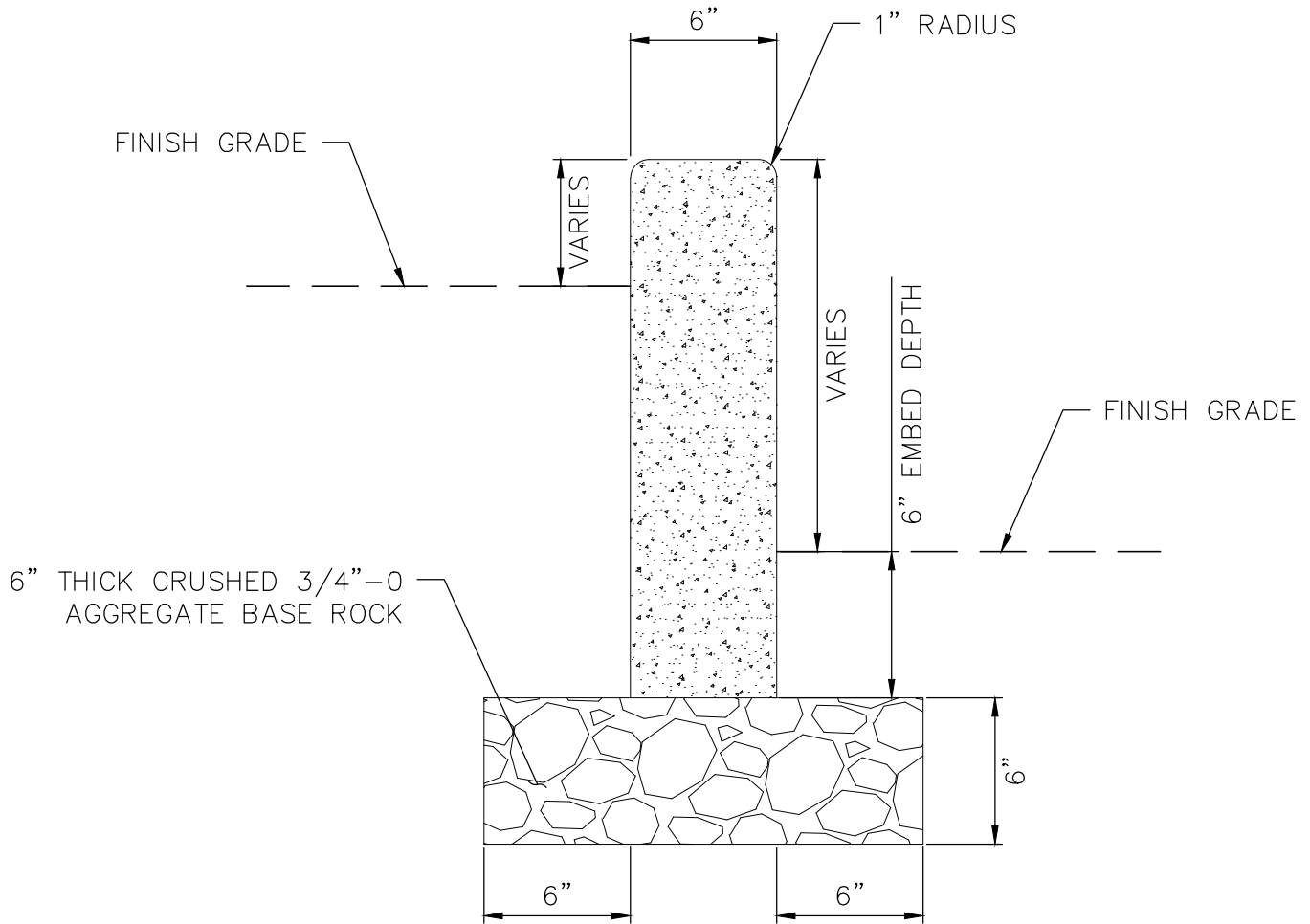


REV #	DATE	DESCRIPTION

**BELOW-GRADE
WALL DETAIL**

BY EGW	CHECKED RDV
DATE 03/03/22	DATE 03/03/22

C12



**PEDESTRIAN CURB
DETAIL**

C13

REV #	DATE	DESCRIPTION

BY EGW
DATE 03/03/22

CHECKED RDV
DATE 03/03/22